



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

Agenda

City Council Regular and Joint Meeting

City Council Chambers | 50 Natoma Street, Folsom CA 95630

June 08, 2021

6:30 PM

Welcome to Your City Council Meeting

We welcome your interest and involvement in the city’s legislative process. This agenda includes information about topics coming before the City Council and the action recommended by city staff. You can read about each topic in the staff reports, which are available on the city website and in the Office of the City Clerk. The City Clerk is also available to answer any questions you have about City Council meeting procedures.

Participation

If you would like to provide comments to the City Council, please:




- Fill out a blue speaker request form, located at the back table.
- Submit the form to the City Clerk before the item begins.
- When it’s your turn, the City Clerk will call your name and invite you to the podium.
- Speakers have three minutes, unless the presiding officer (usually the mayor) changes that time.

Reasonable Accommodations

In compliance with the Americans with Disabilities Act, if you are a person with a disability and you need a disability-related modification or accommodation to participate in this meeting, please contact the City Clerk’s Office at (916) 461-6035, (916) 355-7328 (fax) or CityClerkDept@folsom.ca.us. Requests must be made as early as possible and at least two full business days before the start of the meeting.

How to Watch

The City of Folsom provides three ways to watch a City Council meeting:

In Person	Online	On TV
 <p>City Council meetings take place at City Hall, 50 Natoma Street</p>	 <p>Watch the livestream and replay past meetings on the city website, www.folsom.ca.us</p>	 <p>Watch live and replays of meetings on Sac Metro Cable TV, Channel 14</p>

More information about City Council meetings is available at the end of this agenda



CITY OF
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City Council Regular and Joint Meeting

Folsom City Council Chambers
50 Natoma Street, Folsom, CA
www.folsom.ca.us

Tuesday, June 08, 2021 6:30 PM

Mike Kozlowski, Mayor

Sarah Aquino, Vice Mayor
Kerri Howell, Councilmember

YK Chalamcherla, Councilmember
Rosario Rodriguez, Councilmember

REGULAR CITY COUNCIL AGENDA

Pursuant to Governor Newsom's Executive Order N-29-20, members of the Folsom City Council and staff may participate in this meeting via teleconference.

Due to the coronavirus (COVID-19) public health emergency, the City of Folsom is allowing for remote public input during City Council meetings. Members of the public are encouraged to participate by emailing comments to CityClerkDept@folsom.ca.us. Emailed comments must be received no later than thirty minutes before the meeting and will be read aloud at the meeting during the agenda item. Please make your comments brief. Written comments submitted and read into the public record must adhere to the principles of the three-minute speaking time permitted for in-person public comment at City Council meetings. Members of the public wishing to participate in this meeting via teleconference may email CityClerkDept@folsom.ca.us no later than thirty minutes before the meeting to obtain call-in information. Each meeting may have different call-in information. Verbal comments via teleconference must adhere to the principles of the three-minute speaking time permitted for in-person public comment at City Council meetings.

Members of the public may continue to participate in the meeting in person at Folsom City Hall, 50 Natoma Street, Folsom, CA while maintaining appropriate social distancing and wearing face coverings.

CALL TO ORDER

ROLL CALL:

Councilmembers: Rodriguez, Aquino, Chalamcherla, Howell, Kozlowski

The City Council has adopted a policy that no new item will begin after 10:30 p.m. Therefore, if you are here for an item that has not been heard by 10:30 p.m., you may leave, as the item will be continued to a future Council Meeting.

PLEDGE OF ALLEGIANCE

AGENDA UPDATE

BUSINESS FROM THE FLOOR:

Members of the public are entitled to address the City Council concerning any item within the Folsom City Council's subject matter jurisdiction. Public comments are limited to no more than three minutes. Except for certain specific exceptions, the City Council is prohibited from discussing or taking action on any item not appearing on the posted agenda.

SCHEDULED PRESENTATIONS:

1. Proclamation of the City of Folsom Proclaiming the Month of June as LGBTQ+ Pride Month in the City of Folsom
2. City Manager's Fiscal Year 2020-21 Third Quarter Financial Report

CONSENT CALENDAR:

Items appearing on the Consent Calendar are considered routine and may be approved by one motion. City Councilmembers may pull an item for discussion.

3. Approval of May 25, 2021 Special and Regular Meeting Minutes
4. Resolution No. 10635 – A Resolution Authorizing the City Manager to Execute an Agreement with Folsom Lake Ford for the Purchase of Six Vehicles for the Environmental and Water Resources Department
5. Resolution No. 10636 - A Resolution of the City Council Approving Amendment No. 2 to Lease Agreement with the Folsom Chamber of Commerce for the Depot Building Located at 200 Wool Street
6. Resolution No. 10637 - A Resolution Authorizing the City Manager to Execute Amendment No. 1 to the Agreement (Contract No. 173-21 19-051) with Peterson Brustad Inc. for Design and Construction Administration Services for the Oak Avenue Pump Station Peak Wet Weather Flow Relief Project and Appropriation of Funds
7. Resolution No. 10638 – A Resolution Authorizing the City Manager to Execute an Agreement with Badger Meter, Inc. for the Purchase of 1-Inch Water Meters and Rejecting All Bids for 1.5-Inch and 2-Inch Water Meters
8. Resolution No. 10640 - A Resolution Authorizing the City Manager to Execute Non-Exclusive Agreements with Approved Licensed Contractors to Implement the Sacramento Housing and Redevelopment Agency Inter-Agency Contract for the Seniors Helping Seniors Home Repair Program and Appropriation of Funds
9. Resolution No. 10641 - A Resolution Authorizing the City Manager to Execute a Consultant Services Agreement with Ghirardelli Associates, Inc. for Construction Inspection and Materials Testing for the Pavement Resurfacing Project FY 2020-21
10. Resolution No. 10642 – A Resolution Authorizing the City Manager to Execute the Third Amendment with Sacramento County for Reduced Tipping Fees for Municipal Solid Waste at Kiefer Landfill
11. Resolution No. 10644 - A Resolution Authorizing the Public Works Department to Submit a Rebuilding American Infrastructure with Sustainability and Equity Grant Application for the Riley Street Sidewalks Project
12. Resolution No. 10645 - A Resolution Supporting a Rebuilding American Infrastructure with Sustainability and Equity Grant Application by the Capital Southeast Connector JPA for Connector Segments D3B and E1

- [13.](#) Resolution No. 10647 - A Resolution Authorizing the City Manager to Execute an Agreement with Axon Enterprise, Inc. for the Purchase of Police Body-Worn and In-Car Cameras

PUBLIC HEARING:

- [14.](#) Resolution No. 10643 - A Resolution Adopting the 2020 Urban Water Management Plan and Water Shortage Contingency Plan and Authorizing the City Manager to Submit the Plan to the California Department of Water Resources and to the California State Library
- [15.](#) Resolution No. 10639 - A Resolution of the City Council of the City of Folsom Approving the Issuance of Revenue Bonds by the California Municipal Finance Authority in an Aggregate Principal Amount Not to Exceed \$20,000,000 to Finance a 110-Unit Affordable Rental Housing Facility for Low-Income and Very Low-Income Seniors, for the Benefit of an Entity to be Created by USA Properties Fund, Inc. or an Affiliate, and Certain Other Matters Relating Thereto
- [16.](#) Folsom Ranch Medical Center Development Agreement Amendment – Northeast Corner of East Bidwell Street and Alder Creek Parkway (PN 20-193)
- i. Ordinance No. 1314 - An Uncodified Ordinance of the City of Folsom Approving Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement between the City of Folsom and Dignity Health Relative to the Folsom Ranch Medical Center Project (Introduction and First Reading)

OLD BUSINESS:

- [17.](#) Resolution No. 10646 - A Resolution Approving Changes to Transit Services and Vehicle Branding for Transit Services Provided by Sacramento Regional Transit Under the 2019 Annexation Agreement with the Sacramento Regional Transit District

NEW BUSINESS:

- [18.](#) Resolution No 10648 – A Resolution of Support for the Sacramento Abandoned Vehicle Service Authority to Pursue Extension of the Vehicle Registration Fee for Abatement of Abandoned Vehicles

CONVENE JOINT MEETING

JOINT CITY COUNCIL AGENDA

Joint Folsom City Council, Folsom Redevelopment Successor Agency, Folsom Public Financing Authority, South of 50 Parking Authority, and Folsom Ranch Financing Authority Meeting

ROLL CALL: Council / Board Members: Rodriguez, Aquino, Chalamcherla, Howell, Kozlowski

CONSENT CALENDAR:

- [19.](#) Approval of the March 23, 2021 Joint City Council / Successor Agency / Public Financing Authority / Folsom South of 50 Parking Authority / Folsom Ranch Financing Authority Meeting Minutes
- [20.](#) Receive and File the City of Folsom, the Folsom Redevelopment Successor Agency, the Folsom Public Financing Authority, the Folsom Ranch Financing Authority, and the South of 50 Parking Authority Monthly Investment Reports for the Month of March 2021

ADJOURNMENT

RECONVENE CITY COUNCIL MEETING

CITY MANAGER REPORTS:

COUNCIL COMMENTS:

ADJOURNMENT

The City Council's next regular meeting is scheduled for June 22, 2021.

NOTICE: *Members of the public are entitled to directly address the City Council concerning any item that is described in the notice of this meeting, before or during consideration of that item. If you wish to address Council on an issue, which is on this agenda, please complete a blue speaker request card, and deliver it to a staff member at the table on the left side of the Council Chambers prior to discussion of the item. When your name is called, stand to be recognized by the Mayor and then proceed to the podium. If you wish to address the City Council on any other item of interest to the public, when the Mayor asks if there is any "Business from the Floor," follow the same procedure described above. Please limit your comments to three minutes or less.*

NOTICE REGARDING CHALLENGES TO DECISIONS: *Pursuant to all applicable laws and regulations, including without limitation, California Government Code Section 65009 and or California Public Resources Code Section 21177, if you wish to challenge in court any of the above decisions (regarding planning, zoning and/or environmental decisions), you may be limited to raising only those issues you or someone else raised at the public hearing(s) described in this notice/agenda, or in written correspondence delivered to the City at, or prior to, the public hearing.*

As presiding officer, the Mayor has the authority to preserve order at all City Council meetings, to remove or cause the removal of any person from any such meeting for disorderly conduct, or for making personal, impertinent, or slanderous remarks, using profanity, or becoming boisterous, threatening or personally abusive while addressing said Council, and to enforce the rules of the Council.

PERSONS INTERESTED IN PROPOSING AN ITEM FOR THE CITY COUNCIL AGENDA SHOULD CONTACT A MEMBER OF THE CITY COUNCIL.

The meeting of the Folsom City Council is being telecast on Metro Cable TV, Channel 14, the Government Affairs Channel, and will be shown in its entirety on the Friday and Saturday following the meeting, both at 9 a.m. The City does not control scheduling of this telecast and persons interested in watching the televised meeting should confirm this schedule with Metro Cable TV, Channel 14. The City of Folsom provides live and archived webcasts of regular City Council meetings. The webcasts can be found on the online services page of the City's website www.folsom.ca.us.

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Any documents produced by the City and distributed to the City Council regarding any item on this agenda will be made available at the City Clerk's Counter at City Hall located at 50 Natoma Street, Folsom, California and at the Folsom Public Library located at 411 Stafford Street, Folsom, California during normal business hours.

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**PROCLAMATION
OF THE MAYOR OF THE CITY OF FOLSOM
PROCLAIMING THE MONTH OF JUNE AS
LGBTQ+ PRIDE MONTH IN THE CITY OF FOLSOM**

WHEREAS, on June 1, 2009 President Obama issued Proclamation No. 8387 for Lesbian, Gay, Bisexual, and Transgender Pride Month, and each year supporters come together in June for various celebrations; and

WHEREAS, LGBTQ+ Pride Month is a time to recognize and celebrate the impact of LGBTQ+ individuals locally, nationally, and internationally; and

WHEREAS, the City of Folsom is a welcoming city that is dedicated to fostering acceptance of all residents and preventing discrimination and bullying; and

WHEREAS, the City of Folsom is committed to being inclusive and welcoming to all regardless of their sexual orientation;

WHEREAS, the City of Folsom is strengthened by and thrives upon the rich diversity of racial, ethnic, cultural, and sexual identities of its residents; and

WHEREAS, we continue to raise awareness of the existence of discrimination and have improved laws and attitudes of society by encouraging inclusiveness; and

WHEREAS, the City of Folsom supports the rights of every citizen to experience equality and freedom from discrimination:

NOW, THEREFORE, I, MICHAEL D. KOZLOWSKI, Mayor of the City of Folsom, on behalf of the Folsom City Council, do hereby proclaim June 2021 as LGBTQ+ Pride month and encourage everyone to treat others with mutual respect and understanding.

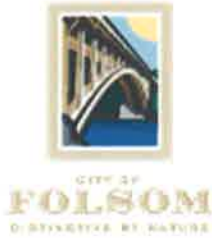
PROCLAIMED this 8th day of June 2021.

Michael D. Kozlowski, MAYOR

Attest:

Christa Freemantle, City Clerk

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Folsom City Council Staff Report

MEETING DATE:	6/8/2021
AGENDA SECTION:	Scheduled Presentations
SUBJECT:	City Manager’s Fiscal Year 2021 Third Quarter Financial Report
FROM:	Finance Department

RECOMMENDATION / CITY COUNCIL ACTION

It is recommended that the City Council receive and file the City Manager’s Fiscal Year 2020-21 Third Quarter Financial Report.

BACKGROUND / ISSUE

Section 5.05R of the Charter of the City of Folsom requires the City Manager submit to the City Council a financial and management report showing the relationship between budgeted and actual revenues, and expenditures and encumbrances on a quarterly basis.

This Quarterly Financial Report is an analysis of the unaudited financial status of the City’s major funds for the third quarter of Fiscal Year (FY) 2020-21, covering the nine-month period from July 2020 through March 2021. Tables and graphs have been integrated into the report to help illustrate financial performance. Please refer to the Appendices of the report for detailed schedules of the City’s key funds for the period ended March 31, 2021, including cumulative fund balances from the prior year.

POLICY / RULE

Section 5.05R of the Charter of the City of Folsom requires the City Manager submit to the City Council a financial and management report showing the relationship between budgeted and actual revenues, and expenditures and encumbrances on a quarterly basis.

Section 3.02.050 (b) of the Folsom Municipal Code states “... within 30 days after the end of each quarter during the fiscal year, and more often if required by the City Council, the City Manager shall submit to the City Council a financial and management report.”

ANALYSIS

Unemployment as of March 2021 was 8.2% and 7.3% in California and Sacramento County respectively and in Folsom the unemployment rate was 4.5%. The UCLA Anderson Forecast is expecting unemployment in California to fall to 5.2% by the end of 2021.

For FY 2021 the General Fund is projected to end the year with the unassigned fund balance of \$14.72 million, an increase from \$13.78 million in FY 2020. The percentage of unrestricted fund balance to expenditures is projected to increase to 16.5% from 14.4% in FY 2020, which is due to lower projected expenditures in FY 2021. Projected revenues of \$90.31 million is an increase of \$2.88 million or 3.29% over FY 2020, mostly due to CARES Act funding received in the amount of \$1.01 million and development south of Highway 50. Projected expenditures of \$89.20 million is a decrease of \$6.21 million from FY 2020. After adjusting for the Transit annexation to Sacramento Regional Transit, the reduction in expenditures is \$3.26 million or 3.42%. The Fiscal Year 2021 budgeted expenditures are \$89.54 million and compared to the year-end projection, the projection is \$342,000 less or 0.38%.

The FY 2021 projected revenue increase over appropriated revenues is seen mostly in charges for services, permits and intergovernmental revenue. Property tax received through the third quarter was in the amount of \$15.66 million and compared to the prior fiscal year of \$14.69 million is an increase of \$971,000 or 6.61%. Property tax is projected to be \$29.88 million which would end the fiscal year 3.75% higher than the FY 2020 property tax receipts which were \$28.80 million. This increase is attributed to the valuation increase when properties are sold as well as sales of new homes. The average median sales price through the third quarter of FY 2021 was \$619,359 which is an increase of 6.83% over the prior fiscal year.

Sales tax is projected to end the fiscal year at \$23.50 million, an increase of \$ 716,000 over the budgeted amount of \$22.78 million. In comparison to FY 2020 sales tax receipts are projected to increase by approximately \$884,000 or 3.91%. The FY 2020 amount was \$22.62 million.

Through the third quarter charges for services were \$8.56 million which is a decrease of \$1.15 million (11.88%) when compared to the 3rd Quarter of the prior year amount of \$9.77 million. Charges for services are projected to end the fiscal year at \$12.54 million which is \$1.05 million more than the budget and approximately \$25,500 (.20%) more than the prior fiscal year. The increase from the current year budgeted amount is seen in development fees (\$982,000), Police Department charges (\$445,000) and Fire Department charges (\$213,000). Charges in the Public Safety Departments are reimbursements from the California Office of Emergency Services for fire strike teams and reimbursements for grants.

The FY 2021 projected expenditures are \$89.20 million, \$342,000 less than the appropriated amount (\$89.54 million). The projected expenditures include savings in salaries and benefits of \$869,000, a decrease in capital outlay of \$619,000, which are partially offset by increases

in contracts and service and supplies. The quarter-to-quarter comparison shows expenditures decreased by \$6.61 million or 9.18% compared to the same period last year.

The Solid Waste, Water and Wastewater Utility Operating Funds are all projected to end the fiscal year with operating revenues exceeding operating expenses. Net assets in the Water and Solid Waste Operating Funds are projected to decrease. The decrease in net assets in both funds is due to capital outlay expenses. In the case of Water, the capital outlay is for capital projects and in Solid Waste it is the purchase of vehicles. A comparison of the quarter-to-quarter expenses and revenues shows charges for service revenues in Water increased by \$1.85 million (18.09%) and operating expenses increased by \$281,000 (2.63%). Wastewater Operating charges for services revenues increased by \$1.12 million (21.69%) and operating expenses decreased by \$112,000 (2.26%). Solid Waste Operating charges for services revenues increased by \$2.62 million (30.52%) and operating expenses increased by \$1.13 million (12.20%). Increases in charges for services revenues is due to the implementation of the rate increase and increases in expenses are mainly due to employee, contract and supply costs.

Expenditures for capital improvements decreased in Water, but only slightly, when compared to the same quarter in the prior year and increased in Wastewater and Solid Waste.

The Risk Management Fund is projected to end the year with unrestricted net assets of \$6.12 million or 32.51% of operational expenses. A quarter-to-quarter comparison shows revenues decreased by \$932,000 and expenses increased by \$1.24 million. The increase is seen mostly in transfers out and property insurance and health care costs.

The negative cash balance in the Trail Grant Fund through the third quarter is \$1.13 million. A quarter-to-quarter comparison shows the negative balance decreased by \$148,000. The negative balance is mainly due to costs associated with the Oak Parkway Trail Undercrossing project of which \$1.03 million of grant reimbursements has not yet been received.

The Compensated Leaves Fund is estimated to end the fiscal year with an unrestricted fund balance of \$59,010. Compared to the prior fiscal year the fund balance is projected to decrease by approximately \$566,000 which is due to a budgeted use of the fund balance in the current fiscal year.

ATTACHMENTS

- 1. City Manager’s Fiscal Year 2020-21 Third Quarter Financial Report

Submitted,



Elaine Andersen
City Manager



Stacey Tamagni
Finance Director/CFO



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

City of Folsom Quarterly Financial Report

Fiscal Year 2020-21 Third Quarter

June 8, 2021

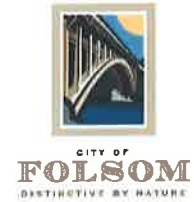
**Prepared by the Office of Management and Budget
Financial Analysis and Reporting Division**

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Third Quarter Financial Report

Fiscal Year 2020-21



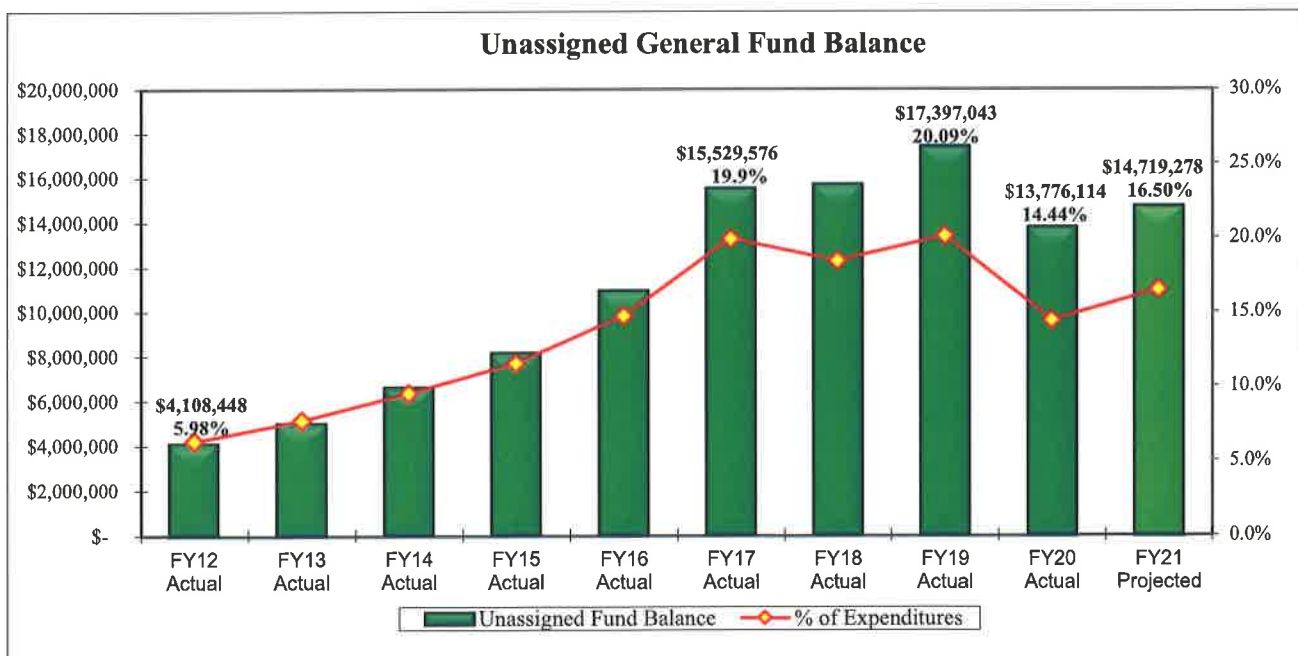
Introduction

This financial report provides an overview of the City’s unaudited financial position through the third quarter of fiscal year (FY) 2020-21 (July 1, 2020, through March 31, 2021) for (1) the General Fund, (2) Housing Special Revenue Fund and L&L Districts, (3) the major enterprise operating funds, and (4) the Risk Management Internal Service Fund. Notable cumulative third quarter to third quarter and budget to actual comparisons are included in this report in addition to year-end projections.

Executive Summary

Through the first three quarters of FY 2020-21, the COVID-19 public health emergency continued to cause a sharp downturn in the City’s revenues. Restrictions have continued to effect Parks and Recreation programming revenues more than other revenue categories. This has resulted in a projected \$860,000 loss in Parks and Recreation programming revenues by June 30, 2021, compared to the budget and \$1.6 million when compared to the prior fiscal year. The City did receive \$1.01 million in CARES Act funding which has been used to offset the costs associated with keeping customers and staff safe both in City buildings and out in the community. The City also received a credit from the state for unemployment costs. The total unemployment charges through the 3rd Quarter of FY 2020-21 were \$244,756 of which \$117,735 was offset by the credit. The unemployment costs were due to all temporary staff released due to the pandemic. The total projected year-end General Fund revenues are \$90.31 million and projected expenditures are \$89.20 million.

We project the General Fund’s unassigned fund balance will increase from \$13.78 million to \$14.72 million at the fiscal year end. Below is a chart of the unassigned fund balance over the last ten years and displays the projected change from FY 2019-20 to FY 2020-21.



General Fund: Operating Revenues

The following table includes cumulative revenue comparisons through the third quarter of FY 2019-20 and FY 2020-21 and a revenue budget comparison for FY 2020-21 with year-end projections.

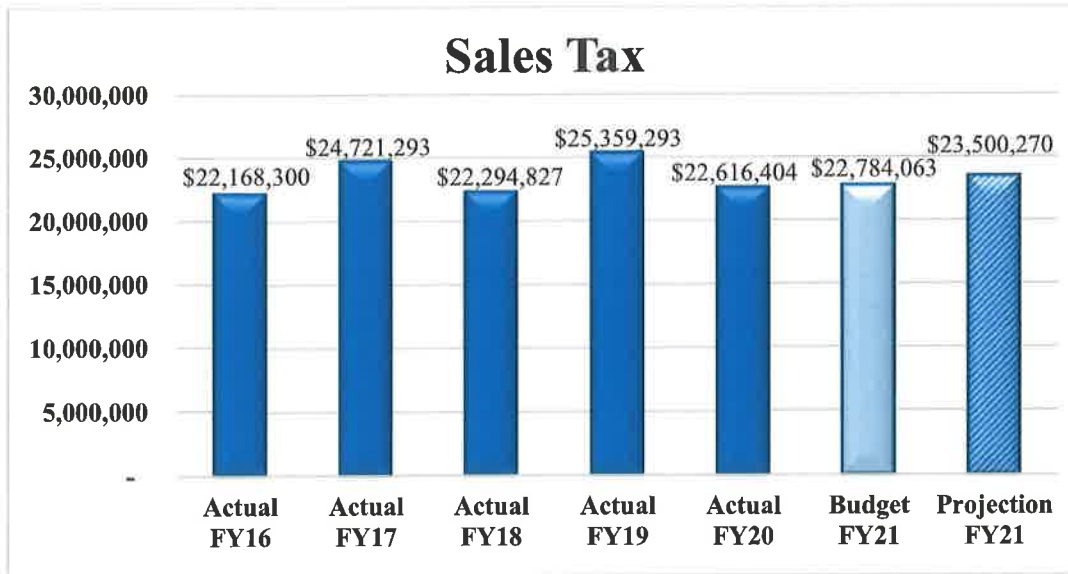
	FY 19-20 Actual Mar. 31, 2020	FY 20-21 Actual Mar. 31, 2021	FY 20-21 Budget	FY 20-21 Projected	Over/Under Budget	% of Budget
Property Tax	\$ 14,685,205	\$ 15,656,039	\$ 29,884,989	\$ 29,884,989	\$ -	100%
Sales Tax	14,266,080	14,763,739	22,784,063	23,500,270	716,207	103%
Transient Occupancy Tax	1,186,394	718,276	1,255,000	1,000,000	(255,000)	80%
Charges for Services	9,774,195	8,561,523	11,481,205	12,535,069	1,053,864	109%
License, Permits & VLF	6,898,827	8,371,898	10,565,421	12,346,423	1,781,002	117%
Transfers In	3,686,941	4,083,086	10,043,152	6,623,478	(3,419,674)	66%
All Other	1,951,410	885,221	3,524,644	3,407,442	(117,202)	97%
Subtotal Revenue	\$ 52,449,051	\$ 53,039,782	\$ 89,538,474	\$ 89,297,671	\$ (240,803)	99.73%
CARES Act	-	1,007,649	-	1,007,649	1,007,649	
Total Revenue	\$ 52,449,051	\$ 54,047,431	\$ 89,538,474	\$ 90,305,320	\$ 766,846	100.86%

General Fund operating revenues through the third quarter are \$54.05 million, or 3.05% above the same period in FY 2019-20. Revenues are at 60.36% of the budget through the third quarter of the current year, primarily due to the timing associated with receiving some of the larger revenue sources. For instance, property tax is the largest General Fund revenue source, but funding is received in two installments. The second installment will be received during the fourth quarter of the fiscal year.

The following is an explanation of the notable variances:

- Property tax revenues exceeded last year's cumulative third quarter by 6.61% or \$971,000. The number of home sales from July through March was 772 and an increase from the prior year of 160 sales (26.14%). The median sales price has also increased by approximately 7.96%. The property tax revenue projection for Fiscal Year 2020-21 year-end is the budgeted amount of \$29.88 million, which would exceed the prior year by \$1.08 million or 3.75%.
- Sales tax revenues also exceeded last year's cumulative third quarter by 3.49% or \$498,000. Except for the categories of construction, business to business and the countywide pool, all other categories show a decrease in a quarter-to-quarter comparison and only construction and the countywide pool show an increase when compared to the same quarter from the prior year. It is not surprising that the categories of apparel stores and restaurants show the largest declines due to closures caused by the public health emergency.

Based on the latest sales tax forecast, sales tax is trending to end the year slightly above the budget at \$23.50 million, an increase from the prior year of \$884,000 or 3.91%. Below is a graph showing sales tax revenue for the current fiscal year and the past five fiscal years.



- Transient Occupancy Tax (TOT) collections are at \$718,000 through the third quarter and are projected to end the fiscal year at the budgeted amount of \$1 million, a decrease of \$677,600 (40.39%) when compared to the FY 2020 amount. This decrease is due to hotel stays being severely reduced during the public health emergency.
- Charges for services, including Building fees, Parks and Recreation user fees, and Ambulance fees are at \$8.6 million through the third quarter and are projected to end the fiscal year at \$12.54 million. The current projection is \$25,000 more than the prior year amount of \$12.51 million. The revenues from Parks and Recreation fees through the third quarter have continued to be impacted especially hard as facilities and programs have had to reduce attendance or stop all together. The Parks and Recreation charges through the third quarter were \$788,000. When compared to the prior fiscal year this is a decrease of \$2.16 million (73.24%). Ambulance fees through the third quarter were \$2.81 million and compared to the prior fiscal year this is a decrease of \$403,500 or 12.55%. The projection for Ambulance Fees at fiscal year-end is \$3.97 million, which will be a decrease from the prior year of \$611,000 or 13.36%. Community Development charges are at \$2.77 million through the third quarter and are projected to end the fiscal year at \$3.76 million. Compared to the prior fiscal year this would be an increase of \$934,000 or 33.07%.
- License and permit fees quarter to quarter increased \$84,000 and are projected to end the fiscal year at \$3.75 million which would be an increase of \$1.34 million compared to the budget and an increase of \$465,600 when compared to the prior year. The increase is due to continued development activity predominately in the plan area.
- Other revenues increased, 44.76% or \$272,000, over the third quarter FY 2019-20. The increase is due to the discontinuance of the sewer credit program with Sacramento Regional Sewer District (SRSD). SRSD remitted to the City its portion of the balance in the program, in the amount of \$293,954.11.

The amount received from SRSD and the \$1.01 million received in CARES Act funding totals \$1.30 million. It is interesting to note that without this additional unexpected \$1.30 million the year-end projection would be \$89.0 million and would be \$535,000 less than the budgeted amount.

General Fund: Department Operating Expenditures

The following table includes cumulative third quarter actual expenditure comparisons for FY 2019-20 and FY 2020-21 and an expenditure budget-to-actual comparison for FY 2020-21.

	FY 19-20 Actual Mar. 31, 2020	FY 20-21 Actual Mar. 31, 2021	FY 20-21 Budget	FY 20-21 Projected	Over/Under Budget	% of Budget
Salaries	\$ 29,355,035	\$ 29,361,780	\$ 38,354,558	\$ 39,067,789	\$ 713,231	101.9%
Benefits	19,941,597	19,995,493	27,628,885	26,827,134	(801,751)	97.1%
O&M	18,403,178	13,587,022	19,722,150	20,089,828	367,678	101.9%
Capital Outlay	2,453,403	1,551,005	2,928,000	2,308,560	(619,440)	78.8%
Debt Service	682,346	901,381	904,881	902,881	(2,000)	99.8%
Adj. for Transit Annexation	1,172,207	-	-	-	-	-
Total Expenditures	\$ 72,007,766	\$ 65,396,681	\$ 89,538,474	\$ 89,196,192	\$ (342,282)	99.6%

Overall, cumulative third quarter General Fund expenditures decreased 9.18% compared to the prior year. This is in part due to a planned decrease in the budget due to anticipated revenue reductions related to COVID-19 restrictions. General Fund expenditures are coming in at 73.04% percent of the budget through the third quarter of FY 2020-21. The projection for the end of the fiscal year is for expenditures to be at \$89.20 million which would be \$342,282 less than the budgeted amount or 99.63% of budget. Staff has been diligent to keep expenses from exceeding the budget, which was already reduced, and this is seen in the projection of the fund balance at year-end.

The table below shows a comparison for FY 2019-20 and FY 2020-21 for each General Fund Department.

	FY 19-20 Actual Mar. 31, 2020	FY 20-21 Actual Mar. 31, 2021	FY 20-21 Budget	FY 20-21 Projected	Over/Under Budget	% of Budget
City Council	\$ 97,869	\$ 77,415	\$ 126,140	\$ 104,080	\$ (22,060)	82.5%
City Manager	1,028,638	851,330	1,311,117	1,153,862	(157,255)	88.0%
City Clerk	454,937	505,665	682,889	656,403	(26,486)	96.1%
City Attorney	727,470	717,258	1,056,421	979,430	(76,991)	92.7%
Mgmt & Budget	3,633,265	3,808,555	4,864,469	5,032,645	168,176	103.5%
Human Resources	521,886	454,342	863,321	627,918	(235,403)	72.7%
Police	17,541,333	17,726,984	24,102,279	23,637,218	(465,061)	98.1%
Fire	17,735,683	17,378,595	20,746,292	22,678,836	1,932,544	109.3%
Community Dev	4,947,107	4,832,403	5,606,333	6,782,350	1,176,017	121.0%
Parks & Recreation	11,742,342	9,929,581	15,121,284	14,087,050	(1,034,234)	93.2%
Library	1,491,444	1,329,431	1,945,379	1,842,460	(102,919)	94.7%
Public Works	4,919,042	5,194,411	7,246,322	7,006,299	(240,023)	96.7%
Non-Departmental	5,994,544	2,590,711	5,866,228	4,607,641	(1,258,587)	78.5%
Adj. for Transit Annex	1,172,207	-	-	-	-	-
Total Expenditures	\$ 72,007,767	\$ 65,396,681	\$ 89,538,474	\$ 89,196,192	\$ (342,282)	99.6%

The following is an explanation of the department specific variances of year-end projections as compared to the budget:

- Community Development department is projected to end the fiscal year \$1.18 million (20.98%) over the budgeted amount, which is mostly due to anticipated increases in contract costs that are mostly offset by increased revenues.
- Fire department is projected to end the fiscal year \$1.93 million (9.32%) over the budgeted amount, which is mainly seen in overtime costs, vehicle maintenance and contracts. Overtime costs have been impacted by employees out after an exposure to COVID-19. Contract cost increases are due to outside costs for plan checks.
- Management and Budget department is projected to end the fiscal year \$168,000 (3.46%) over the budgeted amount, which is mostly due to increased software maintenance costs.
- Parks and Recreation department is projected to end the fiscal year \$1.03 million (6.84%) less than the budgeted amount, which is mostly in capital outlay costs of approximately \$431,000 mostly for the activity

pool renovation and savings in salaries and benefits of approximately \$643,000 which is due to reduced programing. The salary savings is off-set by the increased costs in unemployment of \$348,000.

Overall General Fund departments' expenditures are trending at budget (in line with the 75% expectation) at this point in the fiscal year.

Enterprise Funds:

Water Fund

The Water Fund is reported on a combined basis and includes the following funds: Water Impact, Water Operating, Water Capital and Water Meters.

The table below includes cumulative third quarter actual revenue and expense comparisons for FY 2019-20 and FY 2020-21 and a budget to actual comparison for FY 2020-21 for the Water Operating Fund.

	FY 19-20 Actual Mar. 31, 2020	FY 20-21 Actual Mar. 31, 2021	FY 20-21 Budget	FY 20-21 Projected	Over/Under Budget	% of Budget
Program Revenues	\$ 10,250,731	\$ 12,105,211	\$ 16,116,000	\$ 17,647,029	\$ 1,531,029	109.5%
Salaries	2,148,559	2,212,930	3,158,943	3,003,880	(155,063)	95.1%
Benefits	1,570,576	1,651,684	2,296,088	2,232,113	(63,975)	97.2%
Operating Expenses	3,149,067	3,500,563	6,687,690	4,903,810	(1,783,880)	73.3%
Transfers Out	616,962	668,248	1,093,215	993,215	(100,000)	90.9%
Debt Service	3,455	11,175	1,891,604	1,892,779	1,175	100.1%
	<u>\$ 7,488,619</u>	<u>\$ 8,044,600</u>	<u>\$ 15,127,540</u>	<u>\$ 13,025,797</u>	<u>\$ (2,101,743)</u>	<u>86.1%</u>
Capital Expenses	\$ 2,097,501	\$ 2,005,874	\$ 13,688,467	\$ 3,025,197	\$ (10,663,270)	22.1%
Working Capital			\$ 18,611,833	\$ 20,207,868		

The Water Fund is projected to end the year with program revenues of \$17.65 million, about \$1.53 million above the budgeted amount. Total operating expenses, including transfers out and debt service are projected to end the year at \$13.03 million, or 86.1% of budget. This reduction from budgeted amounts is mostly due to savings from budgeted contracts not anticipated to be fully needed this year. Total expenditures for capital projects are estimated to be \$3.03 million at year-end. The fund will end the year with projected working capital of \$20.21 million.

Wastewater Fund

The Wastewater Fund is reported on a combined basis and includes the Wastewater and Wastewater Capital Funds.

	FY 19-20 Actual Mar. 31, 2020	FY 20-21 Actual Mar. 31, 2021	FY 20-21 Budget	FY 20-21 Projected	Over/Under Budget	% of Budget
Program Revenues	\$ 5,227,195	\$ 6,349,791	\$ 8,112,200	\$ 8,450,154	\$ 337,954	104.17%
Salaries	1,013,195	1,179,924	1,601,114	1,537,168	(63,946)	96.01%
Benefits	912,599	903,194	1,238,987	1,217,273	(21,714)	98.25%
Operating Expenses	1,104,236	797,897	2,003,322	1,111,589	(891,733)	55.49%
Transfers Out	426,923	500,894	681,156	681,156	-	100.00%
Debt Service	-	-	-	-	-	-
	<u>\$ 3,456,953</u>	<u>\$ 3,381,909</u>	<u>\$ 5,524,579</u>	<u>\$ 4,547,186</u>	<u>\$ (977,393)</u>	<u>82.31%</u>
Capital Expenses	\$ 97,174	\$ 289,064	\$ 6,558,444	\$ 529,693	\$ (6,028,751)	8.08%
Working Capital			\$ 13,751,542	\$ 17,124,817		

The Wastewater Fund is projected to end the year with program revenues of \$8.45 million, approximately \$338,000 above the budgeted amount. Total operating expenses, including transfers out, are projected to end the year at \$4.55 million, or 82.3% of budget. This reduction from budgeted amounts is mostly due to savings from budgeted contracts not anticipated to be fully needed this year. Total expenditures for capital projects are estimated to be \$530,000 at year-end. The fund will end the year with projected working capital of \$17.12 million.

Solid Waste Fund

The Solid Waste Fund is reported on a combined basis and includes the Solid Waste Operating, Solid Waste Capital, and Solid Waste Plan Area Capital.

	FY 19-20 Actual	FY 20-21 Actual	FY 20-21	FY 20-21	Over/Under	% of
	Mar. 31, 2020	Mar. 31, 2021	Budget	Projected	Budget	Budget
Program Revenues	\$ 8,597,124	\$ 11,221,276	\$ 12,997,607	\$ 14,974,080	\$ 1,976,473	115.2%
Salaries	2,275,834	2,394,723	3,281,185	3,145,501	(135,684)	95.9%
Benefits	1,769,358	1,878,396	2,609,912	2,498,909	(111,003)	95.7%
Operating Expenses	3,793,058	4,206,446	5,512,692	5,655,007	142,315	102.6%
Transfers Out	1,013,287	1,420,838	1,830,020	1,435,893	(394,127)	78.5%
Debt Service	-	-	-	-	-	0.0%
	<u>\$ 8,851,537</u>	<u>\$ 9,900,403</u>	<u>\$ 13,233,809</u>	<u>\$ 12,735,310</u>	<u>\$ (498,499)</u>	<u>96.2%</u>
Capital Expenses	\$ 72,588	\$ 1,490,261	\$ 2,500,000	\$ 2,650,000	\$ 150,000	106.0%
Working Capital			\$ 5,368,058	\$ 4,956,828		

The Solid Waste Fund is projected to end the year with program revenues of \$15.0 million, approximately \$2.0 million above the budgeted amount. Total operating expenses, including transfers out, are projected to end the year at \$12.74 million, or 96.2% of budget. Total expenditures for capital projects are estimated to be \$2.7 million at year-end. The fund will end the year with projected working capital of \$4.96 million.

Other Funds

City Housing Fund

The City Housing Fund as of March 31, 2021 had a cash balance of \$12,741,536. The City Council had previously approved a loan for the Parkway Apartment project in the amount of \$4,680,000, of which \$3.96 million has now been expended. The City Council had also previously approved loans in the amount of \$4.15 million for the Bidwell Place project and \$3.5 million for the Scholar Way project which have not been expended as of March 31, 2021.

Risk Management Internal Service Fund

The Risk Management Fund captures the activity associated with employee and retiree health, dental and vision insurance, workers' compensation, and liability insurance expense.

As of March 31, 2021, the City has paid \$5.37 million for health, vision, and dental insurance for active employees and \$3.27 million for retired employees and \$1.77 million for workers' compensation. Liability insurance payments were \$2.33 million. The total expenditures for FY 2021 are projected at \$18.81 million, which is an increase from the prior fiscal year of \$1.34 million, which is mostly seen in transfers out, health and liability costs.

The projected ending unrestricted net position is \$6.12 million, a \$2.99 million reduction from FY 2019-20 due to a planned use of fund balance in FY 2020-21.

Lighting and Landscape Funds

There are 29 Lighting and Landscape (L&L) Districts in the City of Folsom. Each District has its own budget and maintenance requirements to maintain various types of assets ranging from shrub beds, mini parks, walls, fences, monument signs, streetlights, bollards, landscape lighting, irrigation systems, artwork, a waterfall, walkways/trails, open space, trees, and electrical services.

Some activities that have taken place in the L&L's during this time period include:

District	Project	Date	Cost
Hannaford Cross	Replaced Damaged Fence Sections on Lakeside Way	March	\$16,302
Broadstone 3	Landscaping Upgrades on Broadstone Parkway	February	\$2,796
American River Canyon	Replaced Damaged Fence Sections on Apline Falls Drive	February	\$11,167
American River Canyon	Landscape Renovation on Temperance River Drive and Flat Rock Drive	February	\$3,433

Plan Area Impact Fees

Total Plan Area Impact Fees received through the 3rd Quarter of FY 2021 was \$8.95 million. In December, the City made the second payment on the Corporation Yard property that is just south of the Plan Area. The payment was \$281,331.90 for a total of \$591,812.01 in principal and interest paid to date, with the Corp Yard Impact Fees. This leaves a balance of approximately \$277,000.

APPENDIX A

City of Folsom, California
 Combined General Fund

Revenue and Expense Statement
 Quarter Ended March 31, 2021

	FY 2020	FY 2021	FY 2020	FY 2021	FY21 Forecast	VARIANCE		VARIANCE	
	As of 3/31/2020	As of 3/31/2021	ACTUAL	BUDGET	As of 3/31/2021	Forecast vs Budget	%	Actual vs Budget	%
						\$	%	\$	%
REVENUES:									
Taxes:									
Property	\$ 14,685,205	\$ 15,656,039	\$ 28,803,455	\$ 29,884,989	\$ 29,884,989	\$ -	100%	\$ (14,228,950)	52%
Sales And Use	14,266,080	14,763,739	22,616,404	22,784,063	23,500,270	716,207	103%	(8,020,324)	65%
Transient Occupancy	1,186,394	718,276	1,677,631	1,255,000	1,000,000	(255,000)	80%	(536,724)	57%
Real Property Transfer	383,925	394,926	650,410	575,000	648,620	73,620	113%	(180,074)	69%
Franchise Fees	-	2,135	765,091	826,000	758,776	(67,224)	92%	(823,865)	0%
Other	576,178	390,966	788,526	1,120,000	550,000	(570,000)	49%	(729,034)	35%
Licenses And Permits	2,819,570	2,903,737	3,287,917	2,415,416	3,753,555	1,338,139	155%	488,321	120%
Intergovernmental	4,137,179	5,468,161	7,890,586	8,150,005	9,600,517	1,450,512	118%	(2,681,844)	67%
Charges For Current Services	9,716,275	8,561,523	12,509,604	11,481,205	12,535,069	1,053,864	109%	(2,919,682)	75%
Fines And Forfeitures	102,951	89,550	169,015	196,500	167,641	(28,859)	85%	(106,950)	46%
Interest	279,575	134,038	670,231	245,000	205,000	(40,000)	84%	(110,962)	55%
Miscellaneous	608,779	881,255	771,681	562,144	1,077,405	515,261	192%	319,111	157%
Operating Transfers In	3,686,941	4,083,086	6,829,425	10,043,152	6,623,478	(3,419,674)	66%	(5,960,066)	41%
TOTAL REVENUES	52,449,052	54,047,431	87,429,976	89,538,474	90,305,320	766,846	100.9%	(35,491,043)	60%
EXPENDITURES:									
Current Operating:									
General Government	\$ 8,648,518	\$ 8,714,431	\$ 11,699,736	\$ 12,067,436	\$ 11,942,918	\$ (124,518)	99%	\$ 3,353,005	72%
Public Safety	35,120,841	34,957,199	46,555,416	44,616,863	46,105,158	1,488,295	103%	9,659,664	78%
Public Ways and Facilities	4,919,042	5,194,411	6,890,731	7,246,322	7,006,299	(240,023)	97%	2,051,911	72%
Community Services	4,947,107	4,832,403	6,968,179	5,606,333	6,782,350	1,176,017	121%	773,930	86%
Culture and Recreation	11,205,507	9,107,525	13,794,757	14,135,292	12,751,826	(1,383,466)	90%	5,027,767	64%
Non-Departmental	7,166,751	2,590,711	9,493,009	5,866,228	4,607,641	(1,258,587)	79%	3,275,517	44%
Operating Transfers Out	-	-	-	-	-	-	-	-	-
TOTAL EXPENDITURES	72,007,766	65,396,681	95,401,828	89,538,474	89,196,192	(342,282)	99.6%	24,141,793	73%
APPROPRIATION OF FUND BALANCE	(19,558,714)	(11,349,250)	(7,971,852)	-	1,109,128				
FUND BALANCE, JULY 1	23,670,012	15,697,622	23,670,012	15,698,160	15,698,160				
FUND BALANCE	4,111,298	4,348,372	15,698,160	15,698,160	16,807,288				
NONSPENDABLE FUND BALANCE	(1,066,791)	(801,458)	(903,432)	(801,458)	(801,458)				
RESTRICTED FUND BALANCE	-	-	-	-	-				
COMMITTED FUND BALANCE	-	-	-	-	-				
ASSIGNED FUND BALANCE	(587,374)	(2,321,454)	(1,018,076)	-	(1,286,552)				
UNRESTRICTED FUND BALANCE	\$ 2,457,133	\$ 1,225,460	\$ 13,776,652	\$ 14,896,702	\$ 14,719,278				

APPENDIX B

City of Folsom, California

Expenditure Summary - General Fund Departments
Quarter Ended March 31, 2021

	FY 2020	FY 2021	FY 2020 ACTUAL	FY 2021 BUDGET	FY21 Forecast	VARIANCE		VARIANCE	
	As of	As of			As of	Forecast vs Budget		Actual vs. Budget	
	3/31/2020	3/31/2021			3/31/2021	\$	%	\$	%
EXPENDITURES:									
City Council	\$ 97,869	\$ 77,415	\$ 129,452	\$ 126,140	\$ 104,080	\$ (22,060)	82.51%	\$ (48,725)	61%
City Manager	1,028,638	851,330	1,372,589	1,311,117	1,153,862	(157,255)	88.01%	(459,787)	65%
City Clerk	454,937	505,665	605,492	682,889	656,403	(26,486)	96.12%	(177,224)	74%
Office of Mgmt & Budget	3,633,265	3,808,555	4,863,298	4,864,469	5,032,645	168,176	103.46%	(1,055,914)	78%
City Attorney	727,470	717,258	1,001,723	1,056,421	979,430	(76,991)	92.71%	(339,163)	68%
Human Resources	521,886	454,342	720,681	863,321	627,918	(235,403)	72.73%	(408,979)	53%
Police	17,541,332	17,726,983	23,522,120	24,102,279	23,637,218	(465,061)	98.07%	(6,375,296)	74%
Fire	17,735,683	17,378,595	23,252,017	20,746,292	22,678,836	1,932,544	109.32%	(3,367,697)	84%
Community Development	4,947,107	4,832,403	6,968,179	5,606,333	6,782,350	1,176,017	120.98%	(773,930)	86%
Parks & Recreation	11,742,341	9,929,581	14,640,110	15,121,284	14,087,050	(1,034,234)	93.16%	(5,191,703)	66%
Library	1,491,444	1,329,431	1,942,427	1,945,379	1,842,460	(102,919)	94.71%	(615,948)	68%
Public Works	4,919,043	5,194,412	6,890,731	7,246,322	7,006,299	(240,023)	96.69%	(2,051,910)	72%
Other	-	-	-	-	-	-	-	-	-
Non Departmental	7,166,751	2,590,711	9,493,009	5,866,228	4,607,641	(1,258,587)	78.55%	(3,275,517)	44%
Operating Transfers Out	-	-	-	-	-	-	-	-	-
TOTAL EXPENDITURES:	\$ 72,007,766	\$ 65,396,681	\$ 95,401,828	\$ 89,538,474	\$ 89,196,192	\$ (342,282)	99.62%	\$ (24,141,793)	73%

APPENDIX C

**City of Folsom, California
Housing Fund**

**Revenue and Expense Statement
Quarter Ended March 31, 2021**

	FY 2021	FY 2020	FY 2021	FY21 Forecast	VARIANCE		VARIANCE	
	As of 3/31/2021	ACTUAL	BUDGET	As of 3/31/2021	Forecast vs Budget \$ %	Actual vs Budget \$ %		
REVENUES:								
Taxes	\$ -	\$ -	\$ -	\$ -	\$ -		\$ -	
Intergovernmental	2,500,000	-	-	2,500,000	2,500,000		2,500,000	
Charges for Current Services	23,161	14,020	15,000	27,000	12,000	180%	8,161	154%
Impact Fee Revenue	2,812,443	2,032,772	135,000	4,016,214	3,881,214	2975%	2,677,443	2083%
Interest Revenue	246,393	630,289	150,000	360,000	210,000	240%	96,393	164%
Other Revenue	128,212	-	72,360	128,212	55,852	177%	55,852	177%
Operating Transfers In	-	-	-	-	-		-	
TOTAL REVENUES	5,710,209	2,677,081	372,360	7,031,426	6,659,066	1888%	5,337,849	1534%
EXPENDITURES:								
Salary & Benefits	\$ -	\$ -	\$ -	\$ -	\$ -		\$ -	
Services & Supplies	-	-	-	-	-		-	
Contracts	7,337,309	112,869	260,000	12,562,428	12,302,428	4832%	(7,077,309)	2822%
Insurance	-	-	-	-	-		-	
Other Operating Expenses	177	-	-	177	177		(177)	
Capital Outlay	-	-	-	-	-		-	
Extraordinary Loss on Dissolution of RDAs	-	-	-	-	-		-	
Operating Transfers Out	107,456	13,853	112,360	112,360	-	100%	4,904	96%
TOTAL EXPENDITURES	7,444,942	126,722	372,360	12,674,965	(12,302,605)	3404%	(7,072,582)	1999%
APPROPRIATION OF FUND BALANCE	(1,734,733)	2,550,359	-	(5,643,539)				
FUND BALANCE, JULY 1	27,537,627	24,987,268	27,537,627	27,537,627				
FUND BALANCE	\$ 25,802,894	\$ 27,537,627	\$ 27,537,627	\$ 21,894,088				
NONSPENDABLE FUND BALANCE	(2,261,875)	(2,261,875)	(27,537,627)	(21,894,088)				
RESTRICTED FUND BALANCE	-	-	-	-				
COMMITTED FUND BALANCE	-	-	-	-				
ASSIGNED FUND BALANCE	-	-	-	-				
UNRESTRICTED FUND BALANCE (DEFICIT)	\$ 23,541,019	\$ 25,275,752	\$ -	\$ -				

APPENDIX D

City of Folsom, California
Lighting and Landscaping Districts

Revenue and Expenditure Statement
 Quarter Ended March 31, 2021

	Fund 204	Fund 205	Fund 207	Fund 208	Fund 209	Fund 210	Fund 212	Fund 213	Fund 214	Fund 231	Fund 232
	Los Cerros	Briggs Ranch	Natoma Station	Folsom Heights	Broadstone Unit 3	Broadstone	Hannaford Cross	Lake Natoma Shores	Cobble Hills Reflect	Sierra Estates	Natoma Valley
Revenues:											
Special Assessment	23,076	45,748	95,033	11,610	11,688	214,255	11,006	12,425	24,518	4,697	37,204
Interest	1,196	2	-	165	403	-	156	745	-	166	1,634
Other Revenue	-	-	-	-	540	540	-	-	-	-	-
Total Revenue	\$ 24,272	\$ 45,750	\$ 95,033	\$ 11,775	\$ 12,631	\$ 214,795	\$ 11,162	\$ 13,170	\$ 24,518	\$ 4,863	\$ 38,838
Expenditures:											
Communications	538	359	1,076	-	-	-	359	179	897	179	179
Utilities	9,173	13,077	50,632	6,621	899	130,618	3,268	3,115	9,770	907	2,511
Contracts	3,339	3,711	11,303	1,642	4,376	1,164	6,706	2,290	4,069	528	337
Maintenance	15,486	36,964	100,830	1,728	-	-	6,924	4,682	17,813	1,901	18,119
Supplies	-	-	-	-	-	-	-	-	-	33	-
Transfers Out	4,395	12,332	27,956	535	969	50,087	3,509	2,672	8,325	1,052	6,401
Total Expenditures	\$ 32,931	\$ 66,443	\$ 191,797	\$ 10,526	\$ 6,244	\$ 181,869	\$ 20,766	\$ 12,938	\$ 40,874	\$ 4,600	\$ 27,547

APPENDIX D

City of Folsom, California
Lighting and Landscaping Districts

Revenue and Expenditure Statement
 Quarter Ended March 31, 2021

	Fund 234	Fund 236	Fund 237	Fund 249	Fund 250	Fund 251	Fund 252	Fund 253	Fund 260	Fund 262	Fund 266	Fund 267
	Cobble Ridge	Praire Oaks Ranch	Silverbrook	Willow Creek East	Blue Ravine Oaks	Steeplechase	Willow Creek So.	Am River Canyon No.	Willow Springs	Willow Sprgs CFD #11	Broadstone 3 CFD #12	ARC No.2 CFD #13
Revenues:												
Special Assessment	7,836	109,521	-	33,627	20,036	14,356	87,929	58,738	8,099	23,848	313,815	59,980
Interest	866	-	841	-	745	682	4,954	680	359	2,722	11,407	515
Other Revenue	-	270	-	-	-	-	-	-	-	-	-	-
Total Revenue	\$ 8,702	\$ 109,791	\$ 841	\$ 33,627	\$ 20,781	\$ 15,038	\$ 92,883	\$ 59,418	\$ 8,458	\$ 26,570	\$ 325,222	\$ 60,495
Expenditures:												
Communications	-	-	-	538	359	359	2,153	-	-	538	522	183
Utilities	907	51,076	472	17,656	12,872	3,808	51,312	40,643	4,349	17,969	89,585	19,559
Contracts	708	13,312	1,297	7,943	993	1,736	8,698	19,727	1,856	-	12,042	-
Maintenance	1,829	103,057	2,077	24,036	7,262	8,916	13,073	6,901	-	57,260	182,722	34,645
Supplies	-	-	-	-	-	-	-	-	-	-	2,002	1,638
Transfers Out	1,214	49,798	1,171	1,508	878	3,619	8,479	4,100	602	12,658	57,742	17,627
Total Expenditures	\$ 4,658	\$ 217,243	\$ 5,017	\$ 51,681	\$ 22,364	\$ 18,438	\$ 83,715	\$ 71,371	\$ 6,807	\$ 88,425	\$ 344,615	\$ 73,652

APPENDIX D

City of Folsom, California
Lighting and Landscaping Districts

Revenue and Expenditure Statement
 Quarter Ended March 31, 2021

	Fund 270	Fund 271	Fund 275	Fund 278	Fund 281	Fund 282	Fund 283	Fund 284	Fund 285	Fund 288	Fund 289	
	ARC No. 2	Residences At ARC	ARC North #3	Blue Ravine Oaks No. 2	Folsom Hts #2	Broadstone #4	Islands CFD #16	Willow Creek Estates #2	Prospect Ridge	Maint Dist CFD #18	Maint Dist CFD #19	TOTAL
Revenues:												
Special Assessment	6,764	11,234	139,257	19,545	33,409	50,668	-	40,632	15,956	-	-	1,546,510
Interest	1,301	513	7,421	1,477	1,943	2,327	2,327	1,008	72	2,950	2,387	51,964
Other Revenue	-	-	-	-	-	-	-	-	-	-	-	1,350
Total Revenue	\$ 8,065	\$ 11,747	\$ 146,678	\$ 21,022	\$ 35,352	\$ 52,995	\$ 2,327	\$ 41,640	\$ 16,028	\$ 2,950	\$ 2,387	\$ 1,599,824
Expenditures:												
Communications	-	359	1,256	-	-	-	-	-	-	-	-	10,033
Utilities	161	3,059	152	-	-	-	7,383	-	2,095	39,198	8,252	601,099
Contracts	3,293	1,942	14,921	16,227	-	49,703	1,800	38,174	330	-	2,997	237,164
Maintenance	-	7,347	43,964	6,191	8,339	125,548	35,403	-	5,140	62,599	7,135	947,891
Supplies	-	-	-	-	-	-	-	-	-	-	-	3,673
Transfers Out	320	3,429	15,213	3,668	4,586	8,236	11,780	12,663	4,037	840	376	342,777
Total Expenditures	\$ 3,774	\$ 16,136	\$ 75,506	\$ 26,086	\$ 12,925	\$ 183,487	\$ 56,366	\$ 50,837	\$ 11,602	\$ 102,637	\$ 18,760	\$ 2,142,637

APPENDIX E

City of Folsom, California
 Combined Water Funds*
 Revenue and Expense Statement
 Quarter Ended March 31, 2021

	FY 2020 As of 3/31/2020	FY 2021 As of 3/31/2021	FY 2020 ACTUAL	FY 2021 BUDGET	FY21 Forecast As of 3/31/2021	VARIANCE		VARIANCE	
						Forecast vs Budget \$	%	Actual vs Budget \$	%
OPERATING REVENUES:									
Charges For Services	10,250,731	12,105,211	15,748,401	16,116,000	17,647,029	1,531,029	110%	(4,010,789)	75%
TOTAL OPERATING REVENUES	10,250,731	12,105,211	15,748,401	16,116,000	17,647,029	1,531,029	110%	(4,010,789)	75%
OPERATING EXPENSES:									
Employee Services	3,719,135	3,864,614	4,999,810	5,455,031	5,235,993	(219,038)	96%	(1,590,417)	71%
Utilities	474,845	501,383	795,571	769,500	727,670	(41,830)	95%	(268,117)	65%
Supplies	503,426	729,614	680,592	1,239,093	985,075	(254,018)	79%	(509,479)	59%
Maintenance and Operation	279,779	267,059	417,517	1,024,970	359,269	(665,701)	35%	(757,911)	26%
Contractual Services	1,316,132	1,322,768	1,844,620	2,688,642	1,810,581	(878,061)	67%	(1,365,874)	49%
Depreciation	3,236,291	2,969,510	4,293,283	-	4,293,283	4,293,283		2,969,510	
Other Operating Expenses	574,885	679,739	1,826,253	965,485	1,021,215	55,730	106%	(285,746)	70%
TOTAL OPERATING EXPENSES	10,104,493	10,334,687	14,857,646	12,142,721	14,433,086	2,290,365	119%	(1,808,034)	85%
OPERATING INCOME	146,237	1,770,524	890,755	3,973,279	3,213,943		81%	230,194	
NONOPERATING REVENUE (EXPENSES):									
Impact Fees	607,256	228,769	704,346	671,970	280,000	(391,970)	42%	(443,201)	34%
Other	11,246	13,364	5,577,274	11,502,204	15,000	(11,487,204)	0%	(11,488,839)	0%
Investment Income	353,261	173,647	613,444	272,000	277,432	5,432	102%	(98,353)	64%
Intergovernmental	8,285	-	10,259	-	-	-		-	
Proceeds of Financing	-	-	-	-	-	-		-	
Debt Service Expense	(3,455)	(11,175)	(774,149)	(1,891,604)	(1,892,779)	(1,175)	100%	1,880,429	1%
Other Reimbursements	-	-	-	-	-	-		-	
Capital Outlay - Projects	(2,097,501)	(2,005,874)	(3,564)	(13,688,467)	(3,025,197)	10,663,270	22%	11,682,592	15%
TOTAL NONOPERATING REVENUE (EXPENSE)	(1,120,907)	(1,601,269)	6,127,610	(3,133,897)	(4,345,544)	(1,211,647)	139%	1,532,628	51%
INCOME (LOSS) BEFORE CAPITAL CONTRIBUTIONS AND TRANSFERS	(974,670)	169,255	7,018,365	839,382	(1,131,601)				
CAPITAL CONTRIBUTIONS AND TRANSFERS:									
Transfers In	20,863	53,833	141,726	253,833	241,726	(12,107)	95%	(200,000)	21%
Transfers Out	(616,963)	(668,248)	(929,126)	(1,093,215)	(993,215)	(1,223,754)	91%	424,967	61%
TOTAL CAPITAL CONTRIBUTIONS AND TRANSFERS	(596,100)	(614,415)	(787,400)	(839,382)	(751,489)				
CHANGE IN NET ASSETS	(1,570,770)	(445,159)	6,230,965	-	(1,883,090)				
NET ASSETS, JULY 1	96,133,601	102,364,564	96,133,599	102,364,564	102,364,564				
NET ASSETS	94,562,831	101,919,405	102,364,564	102,364,564	100,481,474				
RESTRICTED NET ASSETS	(2,541,148)	(3,643,206)	-	(3,643,206)	(3,643,206)				
UNRESTRICTED NET ASSETS	\$ 92,021,683	\$ 98,276,199	\$ 102,364,564	\$ 98,721,358	\$ 96,838,268				

* Includes the following funds: Water Impact Fee, Water Operating, Water Capital and Water Meters
 Prior year includes prior period adjustment for GASB 68

APPENDIX F

**City of Folsom, California
Combined Wastewater Funds*
Revenue and Expense Statement
Quarter Ended March 31, 2021**

	FY 2020	FY 2021	FY 2020	FY 2021	FY21 Forecast	VARIANCE		VARIANCE	
	As of	As of	ACTUAL	BUDGET	As of	Forecast vs Budget		Actual vs Budget	
	3/31/2020	3/31/2021			3/31/2021	\$	%	\$	%
OPERATING REVENUES:									
Charges For Services	5,176,795	6,299,391	7,389,618	8,045,000	8,382,954	337,954	104%	(1,745,609)	78%
Prison Services	50,400	50,400	67,200	67,200	67,200	-	100%	(16,800)	75%
TOTAL OPERATING REVENUES	5,227,195	6,349,791	7,456,818	8,112,200	8,450,154	337,954	104%	(1,762,409)	78%
OPERATING EXPENSES:									
Employee Services	1,925,794	2,083,118	2,608,483	2,840,101	2,754,441	(85,660)	97%	(756,983)	73%
Utilities	53,828	60,339	88,564	75,000	82,963	7,963	111%	(14,661)	80%
Supplies	329,481	176,060	376,673	530,014	249,768	(280,246)	47%	(353,954)	33%
Maintenance and Operation	124,721	128,728	183,776	328,500	157,323	(171,177)	48%	(199,772)	39%
Contractual Services	324,017	165,654	344,481	768,200	273,122	(495,078)	36%	(602,546)	22%
Depreciation	1,515,085	1,477,947	2,035,576	-	2,035,576	2,035,576		1,477,947	
Other Operating Expenses	272,190	267,116	738,088	301,608	348,413	46,805	116%	(34,492)	89%
TOTAL OPERATING EXPENSES	4,545,115	4,358,962	6,375,641	4,843,423	5,901,606	1,058,183	122%	(484,461)	90%
OPERATING INCOME (LOSS)	682,080	1,990,829	1,081,177	3,268,777	2,548,548		78%		
NONOPERATING REVENUE (EXPENSES):									
Impact Fees	56,832	31,642	56,832	206,215	40,000	(166,215)	19%	(186,153)	15.3%
Investment Income	224,750	124,976	427,412	135,000	142,100	7,100	105%	(10,024)	93%
Other	11,837	9,095	4,707,423	3,599,475	10,000	(3,589,475)	0%	(3,590,380)	0%
Debt Service	-	-	-	-	-	-		-	
Capital Outlay - Projects	(97,174)	(289,064)	(341,540)	(6,558,444)	(529,693)	6,028,751	8%	6,269,380	4%
TOTAL NONOPERATING REVENUE (EXPENSE)	196,245	(123,351)	4,850,127	(2,617,754)	(337,593)	2,280,161	13%	2,494,403	5%
INCOME (LOSS) BEFORE CAPITAL CONTRIBUTIONS AND TRANSFERS	878,325	1,867,478	5,931,304	651,023	2,210,955				
CAPITAL CONTRIBUTIONS AND TRANSFERS:									
Transfers In	11,942	30,133	23,884	30,133	30,133	-	0%	-	
Transfers Out	(426,923)	(500,894)	(584,539)	(681,156)	(681,156)	-	0%	(180,263)	278%
TOTAL CAPITAL CONTRIBUTIONS AND TRANSFERS	(414,981)	(470,761)	(560,655)	(651,023)	(651,023)				
CHANGE IN NET ASSETS	463,344	1,396,717	5,370,649	-	1,559,932				
NET ASSETS, JULY 1	59,638,604	65,009,254	59,638,605	65,009,254	65,009,254				
NET ASSETS	60,101,948	66,405,971	65,009,254	65,009,254	66,569,186				
RESTRICTED NET ASSETS	(1,045,135)	(2,437,465)	-	(2,437,465)	(2,437,465)				
UNRESTRICTED NET ASSETS	\$ 59,056,813	\$ 63,968,506	\$ 65,009,254	\$ 62,571,789	\$ 64,131,721				

* Includes the following funds: Sewer Operating and Sewer Capital
Prior year includes prior period adjustment for GASB 68

APPENDIX G

**City of Folsom, California
Combined Solid Waste Funds*
Revenue and Expense Statement
Quarter Ended March 31, 2021**

	FY 2020	FY 2021	FY 2020	FY 2021	FY21 Forecast	VARIANCE		VARIANCE	
	As of 3/31/2020	As of 3/31/2021	ACTUAL	BUDGET	As of 3/31/2021	Forecast vs Budget \$ %	Actual vs Budget \$ %		
OPERATING REVENUES:									
Charges For Services	8,597,124	11,221,276	11,774,723	12,997,607	14,974,080	1,976,473	115%	(1,776,331)	86%
TOTAL OPERATING REVENUES	8,597,124	11,221,276	11,774,723	12,997,607	14,974,080	1,976,473	115%	(1,776,331)	86%
OPERATING EXPENSES:									
Employee Services	4,045,192	4,273,119	5,303,256	5,891,097	5,644,410	(246,687)	96%	(1,617,978)	73%
Utilities	16,108	27,332	26,996	20,500	37,137	16,637	181%	6,832	133%
Supplies	708,823	559,189	889,668	1,180,598	893,186	(287,412)	76%	(621,409)	47%
Maintenance and Operation	629,026	808,496	861,589	538,133	1,042,221	504,088	194%	270,363	150%
Contractual Services	1,995,907	2,456,670	3,019,626	3,236,087	3,185,582	(50,505)	98%	(779,417)	76%
Depreciation	442,880	528,131	570,252	-	570,252	570,252		528,131	
Other Operating Expenses	443,195	354,758	1,321,179	537,374	496,881	(40,493)	92%	(182,616)	66%
TOTAL OPERATING EXPENSES	8,281,130	9,007,696	11,992,566	11,403,789	11,869,669	465,880	104%	(2,396,093)	79%
OPERATING INCOME (LOSS)	315,994	2,213,580	(217,843)	1,593,818	3,104,411				
NONOPERATING REVENUE (EXPENSE):								726,565	
Impact Fees	180,414	271,335	211,585	434,045	420,541	(13,504)	96.9%	(162,710)	63%
Investment Income	110,293	43,617	188,737	50,500	74,200	23,700	147%	(6,883)	86%
Intergovernmental Revenues	33,156	498	159,159	40,499	1,000	(39,499)	2%	(40,001)	1%
Other	126,819	175,565	185,791	1,933,383	240,713	(1,692,670)	12%	(1,757,818)	9%
Debt Service-Expense	-	-	-	-	-	-		-	
Capital Outlay	(72,588)	(1,490,261)	(2,424)	(2,500,000)	(2,650,000)	(150,000)	106%	1,009,739	60%
TOTAL NONOPERATING REVENUE (EXPENSE)	378,094	(999,246)	742,848	(41,573)	(1,913,546)	(1,871,973)	4603%	(957,673)	2404%
INCOME (LOSS) BEFORE CAPITAL CONTRIBUTIONS AND TRANSFERS	694,088	1,214,334	525,005	1,552,245	1,190,865				
CAPITAL CONTRIBUTIONS AND TRANSFERS:									
Transfers In	30,159	80,762	60,178	438,320	94,193	(344,127)	-365%	(357,558)	-23%
Transfers Out	(1,013,287)	(1,076,711)	(1,370,455)	(1,830,020)	(1,435,893)	394,127	-27%	753,309	-143%
TOTAL CAPITAL CONTRIBUTIONS AND TRANSFERS	(983,128)	(995,949)	(1,310,277)	(1,391,700)	(1,341,700)				
CHANGE IN NET ASSETS	(289,040)	218,385	(785,272)	160,545	(150,835)				
NET ASSETS, JULY 1	(4,864,125)	(5,649,397)	(4,864,125)	(5,939,762)	(5,939,762)				
NET ASSETS RESTRICTED NET ASSETS	(5,153,165)	(5,431,012)	(5,649,397)	(5,779,217)	(6,090,597)				
	(59,558)	(893,289)	-	(893,289)	(893,289)				
UNRESTRICTED NET ASSETS	\$ (5,212,723)	\$ (6,324,301)	\$ (5,649,397)	\$ (6,672,506)	\$ (6,983,886)				

* Includes the following funds: Solid Waste Operating, Solid Waste Capital, and Solid Waste Plan Area Capital
Prior year includes prior period adjustment for GASB 68

APPENDIX H

City of Folsom, California
Risk Management
 Revenue and Expense Statement
 Quarter Ended March 31, 2021

	FY 2020 As of 3/31/2020	FY 2021 As of 3/31/2021	FY 2020 ACTUAL	FY 2021 BUDGET	FY 2021 Forecast As of 3/31/2020
OPERATING REVENUES:					
Charges for services	11,694,238	10,563,644	16,414,501	14,084,858	14,084,858
Penalty Fines	-	-	-	-	-
Interest revenue	106,538	37,107	227,373	65,000	65,000
Reimbursement	1,114,038	1,397,751	1,575,935	2,044,985	1,676,985
Other revenue	15,715	328	1,068	3,692,925	-
Total operating revenues	12,930,528	11,998,830	18,218,877	19,887,768	15,826,843
OPERATING EXPENSES:					
Employee Services					
Active Employees:					
Wages	103,411	111,859	146,666	157,310	157,840
FICA	8,057	8,612	11,352	12,558	12,048
PERS	42,560	47,415	60,361	67,275	67,077
Deferred Compensation	2,585	2,796	3,666	3,933	3,933
Workers Compensation	1,679,978	1,769,357	2,054,908	2,320,236	2,360,236
Liability	1,785,753	2,331,720	1,791,980	2,331,720	2,331,720
Health Insurance	4,667,624	4,927,880	6,572,915	7,453,087	6,750,000
Vision	78,655	65,030	110,762	98,354	98,354
Dental	415,911	375,654	585,683	568,151	596,499
Employee Assistance Program	8,564	8,502	9,850	10,863	11,363
Health Retirement Account	108,777	47,443	154,325	166,650	74,618
Health Savings Account	17,838	12,200	22,474	13,265	16,265
Contracts	974,999	831,421	1,017,430	1,010,780	890,000
Small Equipment	-	-	-	-	-
Transfers Out	145,263	707,000	290,526	707,000	707,000
Retirees:					
PERS	11,314	11,532	11,314	15,000	11,532
Health Insurance	2,958,870	2,935,074	4,191,516	4,311,278	4,200,000
Vision	49,893	45,897	61,778	62,280	62,280
Dental	278,589	282,767	373,418	372,000	372,000
Retiree HRA	10,000	64,132	-	-	92,032
Total operating expenses	13,348,642	14,586,292	17,470,924	19,681,740	18,814,797
CHANGE IN NET ASSETS	(418,113)	(2,587,462)	747,953	206,028	(2,987,954)
NET ASSETS, BEGINNING OF YEAR	12,335,083	13,083,036	12,335,083	13,083,036	13,083,036
RESTRICTED FOR INSURANCE DEPOSIT	3,726,923	3,978,049	3,978,049	3,978,049	3,978,049
UNRESTRICTED NET ASSETS	8,190,048	6,517,537	9,104,987	9,311,015	6,117,033
NET ASSETS	11,916,970	10,495,574	13,083,036	13,289,064	10,095,082

APPENDIX I

City of Folsom, California Compensated Leave Revenue and Expense Statement Quarter Ended March 31, 2021

	FY 2020	FY 2021	FY 2020	FY 2021	FY 2021 Forecast	VARIANCE		VARIANCE	
	As of 3/31/2020	As of 3/31/2021	ACTUAL	BUDGET	As of 3/31/2021	Forecast vs Budget	%	Actual vs BUDGET	%
						\$	%	\$	%
REVENUES:									
Reimbursements	407,413	-	543,217	-	-	-		-	
Interest	13,646	3,406	14,574	15,000	9,000	(6,000)	60%	(11,594)	23%
Other revenue	-	-	-	560,000	-	(560,000)	0%	(560,000)	0%
Operating Transfers In	-	-	-	-	-	-		-	
TOTAL REVENUES	421,059	3,406	557,791	575,000	9,000	(566,000)	2%	(571,594)	1%
EXPENDITURES:									
Annual Leave Wages	426,111	451,925	580,916	575,000	575,000	-	100%	(123,075)	79%
Annual Leave Benefits	-	-	-	-	-	-		-	
Operating Transfers Out	139,800	-	279,601	-	-	-		-	
Services & Supplies	-	-	-	-	-	-		-	
TOTAL EXPENDITURES	565,911	451,925	860,517	575,000	575,000	-	100%	123,075	79%
APPROPRIATION OF FUND BALANCE	(144,852)	(448,519)	(302,726)	-	(566,000)				
FUND BALANCE, JULY 1	927,736	625,010	927,736	625,010	625,010				
FUND BALANCE	782,884	176,491	625,010	625,010	59,010				
RESTRICTED FUND BALANCE	-	-	-	-	-				
UNRESTRICTED FUND BALANCE	\$ 782,884	\$ 176,491	\$ 625,010	\$ 625,010	\$ 59,010				

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City Council Special Meeting

MINUTES

Tuesday, May 25, 2021 6:15 PM

Pursuant to Governor Newsom’s Executive Order N-29-20, members of the Folsom City Council and staff may participate in this meeting via teleconference.

CALL TO ORDER

The special City Council meeting was called to order at 6:15 p.m. in City Council Chambers, 50 Natoma Street, Folsom, California, with Vice Mayor Sarah Aquino presiding.

ROLL CALL:

Councilmembers Present: Kerri Howell, Councilmember
Rosario Rodriguez, Councilmember
Sarah Aquino, Vice Mayor
YK Chalamcherla, Councilmember

Councilmembers Absent: Mike Kozlowski, Mayor (arrived during closed session)

Participating Staff: City Manager Elaine Andersen
City Attorney Steve Wang
City Clerk Christa Freemantle

ADJOURNMENT TO CLOSED SESSION FOR THE FOLLOWING PURPOSES:

1. Conference with Real Property Negotiator - Pursuant to Government Code section 54956.8: Depot Building at 200 Wool Street. Negotiating Parties: City Manager Elaine Andersen on behalf of the City of Folsom, and Joseph Gagliardi on behalf of the Folsom Chamber of Commerce. Under Negotiation: Price and Terms of Lease

Motion by Councilmember Kerri Howell, second by Councilmember Rosario Rodriguez to adjourn to Closed Session for the above referenced item. Motion carried with the following roll call vote:

AYES: Councilmember(s): Howell, Rodriguez, Aquino, Chalamcherla
NOES: Councilmember(s): None
ABSENT: Councilmember(s): Kozlowski
ABSTAIN: Councilmember(s): None

RECONVENE

City Attorney Steve Wang announced that no final action was taken during Closed Session.

ADJOURNMENT

There being no further business to come before the Folsom City Council, the meeting was adjourned at 6:30 p.m.

SUBMITTED BY:

Christa Freemantle, City Clerk

ATTEST:

Mike Kozlowski, Mayor

City Council Regular Meeting

MINUTES

Tuesday, May 25, 2021 6:30 PM

Pursuant to Governor Newsom’s Executive Order N-29-20, members of the Folsom City Council and staff may participate in this meeting via teleconference.

CALL TO ORDER

The regular City Council meeting was called to order at 6:30 p.m. in City Council Chambers, 50 Natoma Street, Folsom, California, with Mayor Mike Kozlowski presiding.

ROLL CALL:

Councilmembers Present: Kerri Howell, Councilmember
Rosario Rodriguez, Councilmember
Sarah Aquino, Vice Mayor
YK Chalamcherla, Councilmember
Mike Kozlowski, Mayor

Councilmembers Absent: None

Participating Staff: City Manager Elaine Andersen
City Attorney Steve Wang
City Clerk Christa Freemantle
Environmental and Water Resources Director Marcus Yasutake
Finance Director/CFO Stacey Tamagni
Police Chief Rick Hillman

PLEDGE OF ALLEGIANCE

The Pledge of Allegiance was recited.

AGENDA UPDATE

City Clerk Christa Freemantle announced that Item 7 would be continued to a future meeting.

BUSINESS FROM THE FLOOR:

The following speakers addressed the City Council:

1. Jay Coffero regarding homelessness
2. Jeanne Shuman regarding homelessness
3. Jason Davis regarding homelessness

City Clerk Christa Freemantle read written comments from Rennie James regarding maintenance of Briggs Ranch Park.

SCHEDULED PRESENTATIONS

1. Presentation of the City's Compliance with Water Quality Protection Standards and Groundwater Monitoring at the Corporation Yard

Environmental and Water Resources Director Marcus Yasutake made a presentation and responded to questions from the City Council.

2. Presentation on the Requirements of the City's Water Shortage Contingency Plan

Environmental and Water Resources Director Marcus Yasutake made a presentation and responded to questions from the City Council.

CONSENT CALENDAR:

3. Approval of April 27, 2021 Regular Meeting Minutes
4. Approval of May 11, 2021 Special and Regular Meeting Minutes
5. Ordinance No. 1313 – An Ordinance of the City of Folsom Adding Section 9.36.220 to the Folsom Municipal Code Pertaining to Host Liability for Fireworks Ordinance Violation (Second Reading and Adoption)
6. Resolution No. 10627 - A Resolution Authorizing the City Manager to Execute a Construction Agreement with VSS International, Inc. for the Pavement Resurfacing Fiscal Year 2020-21 Project No. 8017, Acceptance of Grant and Appropriation of Funds
7. Resolution No. 10628 – A Resolution Adopting the Local Road Safety Plan (**Continued to a future meeting**)
8. Resolution No. 10629 - A Resolution Authorizing the City Manager to Receive and Execute an Agreement for SB-1 Local Partnership Program Funds for the Iron Point Median Project, Project No. PW2102
9. **pulled for discussion**
10. Resolution No. 10631 - A Resolution Adopting a List of Projects for Fiscal Year 2021-22 to be Funded by Senate Bill 1: The Road Repair and Accountability Act
11. Resolution No. 10632 - A Resolution Authorizing the Public Works Department to Install All-Way Stop Sign Control at the Intersection of Willard Drive and Chan Court
12. Resolution No. 10633 - A Resolution Authorizing the Public Works Department to Install All-Way Stop Sign Control at the Intersection of Levy Road and Hunter Place/Sands Way

13. pulled for discussion

Motion by Councilmember Kerri Howell, second by Councilmember Rosario Rodriguez to Approve Items 3 – 6, 8 and 10 – 12 of the Consent Calendar.

Motion carried with the following roll call vote:

AYES: Councilmember(s): Howell, Rodriguez, Aquino, Chalamcherla, Kozlowski
NOES: Councilmember(s): None
ABSENT: Councilmember(s): None
ABSTAIN: Councilmember(s): None

CONSENT CALENDAR ITEM PULLED FOR DISCUSSION:

9. Resolution No. 10630 - A Resolution of the City Council of the City of Folsom Approving a Grant in the Amount of \$150,693.75 to Bidwell Place, LP for Construction of the 75-unit Bidwell Place Affordable Multifamily Project, and Appropriation of Funds

Councilmember Kerri Howell explained that she pulled Item 9 to recuse herself because she leases a building that may be within 1,500 feet of the project location.

Motion by Vice Mayor Sarah Aquino, second by Councilmember Rosario Rodriguez to Approve Resolution No. 10630.

Motion carried with the following roll call vote:

AYES: Councilmember(s): Rodriguez, Aquino, Chalamcherla, Kozlowski
NOES: Councilmember(s): None
ABSENT: Councilmember(s): Howell (recused)
ABSTAIN: Councilmember(s): None

13. Resolution No. 10634– A Resolution Authorizing the City Manager to Execute a Subdivision Improvement Agreement and Accept Offers of Dedication for the Mangini Ranch Phase 1 Village No. 3 Subdivision, and Approval of the Final Map for the Mangini Ranch Phase 1 Village No. 3 Subdivision

Councilmember YK Chalamcherla explained that he pulled Item 13 for clarification and possible amendment to the Subdivision Improvement Agreement language. Following advice from City Attorney Steve Wang, it was determined that amendments were not necessary.

Motion by Councilmember Kerri Howell, second by Councilmember YK Chalamcherla to approve Resolution No. 10634.

Motion carried with the following roll call vote:

AYES: Councilmember(s): Howell, Rodriguez, Aquino, Chalamcherla, Kozlowski
NOES: Councilmember(s): None
ABSENT: Councilmember(s): None
ABSTAIN: Councilmember(s): None

OLD BUSINESS:

14. Resolution No. 10623 - A Resolution Adopting the City Manager's Fiscal Year 2021-22 Operating and Capital Budgets for the City of Folsom, the Successor Agency, the Folsom Public Financing Authority and the Folsom Ranch Public Financing Authority

City Manager Elaine Andersen made a presentation and responded to questions from the City Council.

The following speakers addressed the City Council:

1. Heather Riley regarding homelessness resources and funding
2. Beth Broteau regarding homelessness resources and funding
3. Vijay Jonnalagadda regarding homelessness resources and funding
4. Judi Alexander regarding homelessness resources and funding

City Clerk Christa Freemantle read written comments from Elena Hanrahan regarding homelessness resources and funding.

Ms. Andersen and Finance Director/CFO Stacey Tamagni provided clarification in response to questions from the City Council.

Motion by Councilmember YK Chalamcherla to use \$45,000 from the contingency fund to hire three student assistants to organize volunteers (two for the Parks and Recreation Department and one for the Communications division of the City Manager's Department).

Mayor Mike Kozlowski inquired if there was a second to Councilmember YK Chalamcherla's motion. Hearing no second, the motion died.

Motion by Councilmember Kerri Howell, second by Vice Mayor Sarah Aquino to approve Resolution No. 10623.

Motion carried with the following roll call vote:

AYES: Councilmember(s): Howell, Rodriguez, Aquino, Chalamcherla, Kozlowski
NOES: Councilmember(s): None
ABSENT: Councilmember(s): None
ABSTAIN: Councilmember(s): None

Mayor Mike Kozlowski called for a five-minute break at 8:23 pm. The meeting reconvened at 8:31 pm.

15. Workshop Discussion Regarding City Council Governance Manual (Part 2 of 2) and Direction to Staff

City Clerk Christa Freemantle made a presentation and responded to questions from the City Council. The City Council suggested clarifying language in certain sections of the manual.

The City Council directed that the Governance Manual be updated as suggested and brought back for final approval at an upcoming meeting.

NEW BUSINESS:

16. Resolution No. 10619 – A Resolution Approving the City of Folsom Legislative Platform and Advocacy Manual

City Clerk Christa Freemantle made a presentation and responded to questions from the City Council. The City Council suggested clarifications in certain sections of the document.

Motion by Councilmember Kerri Howell, second by Councilmember Rosario Rodriguez to continue Resolution No. 10619 to an upcoming meeting for approval under the Consent Calendar.

Motion carried with the following roll call vote:

- AYES: Councilmember(s): Howell, Rodriguez, Aquino, Chalamcherla, Kozlowski**
- NOES: Councilmember(s): None**
- ABSENT: Councilmember(s): None**
- ABSTAIN: Councilmember(s): None**

17. Governor Gavin Newsom’s Drought State of Emergency and Direction to Staff

Environmental and Water Resources Director Marcus Yasutake made a presentation and responded to questions from the City Council. City Manager Elaine Andersen provided additional clarification.

The City Council’s consensus was to start an educational program to encourage a 10% reduction in water use.

CITY MANAGER REPORTS:

City Manager Elaine Andersen explained that PG&E will be performing gas line vegetation work on the Johnny Cash Trail. She announced that the City is offering a pandemic relief waiver for one year for business license fees and announced the launch of a new user-friendly online payment system for utilities customers.

COUNCIL COMMENTS:

Councilmember Rosario Rodriguez spoke of her visits to the Human Resources, City Attorney, Finance and Library departments. She mentioned an upcoming Future Folsom meeting. Ms. Rodriguez also reminded all that the Rodeo and Hometown Parade are happening soon.

Councilmember Kerri Howell spoke of attending the Regional Transit, Regional Sanitation District and SASP board meetings and the upcoming Regional Sanitation District and the SE Connector JPA meetings. She encouraged everyone to drive safely.

Vice Mayor Sarah Aquino spoke of the Folsom Cordova School Board’s discussion of having School Resource Officers on school campuses.

Councilmember YK Chalamcherla asked if the Local Road Safety Plan could be discussed at a future Council meeting. City Manager Elaine Andersen responded that it would be coming back to City Council as a presentation. Mr. Chalamcherla congratulated graduates from both of Folsom's high schools. He spoke of the See Click Fix program and his visit to the Police Department.

Mayor Mike Kozlowski spoke of recent meetings at SACOG and STA. He congratulated the graduates at both high schools. Mr. Kozlowski discussed some of the enjoyable activities he has been able to attend recently and congratulated Powerhouse Ministries on their new facility. He mentioned attending the All Creatures ribbon cutting ceremony, an Eagle Scout Court of Honor Ceremony for Carleton Liden and participating as a judge for the Foothill Wine Festival. He concluded by congratulating Handley's Western Wear on their 60th Anniversary of business in Folsom.

ADJOURNMENT

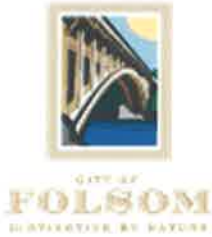
There being no further business to come before the Folsom City Council, Mayor Mike Kozlowski adjourned the meeting at 9:58 p.m.

SUBMITTED BY:

Christa Freemantle, City Clerk

ATTEST:

Mike Kozlowski, Mayor



Folsom City Council Staff Report

MEETING DATE:	6/8/2021
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10635 – A Resolution Authorizing the City Manager to Execute an Agreement with Folsom Lake Ford for the Purchase of Six Vehicles for the Environmental and Water Resources Department
FROM:	Environmental and Water Resources Department

RECOMMENDATION / CITY COUNCIL ACTION

The Environmental and Water Resources Department recommends that the City Council pass and adopt Resolution No. 10635 – A Resolution Authorizing the City Manager to Execute an Agreement with Folsom Lake Ford for the Purchase of Six Vehicles for the Environmental and Water Resources Department.

BACKGROUND / ISSUE

The Environmental and Water Resources Department (EWR) recognizes the need to maintain a fleet of vehicles that will be able to provide reliable and responsive service. EWR currently owns several vehicles that have high mileage, are nearing the end of their service life and need to be replaced.

A review of EWR Department vehicles identified six vehicles as highest priority for replacement. This resolution will authorize the City Manager to execute an agreement with Folsom Lake Ford for the purchase of replacements for these six vehicles for a total amount of \$342,277.59.

POLICY / RULE

In accordance with Chapter 2.36 of the Folsom Municipal Code; supplies, equipment, services, and construction with a value of \$62,657 or greater shall be approved by City Council.

ANALYSIS

EWR staff coordinated with the City of Folsom Fleet Manager to review current department vehicles, to help identify priority vehicles for replacement based on service life, mileage and purpose. This review identified six vehicles nearing the end of their service life, along with specifications for replacement vehicles.

The City requested quotes for the six vehicles from three separate vendors: Folsom Lake Ford, National Auto Fleet Group (providing vehicles under the Sourcewell (formerly known as NJPA) Contract 120716-NAF), and Downtown Ford (providing vehicles under State of California contracts). As shown in the summary below, Folsom Lake Ford provided the lowest total quote for the six vehicles.

Divison	Vehicle # to be replaced	New vehicle model	Quotes		
			National Auto Fleet Group (Sourcewell)	Downtown Ford (State Contract)	Folsom Lake Ford
Utility Maintenance - WA	711	F250	\$36,732.41	\$35,610.66	\$33,396.17
Water Treatment Plant	640	F350	\$79,865.48	\$77,108.01	\$78,083.59
Water Quality	820	F250	\$65,912.73	\$69,573.46	\$64,448.80
Conservation	504	Escape Hybrid	\$29,448.81	\$27,098.41	\$27,258.85
Wastewater	470	F250	\$65,912.73	\$69,573.46	\$64,448.80
Utility Maintenance - WW	735	F550	\$78,773.81	\$76,800.92	\$74,641.38
Totals			\$356,645.97	\$355,764.92	\$342,277.59

Folsom Lake Ford total, split by Fund	WA total	\$203,187.41
	WW total	\$139,090.18

The Environmental and Water Resources Department recommends that the City Council authorize the City Manager to execute an agreement with Folsom Lake Ford for the purchase of six vehicles for a total amount of \$342,277.59.

FINANCIAL IMPACT

Sufficient funds are budgeted and available in FY 2020-21 in the Water Operating Fund (Fund 520) and Sewer Operating Fund (Fund 530) for the requested vehicles. The cost of vehicles will be divided between funds, applying \$203,187.41 to Fund 520 and \$139,90.18 to Fund 530, for a total purchase price of \$342,277.59.

ENVIRONMENTAL REVIEW

This action is exempt from environmental review under the California Environmental Quality Act (CEQA).

ATTACHMENT

Resolution No. 10635 – A Resolution Authorizing the City Manager to Execute an Agreement with Folsom Lake Ford for the Purchase of Six Vehicles for the Environmental and Water Resources Department

Submitted,

Marcus Yasutake, Director
ENVIRONMENTAL AND WATER RESOURCES DEPARTMENT

RESOLUTION NO. 10635

A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE AN AGREEMENT WITH FOLSOM LAKE FORD FOR THE PURCHASE OF SIX VEHICLES FOR THE ENVIRONMENTAL AND WATER RESOURCES DEPARTMENT

WHEREAS, staff has identified the need to replace six vehicles in order to provide reliable and responsive service for the Environmental & Water Resources Department; and

WHEREAS, quotes were obtained from Folsom based vendors and others offering negotiated pricing; and

WHEREAS, the total quote for six vehicles provided by Folsom Lake Ford was the lowest; and

WHEREAS, sufficient funds are budgeted and available in the Water Operating Fund (Fund 520) for the purchase in the amount of \$203,187 and in the Wastewater Operating Fund (Fund 530) for the purchase in the amount of \$139,090; and

WHEREAS, the agreement will be in a form acceptable to the City Attorney:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom authorizes the City Manager to execute an Agreement with Folsom Lake Ford for the Purchase of Six Vehicles for the Environmental and Water Resources Department.

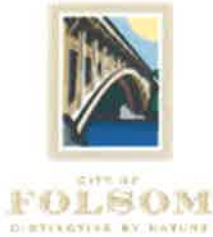
PASSED AND ADOPTED this 8th day of June 2021, by the following roll-call vote:

- AYES:** Councilmember(s):
- NOES:** Councilmember(s):
- ABSENT:** Councilmember(s):
- ABSTAIN:** Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK



Folsom City Council Staff Report



MEETING DATE:	6/8/2021
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10636 – A Resolution of the City Council Approving Amendment No. 2 to Lease Agreement with the Folsom Chamber of Commerce for the Depot Building Located at 200 Wool Street
FROM:	City Manager's Office

RECOMMENDATION / CITY COUNCIL ACTION

Staff respectfully recommends that the City Council move to adopt Resolution No. 10636 – A Resolution of the City Council Approving Amendment No. 2 to Lease Agreement with the Folsom Chamber of Commerce for the Depot Building Located at 200 Wool Street.

BACKGROUND / ISSUE

The Folsom Chamber of Commerce (“Chamber”) has leased the Depot Building at 200 Wool Street from the former Folsom Redevelopment Agency and the City of Folsom continuously since 1989. The current lease authorized by the Council runs from January 1, 2015 to December 31, 2022. Rent for the property has been \$1.00 per year for more than a decade, if not longer.

The 2,383 square feet property is zoned HD in the General Plan, which means it can be used for retail, service, public/quasi-public and office uses, as well as small light industrial and residential uses (subject to certain limitations). The Chamber has used the property as its principal place of business, as well as a visitor’s center for people visiting the City and the Historic District.

POLICY / RULE

The City Council is vested with authority to approve agreements to lease City facilities and City-owned real properties.

ANALYSIS

The Chamber has recently requested that the lease be extended for 20 more years (5-year renewal with three 5-year options to extend), and to allow a portion of the leased premises to be sublet to a third-party under a revenue-sharing arrangement upon approval of the proposed lease amendment by the City Council. Rent remains at \$1.00 per year, with the same existing obligations for maintenance, upkeep, insurance, and indemnification. The request is made long in advance of the December 2022 expiration due to the Chamber's desire to make improvements to the property.

The Chamber has been a good steward of this historical structure and has provided valuable services to the Historic District with the Visitor's Center and services to the community. The Chamber has continued to provide maintenance and upkeep to the building. Staff believes there continues to be substantial public benefit to the Historic District and the City to lease the Depot Building to the Chamber under terms and conditions of the existing lease, modified as follows:

- Term of the lease extended to December 31, 2027.
- Rent remains \$1.00 per year.
- Chamber has option to extend the lease 3 times; each extension is 5 years. If Chamber exercises all 3 options, the total term of the lease under this Amendment No. 2 will be 20 years (terminating on December 31, 2042).
- Chamber may sublease a portion of the Depot Building under a revenue-sharing arrangement. Once the Chamber recovers its initial investment in tenant improvement of the subleased area, the revenue from the sublease shall be shared with the City on a 50%-50% basis.
- All proposed improvements to the Depot Building require City review and approval.

The City has not experienced any issue with the Chamber leasing the Depot Building, operating the Visitor's Center, making lease payments, or honoring the lease terms.

FINANCIAL IMPACT

This action is likely to generate nominal revenue to the City's General Fund.

ENVIRONMENTAL REVIEW

The proposed extension of existing lease of public facility is exempt from environmental review under the California Environmental Quality Act pursuant to Section 15301 (Existing Facilities) of the CEQA Guidelines.

ATTACHMENTS

1. Resolution No. 10636 – A Resolution of the City Council Approving Amendment No. 2 to Lease Agreement with the Folsom Chamber of Commerce for the Depot Building Located at 200 Wool Street
2. Amendment No. 2 to Lease Agreement

Respectfully submitted,

Elaine Andersen, City Manager

ATTACHMENT 1

RESOLUTION NO. 10636

A RESOLUTION OF THE CITY COUNCIL APPROVING AMENDMENT NO. 2 TO LEASE AGREEMENT WITH THE FOLSOM CHAMBER OF COMMERCE FOR THE DEPOT BUILDING LOCATED AT 200 WOOL STREET

WHEREAS, the Folsom Chamber of Commerce (“Chamber”) has leased the property and building at 200 Wool Street (the “Depot Building”) continuously since 1989 from the former Folsom Redevelopment Agency (“RDA”) and the City of Folsom; and

WHEREAS, on April 18, 2012 the Folsom Successor Agency Oversight Board, pursuant to Resolution No. 004-FSAOB, approved the transfer of Parcel B, on which the Depot Building sits, to the City; and

WHEREAS, the City acquired Parcel B by Grant Deed on March 17, 2012; and

WHEREAS, the City Council has in the past authorized extensions of the Depot Building lease to the Chamber, most recently on September 12, 2017, with the current lease expiring on December 31, 2022; and

WHEREAS, the Chamber has expressed a desire to extend the lease under the same terms as the current lease, and to allow subleasing a portion of the Depot Building under a revenue-sharing arrangement with the City; and

WHEREAS, the City desires to continue leasing the Depot Building to the Chamber; and

WHEREAS, the Chamber provides substantial public benefit to the City and to the Historic District by its presence at the Depot Building including but not limited to, operating the Visitor’s Center, providing assistance to merchants, businesses and visitors in the Historic District and the City generally, and providing assistance with tourism, economic development and events throughout the City; and

WHEREAS, the City Council finds that the public benefit provided by the Chamber supports continuing the lease with the Chamber under the terms in the current lease and as modified by Amendment No. 2 to the Lease Agreement.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Folsom that the City Manager is hereby authorized to execute Amendment No. 2 to Lease Agreement for premises located at 200 Wool Street – SP Depot Building, dated May 27, 2014, in a form acceptable to the City Attorney, to extend the lease to December 31, 2027 with options to renew.

PASSED AND ADOPTED this 8th day of June, 2021, by the following roll-call vote:

- AYES: Councilmember(s):
- NOES: Councilmember(s):
- ABSENT: Councilmember(s):
- ABSTAIN: Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

ATTACHMENT 2

**CITY OF FOLSOM
AMENDMENT NO. 2
TO
LEASE AGREEMENT FOR PREMISES LOCATED AT:
200 WOOL STREET – SP DEPOT BUILDING**

The Lease Agreement between the City of Folsom and the Folsom Chamber of Commerce dated May 27, 2014 (“Lease”) for premises located at 200 Wool Street (SP Depot Building), previously amended by Amendment No. 1 dated September 29, 2017, is further amended by this Amendment No. 2 as follows:

1. Section 2, Term, of the Lease is hereby amended as follows:

The term of this Lease shall terminate on December 31, 2027, with such rights of prior termination as may be provided herein.

The Chamber has the option to extend the Lease 3 times as follows: No later than 60 days prior to the expiration of the Lease term provided for in this Amendment No. 2 or any extended term, the Chamber may request, in writing, that the City extend the Lease for 5 more years. Should the Chamber exercise all 3 options to extend, the Lease under this Amendment No. 2 shall terminate on December 31, 2042.

2. Section 7, Termination, of the Lease is hereby amended as follows:

City may terminate this Lease, in whole or in part, at any time during the lease term, or any extension thereof, by giving Chamber notice, in writing, at least 12 months prior to the date when any such termination shall become effect.

3. Section 10, Subletting/Assignments, of the Lease is hereby amended as follows:

Chamber shall not sublet the Premises, in whole or in part, nor assign this Lease or any interest therein, except as agreed to by the City Manager in writing. As later provided herein, Chamber may rent, on a periodic basis, portion of the Premises (see Use of Premises).

Should the Chamber sublease a portion of the Premises for longer than 30 days, the Chamber shall share 50% of the revenue from the sublease with the City, payable on the 10th day of each month. The aforementioned revenue-sharing requirement shall commence once the Chamber has recovered its initial investment in tenant improvement for the subleased area. All proposed tenant improvement for the sublease area, including the projected costs, shall require prior submittal to the City for review and approval in accordance with Section 20. The term of the sublease shall not exceed the then-current term of the Chamber’s lease, and the sublease shall contain the same indemnity and

insurance requirements in favor of the City as in Sections 12 and 13 of the Lease.

4. Section 24, Use of Premises, of the Lease is hereby amended as follows:

The Premises shall be used for the official functions of the Chamber. Chamber may rent portions of the Premises on a periodic basis, not to exceed 30 consecutive days at any given time, for private parties, classes, and other similar uses with the consent of the City Manager or his/her designee. Unless prior approval is first obtained from the City Manager, no portion of the Premises shall be rented on an ongoing basis for more than 30 days, and no such rental shall compete with an existing business in the Historic District. The parties recognize that the Chamber occasionally rents out space on the Premises for meetings or conferences and nothing in this Section is intended to prohibit such use.

In the event that the Premises are rented or permissibly used as set forth above, either the Chamber's insurance shall cover the activity, or the Chamber shall obtain insurance from the person or entity renting, using, or subleasing the portion of the Premises that complies in all particulars with the insurance requirements set forth in Exhibit B.

IN WITNESS WHEREOF, the parties hereto have caused this Amendment No. 2 to Lease Agreement to be duly executed.

Date: _____

City of Folsom,
A Municipal Corporation

By: Elaine Andersen, City Manager

Date: _____

Folsom Chamber of Commerce,
A California Non-Profit Corporation

By: _____

By: _____

Approved As To Form:

Steven Wang, City Attorney

Date

Attests:

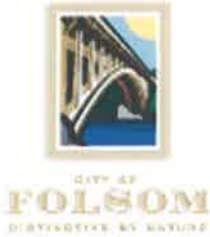
Funding Available:

Christa Freemantle, City Clerk

Stacey Tamagni, Finance Director/CFO Date

NOTICE: SIGNATURE(S) ON BEHALF OF LESSEE MUST BE NOTARIZED.

A certificate of acknowledgment in accordance with the provisions of civil code section 1189 must be attached for each person executing this agreement on behalf of consultant. California Civil Section provides, at part (b): "Any certificate of acknowledgment taken in another place shall be sufficient in this state if it is taken in accordance with the laws of the place where the acknowledgment is made."



Folsom City Council Staff Report



MEETING DATE:	6/8/2021
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10637 - A Resolution Authorizing the City Manager to Execute Amendment No. 1 to the Agreement (Contract No. 173-21 19-051) with Peterson Brustad Inc. for Design and Construction Administration Services for the Oak Avenue Pump Station Peak Wet Weather Flow Relief Project and Appropriation of Funds
FROM:	Environmental and Water Resources Department

RECOMMENDATION / CITY COUNCIL ACTION

The Environmental and Water Resources Department recommends the City Council pass and adopt Resolution No. 10637 - A Resolution Authorizing the City Manager to Execute Amendment No. 1 to the Agreement (Contract No. 173-21 19-051) with Peterson Brustad Inc. for Design and Construction Administration Services for the Oak Avenue Pump Station Peak Wet Weather Flow Relief Project and Appropriation of Funds.

BACKGROUND / ISSUE

The Environmental and Water Resources (EWR) Department identifies sewer infrastructure rehabilitation and replacement projects through sewer master plans and ongoing sewer condition assessment programs. As part of the City’s Sewer System Management Plan (SSMP) for its wastewater collection system, the EWR Department performs ongoing condition assessments on the wastewater system and corrects any defects/deficiencies identified through this process. Through these efforts, City staff identified the Oak Avenue Pump Station Peak Wet Weather Flow Relief Project.

In August of 2019, through Resolution No. 10322, City Council authorized an agreement with Peterson Brustad, Inc. to provide design and construction administration services for the Oak Avenue Pump Station Peak Wet Weather Flow Relief Project. The agreement with Peterson

Brustad, Inc. covered design services, including development of plans and specifications, as well as construction administration services, including submittal review, response to requests for information, and attendance at various meetings during project construction. Amendment No. 1 is for Peterson Brustad, Inc. to provide additional construction administration services only.

This resolution will authorize the City Manager to execute Amendment No. 1 to the Agreement (Contract No. 173-21 19-051) with Peterson Brustad, Inc. for additional construction administration services for the Oak Avenue Pump Station Peak Wet Weather Flow Relief Project and Appropriation of Funds.

POLICY / RULE

In accordance with Chapter 2.36 of the Folsom Municipal Code, supplies, equipment, services, and construction with a value of \$62,657 or greater shall be awarded by City Council.

ANALYSIS

Currently the City is in the construction phase of the Oak Avenue Pump Station Peak Wet Weather Flow Relief Project. Construction activity includes the installation of approximately 1,500 lineal feet of 12-inch sewer force main parallel to the existing force main. The project also includes the installation of plug valves, potholing to verify high points along the City’s existing force main, replacing/relocating air release valves along the City’s existing force main as necessary, and the installation of various sewer appurtenances.

The initial proposal for construction administration support was developed in 2019 and the scope of the project has significantly changed since that time. The original project scope consisted of the addition of a gravity bypass pipeline to address the peak wet weather flow. The current project scope consists of installation of a permanent bypass force main and isolation plug valves along the existing Oak Avenue force main, replacement of the existing air relief valve systems, and recoating of the existing Oak Avenue Pump Station wet well. This change in project scope along with an extended construction schedule, as a result of material supply delays due to COVID-19, require additional construction administration support. The table below lists the original construction administration services, services performed to date under the original contract and the proposed amendment construction support services.

Construction Support Services Task	Original Scoped Activity	Project Activity to Date	Estimated Activity at Completion	Proposed Amendment Activity
RFI Review	5 RFIs	14 RFIs	45 RFIs	40 RFIs
Submittal Review	15 Submittals	41 Submittals	75 Submittals	60 Submittals
Construction Meetings	6 meetings	6 meetings	18 meetings	12 meetings

This resolution will authorize the City Manager to execute Amendment No. 1 to the Agreement (Contract No. 173-21 19-051) with Peterson Brustad, Inc. for Design and Construction Administration Services for the Oak Avenue Pump Station Peak Wet Weather Flow Relief Project and Appropriation of Funds. Amendment No. 1 will be in the amount of \$89,219 and the new total contract amount, including Amendment No. 1 is \$264,107.

FINANCIAL IMPACT

The Fiscal Year 2020-21 Sewer Operating Budget did not include funding for additional construction administration services for the Oak Avenue Pump Station Peak Wet Weather Flow Relief Project. Staff is requesting an additional appropriation in the amount of \$89,219 in order to execute Amendment No. 1 to the agreement with Peterson Brustad, Inc. for a new total contract not-to-exceed amount of \$264,107. The additional appropriation would be to the Sewer Operating Budget (Fund 530) and the funding source for this appropriation would be current fund balance, which is available. This will increase the overall project budget to \$2,064,107.

ENVIRONMENTAL REVIEW

This project is a replacement and/or improvement of existing infrastructure with negligible or no expansion of use and therefore is categorically exempt from environmental review under the California Environmental Quality Act as noted in Title 14 – California Code of Regulations, Chapter 3 – Guidelines for Implementation of the California Environmental Quality Act, Article 19 – Categorical Exemptions, Section 15301 (Existing Facilities), 15302 (Replacement or Reconstruction), and/or 15304 (Minor Alterations to Land).

ATTACHMENT

Resolution No. 10637 - A Resolution Authorizing the City Manager to Execute Amendment No. 1 to the Agreement (Contract No. 173-21 19-051) with Peterson Brustad, Inc. for Design and Construction Administration Services for the Oak Avenue Pump Station Peak Wet Weather Flow Relief Project and Appropriation of Funds

Submitted,

Marcus Yasutake, Director
ENVIRONMENTAL AND WATER RESOURCES DEPARTMENT

RESOLUTION NO. 10637

A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE AMENDMENT NO. 1 TO THE AGREEMENT (CONTRACT NO. 173-21 19-051) WITH PETERSON BRUSTAD, INC. FOR DESIGN AND CONSTRUCTION ADMINISTRATION SERVICES FOR THE OAK AVENUE PUMP STATION PEAK WET WEATHER FLOW RELIEF PROJECT AND APPROPRIATION OF FUNDS

WHEREAS, the City is currently implementing its Sewer System Management Plan (SSMP) which consists of condition assessment, as well as operation and system improvements; and

WHEREAS, the City is continuing to update its System Evaluation and Capacity Assurance Plan to identify recommended system capacity improvements; and

WHEREAS, the City has identified this project to help relieve peak wet weather flows from the Oak Avenue Pump Station into sewer shed basin 10 and the Folsom East 3 trunk sewer line; and

WHEREAS, Peterson Brustad Inc. by reason of their past experience and abilities for performing these types of services, is qualified to perform the required engineering services for the project; and

WHEREAS, sufficient funds are available in the Sewer Operating Fund (Fund 530), however, an additional appropriation in the amount of \$89,219 will be needed for this amendment, increasing the overall project budget to \$2064,107; and

WHEREAS, the agreement will be in a form acceptable to the City Attorney:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom authorizes the City Manager to execute Amendment No. 1 to the Agreement (Contract No. 173-21 19-051) with Peterson Brustad, Inc. for Design and Construction Administration Services for the Oak Avenue Pump Station Peak Wet Weather Flow Relief Project in the amount of \$89,219 and the new total contract amount, including Amendment No. 1 is \$264,107.

BE IT FURTHER RESOLVED that the Finance Director is directed to appropriate \$89,219 to the FY2020-21 Sewer Operating Fund (Fund 530). The appropriation will be from fund balance which is currently available.

PASSED AND ADOPTED this 8th day of June 2021, by the following roll-call vote:

AYES: Councilmember(s):

NOES: Councilmember(s):

ABSENT: Councilmember(s):

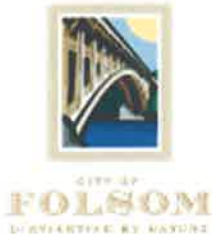
ABSTAIN: Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

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Folsom City Council Staff Report

MEETING DATE:	6/8/2021
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10638 – A Resolution Authorizing the City Manager to Execute an Agreement with Badger Meter, Inc. for the Purchase of 1-Inch Water Meters and Rejecting all Bids for 1.5-Inch and 2-Inch Water Meters
FROM:	Environmental and Water Resources Department

RECOMMENDATION / CITY COUNCIL ACTION

The Environmental and Water Resources Department recommends that the City Council pass and adopt Resolution No. 10638 – A Resolution Authorizing the City Manager to Execute an Agreement with Badger Meter, Inc. for the Purchase of 1-Inch Water Meters and Rejecting all Bids for 1.5-Inch and 2-Inch Water Meters.

BACKGROUND / ISSUE

The Environmental and Water Resources Department has identified the need to purchase 1-inch water meters for installation on new homes and to replace older water meters that may be reaching the end of their serviceable life. As required by State Law, water meters have been installed on all new homes since 1992, with metered rate billing for all the City’s water customers beginning in 2013. Water meters are used for water metering, water conservation and are an integral part of the water billing process. This authorization for the purchase of water meters will meet the anticipated demands of new residential construction and the necessary residential meter replacements.

The City currently has approximately 22,500 installed water meters, which range in size from 5/8-inch to 12-inch. It is anticipated that approximately 750 of these need to be replaced annually, while new construction will require up to 1,500 new water meters installations per year. To meet this demand of the most widely used meters, staff issued a request for bids for the supply of 1-inch and/or 1.5-inch and 2-inch water meters.

The objective of this process is to bid consistent meter specifications and receive competitive pricing, while maintaining a specific level of quality and accuracy for the water meters. Staff recommends a five-year contract to maintain consistent meter pricing across all new meter purchases.

As a result of the bids, the Environmental and Water Resources Department is requesting authorization for the City to enter into an agreement for five years with Badger Meter, Inc. for the purchase of 1-inch water meters. Upon review of the 1.5-inch and 2-inch positive displacement meter bids, staff determined that there would be a need to revise the specifications for the 1.5-inch and 2-inch water meters in order to have a more comprehensive meter bid package to encompass multiple meter styles and technologies to meet operational needs. There is also an opportunity to include meter sizes from 3-inch to 12-inch in the bid package. It was decided that the department should therefore issue a new request for bids for all larger meters, sized from 1.5 inch to 12 inch. Consequently, staff requests the rejection of all the 1.5-inch and 2-inch bids received on April 13, 2021.

POLICY / RULE

In accordance with Chapter 2.36 of the Folsom Municipal Code supplies, equipment, services, and construction with a value of \$62,657 or greater shall be approved by City Council.

ANALYSIS

Bids were solicited by advertising in the March 28 and April 2, 2021, editions of the Sacramento Bee, and online at www.CIPList.com. On April 13, 2021, the Environmental and Water Resources Department received the following responses to the 1-inch water meter bid and the combined 1.5-inch and 2-inch water meter bid:

1-inch Water Meters

	<u>Vendor</u>	<u>Unit Price</u>
1.	Zenner USA	\$110.74*
2.	Badger Meter Inc.	\$147.00
3.	Aqua-Metric	\$150.00
4.	Ferguson Water Works	\$152.00

1-1/2-inch and 2-inch Water Meters

	<u>Vendor</u>	<u>Unit Price</u>		
		1 ½”	2”	Total Bid
1.	Aqua-Metric	\$293.09	\$326.42	\$30,975.50**
2.	Zenner USA	\$270.03	\$350.13	\$31,008.00**
2.	Ferguson Water Works	\$312.50	\$405.00	\$35,975.00**
5.	Badger Meter Inc.	\$320.00	\$410.00	\$36,500.00**

*Meter proposed does not meet specifications.

** Bids requested to be rejected.

Pursuant to the Environmental and Water Resources Department procedure for evaluation of bids, 1-inch meter bids were reviewed by staff and all except Zenner USA meters were determined to meet the required specifications. After discussion with the City Attorney regarding the submittal from Zenner USA, their bid was deemed non-responsive because their meter submittal did not meet the required meter specification. Specifically, the Zenner meter proposal was deemed non-responsive because they did not meet the specification as related to the meter register in the following areas: registers are a pulse type, Nicor wire is not permanently potted to the register and the register ID is programmable. Therefore, the Environmental and Water Resources Department recommends execution of an agreement with the apparent, responsive low bidder of 1-inch meters, Badger Meter, Inc.

Subsequently, with the need for a more comprehensive meter bid to encompass multiple meter styles and technologies, the Environmental and Water Resources Department requests rejection of all 1.5-inch and 2-inch bids so a new request for bids, including large water meters up to 12-inch, can be issued.

This resolution will authorize the City Manager to execute an Agreement with Badger Meter Inc. for the purchase of 1-inch water meters and reject all 1.5-inch and 2-inch meter bids.

FINANCIAL IMPACT

This agreement will allow for the annual purchase of up to 1,500 1-inch water meters for new homes, to be paid for out of the Water Meter Fund (Fund 522). The Water Meter Fund (Fund 522) is funded through fees paid by Developers for the installation of water meters and meter reading equipment. Additionally, the agreement will allow for the purchase of up to 750 1-inch water meters for replacement of existing water meters, to be paid for out of the Water Operating Fund (Fund 520). Based on Badger Meter, Inc’s bid price of \$147.00 per meter, the combined total annual cost for the meters is \$330,750, split out by fund as shown in the table below.

	Unit Price	Estimated Quantity per year for new homes (Fund 522)	Total Cost per year (Fund 522)	Estimated Quantity per year for replacement meters (Fund 520)	Total Cost per year (Fund 520)
1-inch meters	\$147.00	1,500	\$220,500	750	\$110,250

Staff recommends approval of a five-year contract for a not-to-exceed total \$1,653,750. Sufficient funds will be made available through the budget process throughout the period of the Agreement in the Water Meter Fund (Fund 522) and the Water Operating Fund (Fund 520).

ENVIRONMENTAL REVIEW

The purchase of water meters is exempt from environmental review under the California Environmental Quality Act (CEQA).

ATTACHMENT

Resolution No. 10638 – A Resolution Authorizing the City Manager to Execute an Agreement with Badger Meter, Inc. for the Purchase of 1-Inch Water Meters and Rejecting all Bids for 1.5-Inch and 2-Inch Water Meters

Submitted,

Marcus Yasutake, Director
ENVIRONMENTAL AND WATER RESOURCES DEPARTMENT

RESOLUTION NO. 10638**A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE AN AGREEMENT WITH BADGER METER, INC. FOR THE PURCHASE OF 1-INCH WATER METERS AND REJECTING ALL BIDS FOR 1.5-INCH AND 2-INCH WATER METERS**

WHEREAS, staff has identified the need to purchase water meters for installing on new homes as required by State Law and to replace older water meters that may be reaching the end of their serviceable life; and

WHEREAS, a Request for Proposals (“RFP”) for 1-inch, 1.5-inch, and 2-inch water meters were issued on March 28, 2021; and

WHEREAS, Zenner USA submitted a non-responsive low bid for the 1-inch water meter that does not meet the City’s specifications in the RFP; and

WHEREAS, Badger Meter, Inc. submitted the lowest responsive, responsible bid that met required specification for the 1-inch meters, opened on April 13, 2021; and

WHEREAS, based on the City need of a more comprehensive meter bid to encompass multiple meter styles and technologies to meet operational needs, staff requests rejection of all 1.5-inch and 2-inch bids so a new request for bids, including large water meters up to 12-inch, can be issued; and

WHEREAS, funds will be made available in the Water Meter Fund (Fund 522) for \$220,500 per year for the next five fiscal years; and

WHEREAS, funds will be made available in the Water Operating Fund (Fund 520) for \$110,250 per year for the next five fiscal years; and

WHEREAS, the agreement will be in a form acceptable to the City Attorney.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Folsom that the low bid for 1-inch water meter submitted by Zenner USA is hereby rejected as being non-responsive to the City’s specifications in the RFP.

BE IT FURTHER RESOLVED that the City Manager is authorized to execute an agreement with Badger Meter, Inc. for the purchase of 1-inch water meters not to exceed \$110,250 per year for 5 years.

BE IT FURTHER RESOLVED that all bids received in response to the RFP for 1.5-inch and 2-inch water meters are hereby rejected.

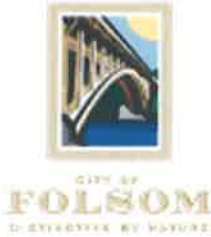
PASSED AND ADOPTED this 8th day of June, 2021, by the following roll-call vote:

AYES: Councilmember(s):
NOES: Councilmember(s):
ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK



Folsom City Council Staff Report

MEETING DATE:	6/8/2021
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10640 - A Resolution Authorizing the City Manager to Execute Non-Exclusive Agreements with Approved Licensed Contractors to Implement the Sacramento Housing and Redevelopment Agency Inter-Agency Contract for the Seniors Helping Seniors Home Repair Program and Appropriation of Funds
FROM:	Community Development Department

RECOMMENDATION / CITY COUNCIL ACTION

Move to adopt Resolution No. 10640 - A Resolution Authorizing the City Manager to Execute Non-Exclusive Agreements with Approved Licensed Contractors to Implement the Sacramento Housing and Redevelopment Agency Inter-Agency Contract for the Seniors Helping Seniors Home Repair Program and Appropriation of Funds.

BACKGROUND / ISSUE

On June 9, 2020, the City Council approved Resolution No. 10463 which authorized the City Manager to Execute an Agreement with the Sacramento Housing and Redevelopment Agency (SHRA) for continued participation in the Community Development Block Grant (CDBG) Program from January 1, 2021 to December 31, 2023. The City Manager is currently in process to enter into an Inter-Agency Contract with SHRA to receive \$220,000 in CDBG funds to subsidize the City of Folsom Seniors Helping Seniors (SHS) Program for FY 2021/2022.

On May 12, 2020, the City Council approved Resolution No. 10432, authorizing the City Manager to execute agreements with a total of seven contractors (ABC Home Improvements, Benchmark Plumbing, F.L. Electric Service Company, Gray Construction and Securement Services, Inc., Robnett Construction, Inc., Roseville Sheet Metal Inc., and Sacramento City Building Co.) to perform contractor services in accordance with the Sacramento Housing and Redevelopment Agency (SHRA) Seniors Helping Seniors (SHS) Inter-Agency Contract.

For Fiscal Year 2019/2020 the City Manager approved annual contracts with each of the seven Senior Helping Seniors contractors (ABC Home Improvements, Benchmark Plumbing, F.L. Electric Service Company, Gray Construction and Securement Services, Inc., Robnett Construction, Inc., Roseville Sheet Metal Inc., and Sacramento City Building Co.). The existing seven non-exclusive agreements will terminate on June 30, 2021 and staff is recommending that the City Manager be authorized to execute new contracts with the seven aforementioned contractors which will be in effect until June 30, 2022 and with a not-to-exceed amount of \$220,000 (total aggregate amount for all contractors).

Staff requests that the City Council authorize the City Manager to execute non-exclusive agreements with ABC Home Improvements, Benchmark Plumbing, F.L. Electric Service Company, Gray Construction and Securement Services, Inc., Roseville Sheet Metal Inc., Robnett Construction, Inc. and Sacramento City Building to perform contractor services in accordance with the Seniors Helping Seniors Home Repair Program.

POLICY / RULE

In accordance with Section 2.36.120 of the Folsom Municipal Code, City Council approval is required for professional services contracts in excess of \$62,657.

ANALYSIS

The Seniors Helping Seniors Home Repair Program is available to all low-income, age-qualified senior citizens who reside in the City of Folsom. The program is designed to assist those seniors who do not have the assets or financial capability to make necessary home repairs related to health and safety. Both manufactured homes and non-manufactured homes are eligible for the program. On average, the City's SHS Program responds to over 300 calls for service each year.

As directed by SHRA and required by CDBG Federal Requirements, the City solicited a Request for Qualifications (RFQ) for Contractor Services for the SHS Program. The intent of the RFQ was to contract with four to eight contractors for the SHS Program.

A total of seven companies (ABC Home Improvements, Benchmark Plumbing, F.L. Electric Service Company, Gray Construction and Securement Services, Inc., Robnett Construction, Inc., Roseville Sheet Metal Inc., and Sacramento City Building) submitted their qualification packages to City and the Community Development staff determined that all seven companies are qualified to provide a range of contracting services necessary for the SHS Program. Furthermore, all seven of the contractors are experienced in small home repairs and working with low-income senior citizens and/or physically disabled homeowners.

With approval of this resolution and the necessary contracts (not-to-exceed a combined total of \$220,000 for all said contracts), the following contractors will assist the City with providing contractor services as needed for the SHS Program:

1. ABC Home Improvements
2. Benchmark Plumbing
3. F.L. Electric Service Company
4. Gray Construction and Securement Services, Inc.
5. Robnett Construction, Inc.
6. Roseville Sheet Metal Inc.
7. Sacramento City Building Co.

FINANCIAL IMPACT

There is no financial impact to the General Fund. The CDBG funding program requires the expenditure of funds with reimbursement requested and therefore the Community Development Block Grant Fund (Fund 201) is utilized to cover the upfront contractor and material costs and invoices are sent to SHRA monthly for reimbursement from the grant proceeds. For the Fiscal Year 2021/22, a total of \$165,000 of the Community Development Block Grant Fund (Fund 201) has been appropriated for the SHS Program to cover the cost of materials and outside contractors. An additional appropriation in the amount of \$55,000 will be required to match the funding available through SHRA.

ENVIRONMENTAL REVIEW

This action by the City Council is exempt from environmental review pursuant to Section 15061(b)(3) (Review for Exemption) of the California Environmental Quality Act.

ATTACHMENTS

Resolution No. 10640 - A Resolution Authorizing the City Manager to Execute Non-Exclusive Agreements with Approved Licensed Contractors to Implement the Sacramento Housing and Redevelopment Agency Inter-Agency Contract for the Seniors Helping Seniors Home Repair Program and Appropriation of Funds.

Submitted,



Pam Johns, Community Development Director

RESOLUTION NO. 10640**A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE NON-EXCLUSIVE AGREEMENTS WITH APPROVED LICENSED CONTRACTORS TO IMPLEMENT THE SACRAMENTO HOUSING AND REDEVELOPMENT AGENCY INTER-AGENCY CONTRACT FOR THE SENIORS HELPING SENIORS HOME REPAIR PROGRAM AND APPROPRIATION OF FUNDS**

WHEREAS, Resolution No. 10463, adopted by the City Council on June 9, 2020, authorized the City Manager to execute an agreement with the Sacramento Housing and Redevelopment Agency (SHRA) for continued participation in the Community Development Block Grant (CDBG) Program from January 1, 2021 to December 31, 2023; and

WHEREAS, funds from the CDBG Program subsidize the City's Seniors Helping Seniors (SHS) Program, which provides eligible senior citizens in the City with financial assistance to repair and remedy health and safety hazards in their homes; and

WHEREAS, SHRA has approved \$220,000 in CDBG funds to subsidize the City's SHS Program for Fiscal Year 2021/22; and

WHEREAS, the City Manager is required to enter into an Inter-Agency Contract with SHRA to receive the approved \$220,000 CDBG funds; and

WHEREAS, the CDBG funding program requires the expenditure of funds with reimbursement requested and for Fiscal Year 2021/22; and

WHEREAS, the proposed Fiscal Year 2021/22 Budget, includes an appropriation of \$165,000 in the Community Development Block Grant Fund (Fund 201) for the SHS Program, and an additional appropriation of \$55,000 will be needed; and

WHEREAS, contractors are selected based on their professional qualifications and experience to perform services under the SHS Program; and

WHEREAS, the seven contractors (ABC Home Improvements, F.L. Electrical Service Company, Gray Construction and Securement Services, Inc., Robnett Construction, Inc., Roseville Sheet Metal Inc., and Sacramento City Building Co.) have existing contracts with the City for performing this work to June 30, 2021; and

WHEREAS, in accordance with Chapter 2.36 of the Folsom Municipal Code, contracts of \$62,657 or greater shall be approved by the City Council;

NOW, THEREFORE, BE IT RESOLVED in the event that CDBG funding from SHRA is available, the City Council of the City of Folsom hereby authorizes the City Manager to execute non-exclusive contracts for professional licensed contractor services with ABC Home Improvements, Benchmark Plumbing, F.L. Electric Service Company, Gray Construction and Securement Services, Inc., Robnett Construction, Inc., Roseville Sheet Metal Inc., and

Sacramento City Building Co., to render services under the City’s SHS Program through June 30, 2022, not-to-exceed a combined total of \$220,000 for all said contracts.

NOW, THEREFORE, BE IT FURTHER RESOLVED the Finance Director is authorized to appropriate an additional \$55,000 to the Community Development Block Grant Fund (Fund 201) in the amount of \$55,000 in the FY 2021-22 Budget.

PASSED AND ADOPTED this 8th day of June 2021, by the following roll-call vote:

AYES: Councilmember(s):

NOES: Councilmember(s):

ABSENT: Councilmember(s):

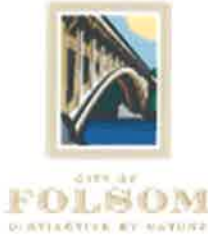
ABSTAIN: Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

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Folsom City Council Staff Report



MEETING DATE:	6/8/2021
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10641 - A Resolution Authorizing the City Manager to Execute a Consultant Services Agreement with Ghirardelli Associates, Inc. for Construction Inspection and Materials Testing for the Pavement Resurfacing Project FY 2020-21
FROM:	Public Works Department

RECOMMENDATION / CITY COUNCIL ACTION

The Public Works Department recommends that the City Council pass and adopt Resolution No. 10641 - A Resolution Authorizing the City Manager to Execute a Consultant Services Agreement with Ghirardelli Associates, Inc. for Construction Inspection and Materials Testing for the Pavement Resurfacing Project FY 2020-21.

BACKGROUND / ISSUE

The Public Works Department utilizes consultant inspection and materials testing services on projects that will benefit from full time oversight. The Pavement Resurfacing Project FY 2020-21 is scheduled to begin in July 2021 and will require this additional oversight. In addition to providing one full time inspector, the consultant will perform materials sampling and testing to ensure compliance with contract specifications.

POLICY / RULE

Section 2.36.080, Award of Contracts of the Folsom Municipal Code states, in part, that contracts for supplies, equipment, services and construction with an estimated value of \$62,657 or greater shall be awarded by City Council.

ANALYSIS

A Request for Proposals was advertised in December 2018 for qualified engineering firms to provide construction inspection and materials testing for various capital improvement projects. Of the nine proposals received, three firms were shortlisted based on the experience level of their staff and their working knowledge of construction inspection and materials testing: 4Leaf, Inc., Ghirardelli Associates, Inc. (Ghirardelli Associates) and Unico Engineering, Inc.

All three shortlisted firms recently provided construction inspector resumes for their proposed inspector to be assigned to this project. Staff reviewed the resumes, and based on the qualifications of the proposed inspector, selected Ghirardelli Associates to provide the inspection and materials testing services for the Pavement Resurfacing Project FY 2020-21. Staff has found the provided scope and fee to be acceptable for the level of professional services requested.

FINANCIAL IMPACT

Ghirardelli Associates has provided a scope and fee to provide the Inspection and Materials Testing in the amount of \$119,416.

Staff is requesting that a Consultant Services Agreement in the amount of \$119,416 be authorized for the Inspection and Material Testing for the Pavement Resurfacing Project FY 2020-21.

Funds in the amount of \$119,416 are budgeted and available in the Street Overlay/Pavement Management Project No. 8017 for Fiscal Year 2020-21, utilizing \$119,416 of Measure A Funds (Fund 276).

ENVIRONMENTAL REVIEW

This project has been deemed categorically exempt from environmental review.

ATTACHMENTS

Resolution No. 10641 - A Resolution Authorizing the City Manager to Execute a Consultant Services Agreement with Ghirardelli Associates, Inc. for Construction Inspection and Materials Testing for the Pavement Resurfacing Project FY 2020-21.

Submitted,

Dave Nugen, PUBLIC WORKS DIRECTOR

RESOLUTION NO. 10641

A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE A CONSULTANT SERVICES AGREEMENT WITH GHIRARDELLI ASSOCIATES, INC. FOR CONSTRUCTION INSPECTION AND MATERIALS TESTING FOR THE PAVEMENT RESURFACING PROJECT FY 2020-21

WHEREAS, the City of Folsom desires to augment existing engineering staff with construction inspection and materials testing support; and

WHEREAS, Ghirardelli Associates, Inc. is a firm included in the Public Works Department’s shortlist of qualified engineering firms to provide construction inspection and materials testing; and

WHEREAS, Ghirardelli Associates, Inc. submitted a scope and fee for the Inspection and Materials Testing of the Pavement Resurfacing Project FY 2020-21 that staff has found to be acceptable for the level of professional services requested; and

WHEREAS, funds in the amount of \$119,416 are budgeted and available in the Street Overlay/Pavement Management Project No. 8017 utilizing Measure A Funds (Fund 276); and

WHEREAS, the agreement will be in a form acceptable to the City Attorney:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom authorizes the City Manager to execute a Consultant Services Agreement with Ghirardelli Associates, Inc. for Construction Inspection and Materials Testing for the Pavement Resurfacing Project FY 2020-21.

PASSED AND ADOPTED this 8th day of June, 2021, by the following roll-call vote:

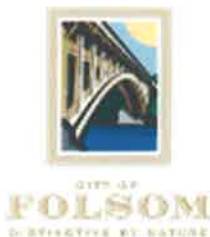
- AYES:** Councilmember(s):
- NOES:** Councilmember(s):
- ABSENT:** Councilmember(s):
- ABSTAIN:** Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

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Folsom City Council Staff Report

MEETING DATE:	6/8/2021
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution 10642 – A Resolution Authorizing the City Manager to Execute the Third Amendment with Sacramento County for Reduced Tipping Fees for Municipal Solid Waste at Kiefer Landfill
FROM:	Public Works Department

RECOMMENDATION / CITY COUNCIL ACTION

Staff recommends the City Council adopt Resolution 10642 – A Resolution Authorizing the City Manager to Execute the Third Amendment with Sacramento County for Reduced Tipping Fees for Municipal Solid Waste at Kiefer Landfill.

BACKGROUND / ISSUE

In 2007, City Council adopted Resolution No. 8065 which authorized the City Manager to execute a 15-year agreement with Sacramento County to handle and dispose of the City's municipal solid waste (MSW) at the Kiefer Landfill (Kiefer). The agreement contains an annual cost adjustment on the tipping (disposal) fee based on the Consumer Price Index (CPI). There was also a limit placed on the CPI where such increases could not exceed three percent of the previous year's disposal fee for solid waste tipped at Kiefer Landfill.

In 2010, City Council adopted Resolution No. 8669 which authorized the City Manager to amend the agreement with Sacramento County. Amendment number one provided a reimbursement to the County for the cost of handling and disposal of household hazardous waste (HHW) dropped off by Folsom residents at County facilities. The cost for this service is modest, totaling approximately \$14,000 annually. Most of these costs are integrated into the City's solid waste disposal tipping fee while a smaller portion is accounted for as a separate fee to validate Folsom residency (Verification Fee). Amendment number one contained

specific costs and a methodology for calculating the HHW management costs to the County. The annual cost adjustment is also based on the CPI with a three percent annual limit.

In 2015, City Council adopted Resolution No. 9594 which authorized the City Manager to amend the agreement a second time. In amendment number two, the charges for the handling and disposal of City-generated HHW materials disposed at County facilities was changed and was based on a new cost structure which incorporated updated time and motion studies for processing these materials. Amendment number two only applied to the handling, disposal, and administrative costs for HHW items. The third amendment preserves, the CPI-based annual adjustments to the verification and disposal fees and contains a modified cost adjustment method based on year over year changes to the County's actual costs. The third amendment also specifies that the annual adjustments for the combined costs for all services cannot exceed three percent of the cost of the prior year's solid waste disposal fee.

POLICY / RULE

Section 2.36.080, Award of Contracts of the Folsom Municipal Code states in part that contracts for supplies, equipment, services, and construction with an estimated value of \$62,657 or greater shall be awarded by City Council.

Chapter 5.04, Purchasing and Contracting, of the Folsom City Charter includes in part, a provision for procurement and services and joint contracts with or through other government jurisdictions.

ANALYSIS

In this third request to amend the agreement, Sacramento County proposes to change the time frame and the way HHW fees are calculated. They have also added language that allows for adjustments in fees related to changes in regulatory requirements and/or new materials being identified as hazardous waste. The fee schedule will also provide consistency to all the County's HHW customers as it will be the same fee schedule the County uses for all business and residential customer loads. Specifically, the time frame will coincide with the City and County fiscal year and the fees will be calculated based on the actual costs incurred, including but not limited to labor, materials, transportation, and disposal.

There are several key benefits associated with the proposed amendment which include:

- Future fee increases shall continue to be held to an annual adjustment of no more than three percent from the previous year's tipping fee and shall not exceed the County's General Public Fee;
- The agreement provides a resource to help prevent the illegal dumping of household hazardous waste along the City's roadsides and waterways;
- The agreement continues to provide an option for Folsom residents who choose to dispose of their HHW materials on weekends at County facilities; and

- The City preserves a resource for the handling and disposal of municipal solid waste and HHW through June of 2022.

FINANCIAL IMPACT

The verification fees paid for HHW services under this agreement in Fiscal Year 2019-20 were \$5,464.90 for 422 Folsom customers using Sacramento County facilities. Based on the same usage, it is projected this change will increase costs for verification fees approximately \$60. per fiscal year with the under 3% CPI adjustment. The per ton tipping fee to cover the cost of HHW handling is estimated to increase by \$.17 per ton. The City delivers approximately 41,000 tons of MSW each year, so the anticipated annual increase is approximately \$7,000. Since the HHW adjustment is calculated on the prior year actual cost, the tipping fee increase would not be recognized until FY 2022-23.

There are three, one-year extensions available under the existing agreement. However, the County has significantly raised rates at their facilities and our current reduced tipping fee is well under their new rate for MSW. Solid Waste Division staff has informally polled County staff and do not expect the County will wish to extend under these terms and we can expect a future amendment to dramatically increase the rates for MSW. We anticipate having to renegotiate MSW tipping fees in addition to the HHW portion of the fee to extend under the existing agreement, or a new agreement will need to be negotiated. The future contract cost could increase as high as \$700,000 over the Fiscal Year 2021-22 projection.

Funds have been budgeted and are available in the Solid Waste Operating Fund (Fund 540) for Fiscal Year 2021-22. Subsequent fiscal years will be included during the annual budget process.

ENVIRONMENTAL REVIEW

This action is exempt from environmental review under the California Environmental Quality Act (CEQA) in accordance with Section 21080.

ATTACHMENT

Resolution 10642 – A Resolution Authorizing the City Manager to Execute the Third Amendment with Sacramento County for Reduced Tipping Fees for Municipal Solid Waste at Kiefer Landfill

Submitted,

Dave Nugen, Public Works Director

RESOLUTION NO. 10642

A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE THE THIRD AMENDMENT WITH SACRAMENTO COUNTY FOR REDUCED TIPPING FEES FOR MUNICIPAL SOLID WASTE AT KIEFER LANDFILL

WHEREAS, Sacramento County has been handling and disposing of the City’s municipal solid waste as approved by City Council under Resolution No. 8065; and

WHEREAS, City Council passed and adopted Resolution No. 8669, a Resolution that amended Resolution No. 8065, which provides a cost-reimbursement to Sacramento County for Folsom residents who periodically deliver household hazardous waste to Sacramento County Disposal Facilities; and

WHEREAS, Sacramento County has proposed to change the time frame and method by which the fees for household hazardous waste are calculated to ensure adequate cost recovery; and

WHEREAS, the City continues to provide measures to prevent the illegal dumping of household hazardous waste along the City’s roadsides and waterways; and

WHEREAS, the City continues to ensure that annual program cost increases cannot exceed three percent from the previous year’s fees for municipal solid waste and household hazardous waste disposal at Sacramento County facilities; and

WHEREAS, sufficient funds are available in the Solid Waste Operating Fund (Fund 540); and

WHEREAS, the agreement shall be in a form acceptable to the City Attorney:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom authorizes the City Manager to execute the Third Amendment with Sacramento County for reduced tipping fees for municipal solid waste at Kiefer Landfill.

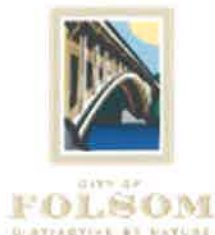
PASSED AND ADOPTED on this 8th day of June 2021, by the following roll-call vote:

- AYES: Councilmember(s)
- NOES: Councilmember(s)
- ABSENT: Councilmember(s)
- ABSTAIN: Councilmember(s)

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK



Folsom City Council Staff Report

MEETING DATE:	6/8/2021
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10644 - A Resolution Authorizing the Public Works Department to Submit a Rebuilding American Infrastructure with Sustainability and Equity Grant Application for the Riley Street Sidewalks Project
FROM:	Public Works Department

RECOMMENDATION / CITY COUNCIL ACTION

The Public Works Department recommends that the City Council pass and adopt Resolution No. 10644 - A Resolution Authorizing the Public Works Department to Submit a Rebuilding American Infrastructure with Sustainability and Equity Grant Application for the Riley Street Sidewalks Project.

BACKGROUND / ISSUE

On April 13, 2021, the U.S. Department of Transportation (USDOT) issued a call for grant applications for the Rebuilding American Infrastructure with Sustainability and Equity (RAISE) Program. Formerly known as the Better Utilizing Investments to Leverage Development (BUILD) and Transportation Investment Generating Economic Recovery (TIGER) grant programs, RAISE supports surface transportation projects that will have significant local or regional impacts. The program has a total funding amount of \$1 billion, with individual awards of up to \$25 million with a local match of 20 percent. Grant applications are due July 12, 2021.

Eligible applicants are State, local, tribal, and U.S. territory governments, including transit agencies, port authorities, metropolitan planning organizations and other political subdivisions of State or local government; therefore, the City of Folsom meets the criteria.

The project for which RAISE funding is requested must be within a census-designated urban area that had a population greater than 200,000 in the 2010 Census; by virtue of being an incorporated City within Sacramento County, the City of Folsom appears to also meet this criterion.

Lastly, the project for which RAISE funding is requested must be within an “Area of Persistent Poverty,” which USDOT has defined as a census tract in which the project is located has a poverty rate of at least 20 percent as measured by the 2014-2018 5-year data series available from the American Community Survey of the Bureau of the Census. Using the lookup tool provided by USDOT, only one census tract in the City of Folsom meets this definition: census tract 84.04, generally described as the Historic District, extending from Lake Natoma on the west and north, Bidwell Street on the south, and Wales Drive on the east.

Based on these criteria, staff has identified that the Riley Street Sidewalk project is eligible for RAISE grant funding. The Riley Street Sidewalk Project will construct sidewalks and curb ramps along Riley Street between Bidwell Street and Sutter Street. This is considered a critical gap closure in the City’s pedestrian network because it links the Central Business District to the Historic District, enhances school age pedestrian safety and provides a transportation alternative for transit dependent residents seeking to access the Historic Folsom Light Rail Station. The total project cost is estimated to be \$5 million, comprised of a \$4 million grant request and a \$1 million local match.

POLICY / RULE

The City Council adopted Resolution No. 10405 Amending Financial Policies of the City of Folsom regarding grant administration on April 14, 2020. Section D – Intergovernmental Revenues, Paragraph A – Grant Applications states, “Grant application shall be the responsibility of the department seeking the grant. Grant applications which require an expenditure of funds by the City of less than or equal to the City Manager contract authority amount may be approved by the City Manager prior to submittal to the grantor. All other grant applications, including any requiring an ongoing commitment of resources or staff, shall be reviewed and approved by the City Council prior to submittal. In circumstances where it is not possible for the City Council to approve the grant application prior to submittal, the City Manager may approve the application subject to the City Council ratification as soon as practicable to do so.

Requests to the City Council or the City Manager for approval of a grant application shall identify anticipated long-term maintenance and/or renovation costs, required City matching funds, and additional personnel that may be needed if the grant is awarded for a capital project. With respect to grants for staffing, the source of funds for long-term staffing after the expiration of the grant shall also be identified in the request for approval of the grant application.

All grant applications on behalf of the City shall be reviewed by the Finance Director before submittal to the City Council or the grantor.”

ANALYSIS

Public Works staff has reviewed the RAISE application and has found it to be complete and accurate for submittal.

FINANCIAL IMPACT

There is no financial impact associated with submitting the RAISE grant application. If the City is awarded funding, a local funding match of \$1 million (20 percent) is required. Staff will return to Council to identify the source of the local funding match and accept the grant if approved.

ENVIRONMENTAL REVIEW

This action is exempt from environmental review under the California Environmental Quality Act (CEQA).

ATTACHMENT

1. Resolution No. 10644 - A Resolution Authorizing the Public Works Department to Submit a Rebuilding American Infrastructure with Sustainability and Equity Grant Application for the Riley Street Sidewalks Project

Submitted,

Dave Nugen, Public Works Director

RESOLUTION NO. 10644

A RESOLUTION AUTHORIZING THE PUBLIC WORKS DEPARTMENT TO SUBMIT A REBUILDING AMERICAN INFRASTRUCTURE WITH SUSTAINABILITY AND EQUITY GRANT APPLICATION FOR THE RILEY STREET SIDEWALK PROJECT

WHEREAS, the U.S. Department of Transportation has issued a call for grant applications for the Rebuilding American Infrastructure with Sustainability and Equity (RAISE) program; and

WHEREAS, the Public Works Department has determined that the Riley Street Sidewalk Project meets the eligibility criteria and would be a competitive project; and

WHEREAS, the Riley Street Sidewalk Project would install curb ramps and sidewalks along Riley Street between Bidwell Street and Sutter Street, providing a safe pedestrian connection between the Central Business District, Sutter Middle School, the Historic District and the Historic District Light Rail Station; and

WHEREAS, staff intends to request \$4 million in RAISE grant funding to construct the project; and

WHEREAS, if the project is awarded grant funding, staff will return to Council for approval of the source of the required 20 percent match funding of \$1 million.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom authorizes the Public Works Department to submit a Rebuilding American Infrastructure with Sustainability and Equity grant application for the Riley Street Sidewalk Project.

PASSED AND ADOPTED this 8th day of June 2021, by the following roll-call vote:

- AYES:** Councilmember(s):
- NOES:** Councilmember(s):
- ABSENT:** Councilmember(s):
- ABSTAIN:** Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK



Folsom City Council Staff Report



MEETING DATE:	6/8/2021
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10645 - A Resolution Supporting a Rebuilding American Infrastructure with Sustainability and Equity Grant Application by the Capital Southeast Connector JPA for Connector Segments D3B and E1
FROM:	Public Works Department

RECOMMENDATION / CITY COUNCIL ACTION

The Public Works Department recommends that the City Council pass and adopt Resolution No. 10645 - A Resolution Supporting a Rebuilding American Infrastructure with Sustainability and Equity Grant Application by the Capital Southeast Connector JPA for Connector Segments D3B and E1.

BACKGROUND / ISSUE

On April 13, 2021, the U.S. Department of Transportation (USDOT) issued a call for grant applications to the Rebuilding American Infrastructure with Sustainability and Equity (RAISE) Program. Formerly known as the Better Utilizing Investments to Leverage Development (BUILD) and Transportation Investment Generating Economic Recovery (TIGER) grant programs, RAISE supports surface transportation projects that will have significant local or regional impact. The program has a total funding amount of \$1 billion, with individual awards of up to \$25 million with a local match of 20 percent. Grant applications are due July 12, 2021.

Eligible applicants are State, local, tribal, and U.S. territory governments, including transit agencies, port authorities, metropolitan planning organizations and other political subdivisions of State or local government, including the Capital Southeast Connector Joint Powers Authority (JPA).

The JPA is seeking RAISE funding for the design and construction of Connector segments D3B (East Bidwell Street to Sacramento/El Dorado County line) and E1 (Sacramento/El Dorado County line to Latrobe Road).

The Connector is a critical element of the City of Folsom's transportation system, as an element of the Folsom Plan Area Specific Plan and as a reliever route to U.S. Highway 50. Therefore, it is in the City's interest for the JPA to obtain the necessary funding to complete the portions of the project that are in close proximity to the City.

ATTACHMENT

Resolution No. 10645 - A Resolution Supporting a Rebuilding American Infrastructure with Sustainability and Equity Grant Application by the Capital Southeast Connector JPA for Connector Segments D3B and E1

Submitted,

Dave Nugen, Public Works Director

RESOLUTION NO. 10645

A RESOLUTION SUPPORTING A REBUILDING AMERICAN INFRASTRUCTURE WITH SUSTAINABILITY AND EQUITY GRANT APPLICATION BY THE CAPITAL SOUTHEAST CONNECTOR JPA FOR CONNECTOR SEGMENTS D3B AND E1

WHEREAS, the U.S. Department of Transportation has issued a call for grant applications for the Rebuilding American Infrastructure with Sustainability and Equity (RAISE) program; and

WHEREAS, the Capital Southeast Connector JPA intends to submit a RAISE grant application to fund the design and construction of Connector segments D3B and E1; and

WHEREAS, Connector segment D3B is a planned element of the City's transportation system; and

WHEREAS, the Connector project is critically important to local and regional transportation.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom hereby supports a Rebuilding American Infrastructure with Sustainability and Equity grant application by the Capital Southeast Connector JPA for Connector segments D3B and E1.

PASSED AND ADOPTED this 8th day of June 2021, by the following roll-call vote:

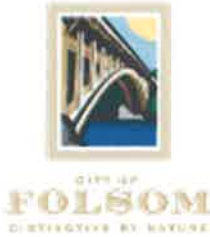
AYES: Councilmember(s):
NOES: Councilmember(s):
ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

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Folsom City Council Staff Report

MEETING DATE:	6/8/2021
AGENDA SECTION:	Consent Calendar
SUBJECT:	Resolution No. 10647 - A Resolution Authorizing the City Manager to Execute an Agreement with Axon Enterprise, Inc. for the Purchase of Police Body-Worn and In-Car Cameras
FROM:	Police Department

RECOMMENDATION / CITY COUNCIL ACTION

Staff recommends the City Council pass and adopt Resolution No. 10647 - A Resolution Authorizing the City Manager to Execute an Agreement with Axon for the Purchase of Police Body-Worn and In-Car Cameras and associated equipment.

BACKGROUND / ISSUE

Body-worn cameras (BWC) and in-car cameras (ICC) provide officers with a reliable and compact tool to systematically and automatically record their field observations and encounters. They can be used for documentation purposes, to include interactions with victims, witnesses, and others during police-public encounters; arrests; and critical incidents. Additionally, body-worn cameras can be used for training and evaluation purposes, allowing agencies to identify and address performance deficiencies with their officers.

Community groups have increasingly called for the Folsom Police Department to implement ICC and BWCs for accountability and transparency in policing. We have piloted BWCs in the past, and they have proven useful for evidence and accountability and were frequently relied upon by officers who carried them.

The implementation of a BWC program would generate a significant workload in terms of public records act requests, and audio/video redaction. Law enforcement agencies in our region that have implemented BWC programs estimate that for each minute of video collected, 4 minutes of staff time is required in order to produce a releasable product.

POLICY / RULE

This purchase is exempt from competitive bids due to Axon being the sole manufacturer, distributor, and retailer of all TASER brand products. A sole source letter is attached.

ANALYSIS

Axon is the manufacturer of TASER, the electronic control device (ECD) deployed by Folsom Police Officers for more than a decade and the ECD of choice for law enforcement agencies across the nation. Axon's firmly established relationship with law enforcement provided them the opportunity to move into the in-car camera (ICC) and body-worn camera (BWC) industry. As technology advanced, Axon began integrating all three technologies, and they now offer a comprehensive "bundle" at per officer pricing.

Generally, body-worn cameras are not a stand-alone technology. Rather, body worn-cameras supplement an in-car camera program. ICCs offer a much more advantageous viewpoint while an officer is seated in their vehicle. In essence, the ICC may capture many of the things an officer sees prior to conducting a traffic stop, during a pursuit, or upon arrival at an incident. These observations are frequently blocked from the officer's body-worn camera by the steering wheel and the officer's arms.

Conversely, once an officer leaves their vehicle, they rarely conduct business in an area directly in front of it. For this reason, the body-worn camera is much more advantageous in capturing in-person contacts, including the verbal communication between an officer and a citizen. Body-worn cameras are still frequently blocked, for instance, by the extended arms of an officer who is in a shooting position. However, these instances are minimized with proper camera placement and solid training practices.

Axon is able to deliver a superior camera bundle by incorporating body-worn camera, in-car camera, and TASER camera (ECD) technologies all integrated with the same software package. Additionally, Axon's per officer annual cost includes installation of ICCs and all necessary training for BWC deployment and use.

FINANCIAL IMPACT

There are many options for implementing a camera program. From full department-wide implementation to a slower buildup and rollout of individual components, Axon is able to build a model that fits budgetary needs. Staff recommends full implementation because all police personnel engage in work and contacts that a camera system should capture.

The cost for full implementation of 72 BWCs, 35 vehicle cameras, 72 Taser 7 (with accessories and docking station), would be \$211,366.90 in the first year and \$257,689.59 per year for the next 4 years, for a total contract amount of \$1,242,125.26. The Police Department will have a small savings over current costs of Tasers. The current cost of Tasers is approximately \$5,629 per year for replacement and new devices, cartridge replacements and batteries. During the contract term these cost would be included in the contract for a savings over the 5 years of \$28,145.

Staff is requesting an agreement in the amount of \$1,242,125.26 be authorized for the purchase of body-worn cameras, in-car cameras and tasers.

The first year of the contract cost has been included in the FY 2021-22 General Fund (Fund 010) Budget in the Police Department as well as funding for 2 part time employees to provide review of the captured video. Years 2 through 5 of the contract will be included in future budgets during the budget process.

ATTACHMENT

Resolution No. 10647 - A Resolution Authorizing the City Manager to Execute an Agreement with Axon Enterprise, Inc. for the Purchase of Police Body-Worn and In-Car Cameras

Submitted,

Richard Hillman, Chief of Police

RESOLUTION NO. 10647

A RESOLUTION AUTHORIZING THE CITY MANAGER TO EXECUTE AN AGREEMENT WITH AXON ENTERPRISE, INC. FOR THE PURCHASE OF POLICE BODY-WORN AND IN-CAR CAMERAS

WHEREAS, the City of Folsom’s mission is to provide a safe, healthy, and vibrant community through innovative, responsive, and effective delivery of services to maintain and enhance the quality of life of our residents; and

WHEREAS, the City of Folsom’s Core Values support innovations; and

WHEREAS, the Police Department believes the program will enhance transparency and strengthen relations between Folsom Police Officers and the community; and

WHEREAS, the agreement with Axon Enterprise, Inc. will be for the not-to-exceed amount of \$1,242,125.26, to be paid over 5 years; and

WHEREAS, this purchase was included in the Fiscal Year 2021-22 General Fund (Fund 010) Budget in the Police Department, and funding is available; and

WHEREAS, the agreement will be in a form acceptable to the City Attorney:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom hereby authorizes the City Manager to execute a purchase agreement with Axon Enterprise, Inc. for the purchase of body-worn cameras.

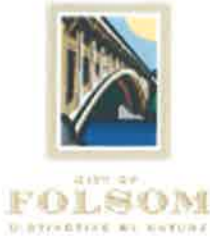
PASSED AND ADOPTED this 8th day of June, 2021, by the following roll-call vote:

- AYES:** Councilmember(s):
- NOES:** Councilmember(s):
- ABSENT:** Councilmember(s):
- ABSTAIN:** Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK



Folsom City Council Staff Report

MEETING DATE:	6/8/2021
AGENDA SECTION:	Public Hearing
SUBJECT:	Resolution No. 10643 – A Resolution Adopting the 2020 Urban Water Management Plan and Water Shortage Contingency Plan and Authorizing the City Manager to Submit the Plans to the California Department of Water Resources and to the California State Library
FROM:	Environmental and Water Resources Department

RECOMMENDATION / CITY COUNCIL ACTION

The Environmental and Water Resources Department recommends the City Council pass and adopt Resolution No. 10643 – A Resolution Adopting the 2020 Urban Water Management Plan and Water Shortage Contingency Plan and Authorizing the City Manager to Submit the Plans to the California Department of Water Resources and to the California State Library.

BACKGROUND / ISSUE

The California Water Code (CWC) Section 10620(a) requires an urban water supplier to prepare and adopt an Urban Water Management Plan (UWMP) consistent with CWC Section 10640. All urban water suppliers, either publicly or privately owned, serving municipal water to 3,000 customers or supplying more than 3,000 acre-feet annually, are required to prepare an UWMP. The UWMP is required for an urban water supplier to be eligible for California Department of Water Resources (DWR) state grants, loans, and drought assistance. The UWMP must be adopted by City Council and submitted to DWR and the California State Library by July 1, 2021. Included in the adoption of the 2020 UWMP shall be a Water Shortage Contingency Plan (WSCP) consistent with the requirements of Senate Bill 606.

The WSCP is a detailed plan for how the City intends to respond to foreseeable and unforeseeable water shortages. A water shortage occurs when the supply is reduced to a level that cannot support the normal demand at any given time or if the state mandates a cutback

regardless of supplies. The intent of this document is to provide guidance to the City's governing body, its staff, and the public by identifying anticipated water shortages and response actions to allow for efficient management of any water shortage with predictability and accountability.

In addition to complying with the CWC, the City must also comply with legislative changes resulting from the November 2009 passage of Senate Bill X7-7. SB X7-7 requires reduction of statewide average per capita daily water consumption by 20% by December 31, 2020 and requires "all water suppliers to increase the efficiency of this essential resource" (CWC Section 10608.4(a)). Based on SB X7-7, the City of Folsom must assess present and proposed future measures, programs, and policies to achieve water use reduction of 20% per capita.

The City previously adopted its 2015 UWMP on June 8, 2016. With the completion of the required public hearing and adoption of the 2020 UWMP by the City Council, the City would remain in compliance with state law. The adoption of the 2020 UWMP is a regulatory requirement by the California Department of Water Resources (DWR) and is independent of the City's Water Supply Management efforts. The 2020 UWMP will include that the City is meeting its 2020 Target gallons per capita per day (GPCD) target as of December 31, 2020.

POLICY / RULE

California Water Code, Section 10620(a):

"Every urban water supplier shall prepare and adopt an urban water management plan in the manner set forth in Article 3 (commencing with Section 10640)."

California Water Code, Section 10621(a):

"Each urban water supplier shall update its plan at least once every five years on or before July 1, in years ending in six and one, incorporating updated and new information from the five years preceding each update."

California Water Code, Section 10632(a):

"Every urban water supplier shall prepare and adopt a water shortage contingency plan as part of its urban water management plan."

California Water Code, Section 10642:

"Prior to the adoption of either, the urban supplier shall make both the urban water management plan and water shortage contingency plan available for public inspection and shall hold a public hearing or hearings thereon. Prior to any of these hearings, notice of the time and place of the hearing shall be published within the jurisdiction of the publicly owned water supplier pursuant to Section 6066 of the Government Code."

The City published a notice of public hearing in the Folsom Telegraph on May 20 and May 27, 2021.

ANALYSIS

Consistent with the requirements of CWC § 10631, the City's 2020 UWMP contains:

1. A description of the City's water service area
2. An assessment of current and projected population
3. An assessment of current and projected supplies
4. An evaluation of the reliability of these supplies given a range of hydrologic conditions considering up to a five-year drought period
5. An assessment of water demands by customer type
6. A description of demand management measures implemented or planned to be implemented by the water supplier
7. An explanation of water management strategies designed to integrate supply and demand conditions
8. Discussion on climate change and any potential impacts to the water supplier
9. A Water Shortage Contingency Plan

In addition to the CWC, the City's 2020 UWMP addresses the Water Conservation Bill of 2009 (SBX7-7) to increase water efficiency and reduce the statewide average per capita water consumption by 20 percent by December 31, 2020. The UWMP identifies the required baseline per capita urban water use, the interim (year 2015) per capita urban water use, and the target (year 2020) per capita urban water use in gallons per capita per day (GPCD).

As detailed in the City's 2010 UWMP, population, residential connections, and water production data were used to generate a gallon per capita day (GPCD) baseline. From this GPCD baseline, the City assessed and determined a 2020 Urban Water Use Target and an Interim 2015 Urban Water Use Target. These values were determined to be 343 and 386, respectively, as presented in the 2010 UWMP.

According to the 2015 DWR Guidebook, a retail water purveyor who did not use actual 2010 Census data must re-calculate its baseline and target GPCD. For the City's 2010 UWMP, the 2010 Census data was not fully available. Thus, the City recalculated its baseline GPCD to re-establish its target and interim-target values with the available 2010 Census data. The comparative results are shown below. As expected, the use of 2010 Census data and the new 2015 DWR population tool did result in changes to the estimated baseline values, and the resulting 2015 Interim GPCD Target and 2020 GPCD Target.

Baseline Period	Baseline Values		2015 Target		2020 Target	
	Original	Revised	Original	Revised	Original	Revised
1996-2005	429	440	386	396	343	352

Extending the population analysis that was revised during the reassessment of the baseline GPCD, the City calculated its 2015 Compliance Value and 2020 Target Value. As demonstrated, the City's 2015 Compliance Value is 246 GPCD, which is significantly below the 2015 Interim GPCD value of 396. In 2020, the City's Compliance Value is 256 GPCD,

also lower than the 2020 Target of 352 GPCD. The table below shows the GPCD numbers for the City from 2010 through 2020.

Year	Water Service Area Population	Gross Water Use (AF)	GPCD
2010	61,187	26,425	386
2011	60,555	26,406	389
2012	61,326	25,510	371
2013	6,2371	26,094	373
2014	63,583	20,515	288
2015	64,966	17,871	246
2016	66,223	19,608	264
2017	67,323	20,404	270
2018	67,498	19,351	256
2019	68,083	18,817	247
2020	69,517	19,898	256

Another key element of the UWMP is to document the City's water supply planning strategies and water shortage contingency planning for the City's existing water service area. This includes a comparison of the total water supply sources available to the City with the total projected water use over the next 25 years, in five-year increments, for a normal water year, a single-dry water year, and multiple dry water years lasting up to five years. As shown in the 2020 UWMP the City will have adequate supplies to meet demands through build-out conditions.

A copy of the 2020 UWMP and WSCP outline are attached. Draft copies of the UWMP and WSCP were made available for review by the public on May 7, 2021 and can be viewed in person by contacting the Environmental and Water Resources Department or City Clerk's office or viewed online by going to the City's website.

FINANCIAL IMPACT

There is no financial impact above the cost to prepare the UWMP.

ENVIRONMENTAL REVIEW

In accordance with the Guidelines for California Environmental Quality Act, Article 19, Section 15306, adoption of the Urban Water Management Plan is Categorical Exempt since it consists of basic data collection, research, and resource evaluation activities that do not result in a serious or major disturbance to an environmental resource.

ATTACHMENTS

1. Resolution No. 10643 – A Resolution Adopting the 2020 Urban Water Management Plan and Water Shortage Contingency Plan and Authorizing the City Manager to

Submit the Plans to the California Department of Water Resources and to the California State Library

2. Outline of the 2020 Urban Water Management Plan
3. Outline of the Water Shortage Contingency Plan

Submitted,

Marcus Yasutake, Director
ENVIRONMENTAL AND WATER RESOURCES DEPARTMENT

ATTACHMENT 1

RESOLUTION NO. 10643**A RESOLUTION ADOPTING THE 2020 URBAN WATER MANAGEMENT PLAN AND WATER SHORTAGE CONTINGENCY PLAN AND AUTHORIZING THE CITY MANAGER TO SUBMIT THE PLANS TO THE CALIFORNIA DEPARTMENT OF WATER RESOURCES AND TO THE CALIFORNIA STATE LIBRARY**

WHEREAS, the Urban Water Management Planning Act requires every urban water supplier providing water for municipal purposes to more than 3,000 customers to adopt and submit a Urban Water Management Plan to the California Department of Water Resources and the California State Library every five years; and

WHEREAS, the City of Folsom wishes to comply with California Water Code (CWC) Section 10610 regarding the preparation of an Urban Water Management Plan; and

WHEREAS, the CWC Section 10620(a) requires an urban water supplier to adopt an Urban Water Management Plan consistent with CWC 10640; and

WHEREAS, the CWC Section 10320(a) requires an urban water supplier to adopt a Water Shortage Contingency Plan consistent with CWC 10640; and

WHEREAS, the City of Folsom is in compliance with Senate Bill X7-7, also known as the Water Conservation Bill of 2009, by reducing per capita water use by 20% by 2020; and

WHEREAS, an adopted Urban Water Management Plan is required for an urban water supplier to be eligible for grants administered by the Department of Water Resources; and

WHEREAS, the City of Folsom has prepared the required plans, published a Notice of Public Hearing pursuant to California Government Code 6066, published May 20 and 27, 2021, and held the appropriate Public Hearing.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Folsom that the 2020 Urban Water Management Plan and Water Shortage Contingency Plan are hereby adopted, subject to minor and typographical edits as deemed necessary by the City Manager.

BE IT FURTHER RESOLVED that the City Manager is hereby authorized to Submit the Plans to the California Department of Water Resources and the California State Library.

PASSED AND ADOPTED this 8th day of June 2021, by the following roll-call vote:

AYES: Councilmember(s):
NOES: Councilmember(s):
ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

ATTACHMENT 2

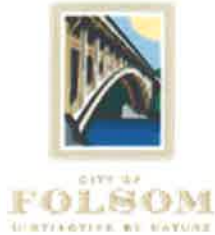
Urban Water Management Plan Organization

- Chapter 1 – Introduction
- Chapter 2 – Plan Preparation
- Chapter 3 – System Description
- Chapter 4 – Water Use Characterization
- Chapter 5 – SB x7-7 Baseline and Targets
- Chapter 6 – Water Supply Characterization
- Chapter 7 – Water Service Reliability and Drought Risk Assessment
- Chapter 8 – Water Shortage Contingency Plan
- Chapter 9 – Demand Management Measures
- Chapter 10 – Plan Adoption, Submittal, and Implementation

ATTACHMENT 3

Water Shortage Contingency Plan Organization

- Chapter 1 – Water Supply Reliability Analysis
- Chapter 2 – Annual Water Supply and Demand Assessment Procedures
- Chapter 3 – Six Shortage Stages
- Chapter 4- Shortage Response Actions
- Chapter 5 – Communication Protocols
- Chapter 6 – Compliance and Enforcement
- Chapter 7 – Legal Authority
- Chapter 8 – Financial Consequences of WSCP Implementation
- Chapter 9 – Monitoring and Reporting
- Chapter 10 – WSCP Refinement Procedures
- Chapter 11 – Special Water Features Distinctions
- Chapter 12 – Plan Adoption, Submittal, and Availability



Folsom City Council Staff Report



MEETING DATE:	6/8/2021
AGENDA SECTION:	Public Hearing
SUBJECT:	Resolution No. 10639 - A Resolution of the City Council of the City of Folsom Approving the Issuance of the California Municipal Finance Authority Multifamily Housing Revenue Bonds in an Aggregate Principal Amount not to Exceed \$20,000,000 to Finance a 110-unit Affordable Rental Housing Facility for Low-income and Very Low-income Seniors, for the Benefit of an Entity to be Created by USA Properties Fund, Inc. or an Affiliate, and Other Certain Matters Relating Thereto
FROM:	Community Development Department

RECOMMENDATION / CITY COUNCIL ACTION

Move to adopt Resolution No. 10639 - A Resolution of the City Council of the City of Folsom Approving the Issuance of the California Municipal Finance Authority Multifamily Housing Revenue Bonds in an Aggregate Principal Amount not to Exceed \$20,000,000 to Finance a 110-unit Affordable Rental Housing Facility for Low-income and Very Low-income Seniors, for the Benefit of an Entity to be Created by USA Properties Fund, Inc. or an Affiliate, and Other Certain Matters Relating Thereto.

BACKGROUND / ISSUE

The Sage at Folsom Apartments (a.k.a. Scholar Senior Apartments) received Planning Commission Planned Development approval on November 18, 2020. The project is a new construction, 100% affordable multifamily rental development and is proposed on the 4.6-acre site located at the northeast corner of the intersection of East Bidwell Street and Scholar Way within the Broadstone Unit No. 3 Specific Plan Area. The Sage project will consist of 110 one-bedroom units affordable to low-, very-low-, and extremely-low-income senior households. On January 26, 2021, the City Council adopted Resolution No. 10583, approving an affordable housing loan, in the amount up to \$3,500,000, to construct the Sage at Folsom Apartments project.

The City Council is being asked to adopt a resolution that would approve the issuance of multifamily housing revenue bonds by the California Municipal Finance Authority ("CMFA"), a joint exercise of powers authority and public entity of the State of California, in an amount not to exceed \$20,000,000, (the "Bonds"), to finance the acquisition, construction, improvement and equipping of a multifamily rental housing project located at the northeast corner of the intersection of East Bidwell Street and Scholar Way, Folsom, California (the "Project").

POLICY / RULE

Section 147(f) of the Internal Revenue Code of 1986 (the "Code") requires that the "applicable elected representatives" of the jurisdiction in which a project is to be financed with "private activity bonds" is situated, adopt a resolution approving the issuance of such "private activity bonds" after holding a public hearing which has been noticed in a newspaper of general circulation in such jurisdiction.

ANALYSIS

USA Properties Fund, Inc. has requested that the CMFA adopt a plan of financing providing for the issuance of exempt facility bonds for a qualified residential rental project pursuant to Section 142(a)(7) of the Internal Revenue Code of 1986 (the "Code") in one or more series in an amount not to exceed \$20,000,000 to finance the acquisition, construction, improvement and equipping of a multifamily rental housing project located at the Northeast corner of Scholar Way & East Bidwell Street, Folsom, California.

The CMFA was created on January 1, 2004 pursuant to a joint exercise of powers agreement to promote economic, cultural and community development, through the financing of economic development and charitable activities throughout California. To date, over 300 municipalities, including the City of Folsom, have become members of CMFA.

In order for all or a portion of the Bonds to qualify as tax-exempt bonds, the City of Folsom must conduct a public hearing under the Tax and Equity Fiscal Responsibility Act ("TEFRA") to allow the members of the community an opportunity to speak in favor of or against the use of tax-exempt bonds for the financing of the Project.

It is important to note that holding a public hearing under the Tax and Equity Fiscal Responsibility Act ("TEFRA") does not obligate the City in any way and has nothing to do with potential funds that the City may or may not contribute to the project.

FINANCIAL IMPACT

The Bonds to be issued by the CMFA for the Project will be the sole responsibility of USA Properties, Inc., and the City will have no financial, legal, or moral obligation, liability or responsibility for the Project or the repayment of the Bonds for the financing of the Project. All financing documents with respect to the issuance of the Bonds will contain clear disclaimers that the Bonds are not obligations of the City or the State of California but are to be paid for solely from funds provided by USA Properties, Inc.

The Board of Directors of the California Foundation for Stronger Communities, a California non-profit public benefit corporation (the "Foundation"), acts as the Board of Directors for the CMFA. Through its conduit issuance activities, the CMFA shares a portion of the issuance fees it receives with its member communities and donates a portion of these issuance fees to the Foundation for the support of local charities. With respect to the City of Folsom, it is expected that a portion of the issuance fee attributable to the City will be granted by the CMFA to the general fund of the City. Such grant may be used for any lawful purpose of the City.

ENVIRONMENTAL REVIEW

This action to approve the issuance of these bonds by the City Council is exempt from environmental review pursuant to Section 15061(b)(3) (Review for Exemption) of the California Environmental Quality Act. An Initial Study, Mitigated Negative Declaration, and Mitigation Monitoring and Reporting have been prepared in accordance with the requirements of the California Environmental Quality Act.

ATTACHMENT

Resolution No. 10639 - A Resolution of the City Council of the City of Folsom Approving the Issuance of the California Municipal Finance Authority Multifamily Housing Revenue Bonds in an Aggregate Principal Amount not to Exceed \$20,000,000 to Finance a 110-unit Affordable Rental Housing Facility for Low-income and Very Low-income Seniors, for the Benefit of an Entity to be Created by USA Properties Fund, Inc. or an Affiliate, and Other Certain Matters Relating Thereto

Submitted,



Pam Johns, Community Development Director

RESOLUTION NO. 10639

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF FOLSOM
 APPROVING THE ISSUANCE OF REVENUE BONDS BY THE
 CALIFORNIA MUNICIPAL FINANCE AUTHORITY IN AN
 AGGREGATE PRINCIPAL AMOUNT NOT TO EXCEED \$20,000,000 TO
 FINANCE A 110-UNIT AFFORDABLE RENTAL HOUSING FACILITY
 FOR LOW-INCOME AND VERY LOW-INCOME SENIORS, FOR THE
 BENEFIT OF AN ENTITY TO BE CREATED BY USA PROPERTIES
 FUND, INC. OR AN AFFILIATE, AND CERTAIN OTHER MATTERS
 RELATING THERETO**

WHEREAS, USA Properties Fund, Inc, (the “Sponsor”), on behalf of an entity to be created by the Sponsor or an affiliate of the Sponsor (such entity, the “Borrower”), has requested that the California Municipal Finance Authority (the “Authority”) issue one or more series of revenue bonds in an aggregate principal amount not to exceed \$20,000,000 (the “Bonds”), including but not limited to revenue bonds issued as part of a plan to (1) finance and refinance the acquisition, construction, improvement and equipping of a 110-unit affordable rental housing facility for low-income and very low-income seniors (the “Project”) to be owned and operated by the Borrower and located at 89 Scholar Way within the City of Folsom (the “City”), and (2) pay certain expenses in connection with the issuance of the Bonds; and

WHEREAS, an “applicable elected representative” of a jurisdiction in which the Project is to be located is required to approve the issuance of the Bonds under Section 147(f) of the Code; and

WHEREAS, the City Council of the City (the “City Council”) is the elected legislative body of the City and is an “applicable elected representative” for purposes of Section 147(f) of the Code; and

WHEREAS, the Authority has requested that the City Council approve the issuance of the Bonds by the Authority in order to satisfy the public approval requirement of Section 147(f) of the Code and the requirements of Section 4 of the Joint Exercise of Powers Agreement Relating to the California Municipal Finance Authority, dated as of January 1, 2004 (the “Agreement”), among certain local agencies, including the City; and

WHEREAS, pursuant to Section 147(f) of the Code, the City Council has, following notice duly given, held a public hearing regarding the issuance of the Bonds, and now desires to approve the issuance of the Bonds by the Authority.

NOW, THEREFORE, BE IT RESOLVED, by the City Council of the City of Folsom as follows:

Section 1. The foregoing recitals are true and correct.

Section 2. The City Council hereby approves the issuance of the Bonds by the Authority, including but not limited to Bonds issued as part of a plan to finance and refinance the facilities

described herein. It is the purpose and intent of the City Council that this resolution constitute approval of the issuance of the Bonds by the Authority for the Project, for the purposes of (a) Section 147(f) of the Code by the applicable elected representative of a governmental unit having jurisdiction over the area in which the Project is to be located, in accordance with said Section 147(f) and (b) Section 4 of the Agreement.

Section 3. The officers of the City are hereby authorized and directed, jointly and severally, to do any and all things and to execute and deliver any and all documents which they deem necessary or advisable in order to carry out, give effect to and comply with the terms and intent of this resolution and the financing transaction approved hereby.

Section 4. The Clerk shall forward a certified copy of this Resolution to the Authority in care of its counsel:

Ronald E. Lee, Esq.
Jones Hall, APLC
475 Sansome Street, Suite 1700
San Francisco, CA 94111

Section 5. This resolution shall take effect immediately upon its adoption.

PASSED AND ADOPTED this 8th day of June 2021, by the following roll-call vote:

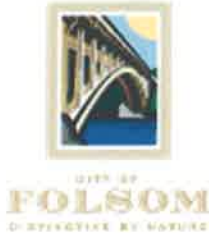
AYES: Councilmember(s):
NOES: Councilmember(s):
ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

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Folsom City Council Staff Report



MEETING DATE:	6/8/2021
AGENDA SECTION:	Public Hearing
SUBJECT:	<p>Folsom Ranch Medical Center Development Agreement Amendment – Northeast corner of East Bidwell Street and Alder Creek Parkway (PN 20-193)</p> <p>i. Ordinance No. 1314 - An Uncodified Ordinance of the City of Folsom Approving Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement between the City of Folsom and Dignity Health Relative to the Folsom Ranch Medical Center Project (Introduction and First Reading)</p>
FROM:	Community Development Department

RECOMMENDATION / CITY COUNCIL ACTION

Move to introduce and conduct first reading of Ordinance No. 1314 - An Uncodified Ordinance of the City of Folsom Approving Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement between the City of Folsom and Dignity Health relative to the Folsom Ranch Medical Center Project (Introduction and First Reading).

BACKGROUND / ISSUE

On May 19, 2021, the Planning Commission held a public hearing to consider a request from Dignity Health for adoption of an Addendum to the Folsom Plan Area Specific Plan EIR/EIS and approval of a Planned Development Permit, Conditional Use Permit, and Development Agreement Amendment for development of a 530,000-square-foot medical center (Folsom Ranch Medical Center) on a 27.44-acre site located at the northeast corner of the intersection of East Bidwell Street and Alder Creek Parkway within the Folsom Plan Area. The Planning Commission staff report and associated Conditions of Approval are included at Attachment 3 to this staff report.

The Planning Commission adopted a motion (7-0-0-0) to adopt the Addendum to the FPASP EIR/EIS, approve a Planned Development Permit, and approve a Conditional Use Permit for the Folsom Ranch Medical Center project. The Commission also moved to recommend that the City Council Approve Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement Relative to the Folsom South Specific Plan for the Folsom Ranch Medical Center project.

POLICY / RULE

As set forth in the State Planning and Zoning Law, approval of, or amendments to, a Development Agreement is a legislative act which requires approval by the City Council following review and recommendation by the Planning Commission.

PROJECT ENTITLEMENTS

As noted above, the applicant requested approval of three entitlements and adoption of an Addendum to allow for development of the proposed Medical Center.

The first entitlement was a request for approval of a Planned Development Permit which contains specific development and architectural standards for the proposed 530,000-square-foot Medical Center. Planned Development Guidelines were submitted which provide the framework for the Planned Development Permit including deviations from the development standards established by the Folsom Plan Area Specific Plan and the Folsom Municipal Code in order to accommodate an acute care hospital, medical office buildings, a heliport, site design and planning, building architecture, landscaping, site lighting, and signage. **This entitlement was approved by the Planning Commission on May 19, 2021.**

The second entitlement requested was for approval of a Conditional Use Permit for development and operation of a ground-level heliport within the western portion of the project site, near the hospital's emergency department ambulance entrance. The heliport, which will be designed and constructed with the second phase of the Medical Center (approximately 2028), is intended to accommodate patient transport to the hospital for emergency care and also to transport patients to other area hospitals where a higher level of emergency care is available. The heliport will feature a broom finish concrete landing area, an eight-foot-wide connecting concrete pathway, and pavement markings, signage, and lighting as required by the California Department of Transportation Aviation Division and the Federal Aviation Administration. **This entitlement was also approved by the Planning Commission on May 19, 2021.**

The third entitlement requested was for approval of a Development Agreement Amendment to the First Amended and Restated Tier 1 Development Agreement to incorporate in the definition of "entitlements" that are vested the entitlements requested by the project applicant including the Planned Development Permit, the Planned Development Guidelines, the Conditional Use Permit, and the Site Plan. The Development Agreement Amendment also intended to account for the longer construction timeline for the project, memorialize

agreements relative to project site access, memorialize agreements relative to water infrastructure and connections, memorialize agreements relative to street and landscape frontage maintenance, memorialize agreements relative to preparation for the future Class I trail, and acknowledge the potential for future modifications to the Medical Center that may be required by the California Office of Statewide Health Planning and Development. **The Planning Commission voted on May 19, 2021 to recommend that the City Council approve the proposed amendment to the development agreement.**

PROJECT OVERVIEW

The proposed Medical Center includes development of a six-story 400,000-square-foot acute care hospital with 300 beds, two three-story 65,000-square-foot medical office buildings, a 20,000-square-foot central utility plant, and a ground-level heliport facility. With respect to building design, the applicant has submitted Planned Development Guidelines which are intended to provide the architectural framework for development of the Medical Center buildings including guidance relative to building design, building materials, and building colors. Each of the Medical Center buildings will require future Design Review approval by the Planning Commission to ensure consistency with the proposed Folsom Ranch Medical Center Planned Development Guidelines.

The Medical Center is proposed to be developed in five phases over an approximate 23-year period. The table below lists the specific details regarding each of the five phases of development for the Medical Center:

TABLE 1: FOLSOM RANCH MEDICAL CENTER PHASING EXHIBIT

Phase	Year	Building	Central Plant*	Occupied Area
Phase 1	2023	Medical Office Building 1	NA	65,000 SF
Phase 2	2028	Hospital (100 Beds)	15,000 SF*	160,000 SF
Phase 2	2028	Heliport	NA	NA
Phase 3	2030	Medical Office Building 2	NA	65,000 SF
Phase 4	2034	Hospital Expansion (100 Beds)	NA	120,000 SF
Phase 5	2045	Hospital Expansion (100 Beds)	5,000 SF	120,000 SF
Total Area			20,000 SF*	530,000 SF

*Central Utility Plant areas are estimated and not included in total development area

The proposed development phases and timing are flexible and are based on current estimates that may vary as the Medical Center is developed and population and patient needs dictate. Another factor impacting development timing is the completion of site improvement work and coordination with construction seasons given the infrastructure and roads needed to serve the project.

At their May 19, 2021 meeting, the Planning Commission engaged in a thorough evaluation of the proposed project. Ultimately, their discussion and action focused on four specific topics listed and described below:

1. Site Access

The Commission expressed support for the future installation of a traffic signal at the intersection of Alder Creek Parkway and McCarthy Way subject to completion of an updated Traffic Impact Analysis. Furthermore, the Commission indicated that they would be in favor of installation of the traffic signal earlier than the anticipated date (Phase 4/Approximately 2034) provided by the applicant if traffic conditions warrant the earlier installation. It is important to acknowledge that the proposed Development Agreement Amendment allows the applicant to consider installation of the traffic signal prior to Phase 4 if traffic conditions require earlier installation.

2. Signage

The Commission expressed concern regarding a proposed 80-foot-tall freestanding pylon sign (750 square feet of sign area) intended to be located in the northwest corner of the project site adjacent to U.S. Highway 50. Specifically, the Commission did not believe that it was appropriate for a hospital/medical use to have a freeway-oriented pylon sign as this type of sign is typically utilized by retail and commercial uses (Folsom Auto Mall, Palladio at Broadstone, etc.) for branding purposes. As a result, the Commission recommended that the freestanding pylon sign not be permitted and that it be removed from the Folsom Ranch Medical Center Planned Development Guidelines.

3. Site Preparation for Class I Bike Path

The proposed Development Agreement Amendment dictates that the project applicant be responsible for rough grading and installation of a retaining wall in preparation for construction of a future Class I Bicycle Path within the northwest corner of the project site, and that this site preparation work be completed no later than issuance of a building permit for the second expansion of the hospital (Phase 4/2034). The Commission was apprehensive that the wording of the Development Agreement Amendment made it appear that the applicant was seeking to delay the site preparation work associated with the future Class I Bicycle Path until Phase 4 of the project. To address this concern, the Commission recommended that the Development Agreement Amendment (and associated condition of approval) language be updated to indicate that the applicant and the City will cooperate on timing the grading and construction of the retaining wall to coincide with phased construction of the project, which may occur earlier than Phase 4 if the City has identified funding for the Class I Bicycle Path prior to Phase 4.

4. Bicycle Parking

As part of their proposal, the applicant was seeking approval to utilize the bicycle parking requirements established by Folsom Municipal Code (less restrictive) rather than the bicycle parking requirements from the Folsom Plan Area Specific Plan, as they indicated that the parking requirements of the Folsom Municipal Code were more reflective of their experiences with bicycle usage and bicycle parking at their other medical centers in the region. The Commission voiced concern that the proposed project did not provide a sufficient amount of bicycle parking spaces (56 bicycle parking spaces proposed) to serve the medical center. The Commission noted that Folsom has an expansive system of bicycle paths and trails and that bicycle usage has and will continue to grow within the community. To address this concern, the Commission modified a condition of approval to require the proposed project to utilize the bicycle parking requirements (99 bicycle parking spaces required) established by the Folsom Plan Area Specific Plan.

One member of the public spoke regarding the proposed project and three members of the public provided letters which were read into the record. The public comments were generally focused on the Class I Bicycle Path and ensuring that the grading and construction of the retaining wall for the future trail would be completed in a timely manner. The Planning Commission adopted a motion (7-0-0-0) to Adopt the Addendum to the FPASP EIR/EIS, Approve a Planned Development Permit, and Approve a Conditional Use Permit for the Folsom Ranch Medical Center project. The Commission also moved to recommend that the City Council Approve Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement Relative to the Folsom South Specific Plan for the Folsom Ranch Medical Center project. It is important to note that Dignity Health was in agreement to the modifications proposed at the Planning Commission meeting and has not objected to nor filed an appeal on any of the conditions of approval placed on the project.

ANALYSIS

The City and Landowner's predecessor (Eagle Commercial Partners, LLC) previously entered into the First Amended and Restated Tier 1 Development Agreement By and Between the City of Folsom and Landowner Relative to the Folsom South Specific Plan on July 15, 2014. Section 1.5 of the Restated Development Agreement allows the Restated Development Agreement to be amended from time to time by mutual written consent of the parties. On November 12, 2015, Eagle Commercial Partners, LLC and the City entered into Amendment No. 1 to First Amended and Restated Tier 1 Development Agreement Relative to the Folsom South Specific Plan. The aforementioned development agreements were assigned by Eagles Commercial Partners, LLC to Dignity Health when the subject property was purchased in June of 2020. The applicant is proposing Amendment No. 2 to the First Amended and Restated Development Agreement by and between the City of Folsom and Dignity Health.

The applicant is requesting to further amend the Development Agreement to incorporate in the definition of “entitlements” the entitlements considered and approved by the Planning Commission: Planned Development Permit, Planned Development Guidelines, Conditional Use Permit, and the Site Plan. The Development Agreement amendment also seeks to take into account the longer construction timeline for the project, memorialize agreements relative to project site access, memorialize agreements relative to water infrastructure and connections, memorialize agreements relative to frontage landscaping and maintenance, memorialize agreements relative to preparation for the future Class I trail, and acknowledge the potential for future modifications to the Medical Center that may be required by the California Office of Statewide Health Planning and Development.

One of the primary purposes of this Development Agreement amendment is to provide flexibility in the approval and term for subsequent entitlements, specifically in relation to development of the Folsom Ranch Medical Center. Staff has determined that this flexibility is warranted because development of the Medical Center presents significant benefits to the City and the region and there are unique characteristics in the buildout of the Medical Center buildings and related structures including all of the following:

- Development of the Medical Center will occur over a long period of time, with a phased timeline for construction and potential adjustments to physical structures as medical delivery systems change over time;
- The California Office of Statewide Health Planning and Development (“OSHPD”) is required to approve the hospital building design which may result in required changes to the design of the Medical Center buildings and related structures;
- The Medical Center will generate significant employment and other economic benefits to the City;
- The Medical Center will provide needed expansion of access to health care services for the City and other jurisdictions in the region;
- A significant capital investment is required for the Medical Center buildings and related structures; and
- The status of Landowner as a nonprofit public benefit corporation.

In light of the unique circumstances associated with the proposed Medical Center referenced above, the term of the Development Agreement amendment is proposed to be extended to June 30, 2056 or until the 530,000-square-foot Medical Center has been built out, whichever is later.

As mentioned previously, the Development Agreement amendment also seeks to address issues related to site access, agreements relative to water infrastructure and connections, agreements relative to street lighting and landscape frontage maintenance, and agreements

relative to preparation for the future Class I trail. Each of the specific elements of the Development Agreement amendment referenced above are discussed in detail within various sections of the Planning Commission Staff Report (Attachment 3). City staff has conducted a thorough review of the proposed modifications to the Development Agreement and is supportive of the Development Agreement amendment as proposed by the applicant.

FINANCIAL IMPACT

No financial impact is anticipated with approval of the Development Agreement amendment associated with the Folsom Ranch Medical Center Project as the project will not result in any change in the total amount of commercial square footage or residential unit count within the Folsom Plan Area.

ENVIRONMENTAL REVIEW

An Addendum to the Final EIR/EIS for the Folsom Plan Area Specific Plan was previously approved for the Folsom Ranch Medical Center project in accordance with the California Environmental Quality Act. The Development Agreement Amendment does not result in substantial changes to the project, and no additional environmental review is required.

ATTACHMENTS

1. Ordinance No. 1314 - An Uncodified Ordinance of the City of Folsom Approving Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement between the City of Folsom and Dignity Health relative to the Folsom Ranch Medical Center Project (First Reading and Introduction)
2. Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement by and between the City of Folsom and Dignity Health relative to the Folsom Ranch Medical Center Project
3. Planning Commission Staff Report, dated May 19, 2021

Submitted,



PAM JOHNS
Community Development Director

Attachment 1

**Ordinance No. 1314 – An Uncodified Ordinance of the City
of Folsom Approving Amendment No. 2 to the First
Amended and Restated Tier 1 Development Agreement
between the City of Folsom and Dignity Health relative to the
Folsom Ranch Medical Center Project
(Introduction and First Reading)**

ORDINANCE NO. 1314

**AN UNCODIFIED ORDINANCE OF THE CITY OF FOLSOM APPROVING
AMENDMENT NO. 2 TO THE FIRST AMENDED AND RESTATED TIER 1
DEVELOPMENT AGREEMENT BETWEEN THE CITY OF FOLSOM AND DIGNITY
HEALTH RELATIVE TO THE FOLSOM RANCH MEDICAL CENTER PROJECT**

WHEREAS, a Final Environmental Impact Report/Environmental Impact Statement for the Folsom Plan Area Specific Plan was prepared and certified by the City Council on June 11, 2011, and the Sacramento Local Agency Formation Commission approved the City's annexation of the Folsom Plan Area on January 18, 2012; and

WHEREAS, pursuant to the authority in Sections 65864 through 65869.5 of the Government Code, the City Council, following a duly notified public hearing on June 28, 2011, approved the Tier 1 Development Agreement relative to the Folsom South Specific Plan (Tier 1 DA) for the development of the Folsom Plan Area by adopting Ordinance No. 1149 on July 12, 2011; and

WHEREAS, the City Council, following a duly noticed public hearing on May 27, 2014, approved a request to amend the Tier 1 DA to the development of the Westland/Eagle Project by approving a First Amended and Restated Tier 1 Development Agreement (ARDA) between the City and the developer of the Westland/Eagle Project, Eagle Commercial Partners, LLC, by adopting Ordinance No. 1204 on June 10, 2014; and

WHEREAS, the City Council, following a duly noticed public hearing on September 22, 2015, approved a request to amend the ARDA to the development of the Westland/Eagle Project by approving Amendment No. 1 to ARDA between the City and the developer of the Westland/Eagle Project, Eagle Commercial Partners, LLC, by adopting Ordinance No. 1237 on October 13, 2015; and

WHEREAS, the proposed Folsom Ranch Medical Center Project consists of the development of an 530,000-square-foot medical center on a 27.44-acre site located at the northeast corner of the intersection of East Bidwell Street and Alder Creek Parkway within the Folsom Plan Area; and

WHEREAS, the City and the landowner/developer of the Folsom Ranch Medical Center Project desire to further amend the ARDA in order to provide greater certainty and clarity to matters that are common, necessary and essential for the development of the project; and

WHEREAS, the Planning Commission, at its regular meeting on May 19, 2021, considered Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement by and between the City of Folsom and Dignity Health relative to the Folsom Ranch Medical Center project at a duly noticed public hearing as prescribed by law, and recommended that the City Council approve said Amendment No. 2; and

WHEREAS, all notices have been given at the time and in the manner required by State Law and the Folsom Municipal Code.

NOW, THEREFORE, the City Council of the City of Folsom hereby does ordain as follows:

SECTION 1 FINDINGS

- A. The above recitals are true and correct and incorporated herein by reference.
- B. The Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement by and between the City of Folsom and Dignity Health consistent with the objectives, policies, general land uses and programs specified in the City's General Plan and the Folsom Plan Area Specific Plan.
- C. The Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement is in conformity with public convenience, general welfare, and good land use practices.
- D. The Amendment No. 2 will not be detrimental to the health, safety, and general welfare of persons residing in the immediate area, nor be detrimental or injurious to property or persons in the general neighborhood or to the general welfare of the residents of the City as a whole.
- E. The Amendment No. 2 will not adversely affect the orderly development of property or the preservation of property values.
- F. The Amendment No. 2 has been prepared in accordance with, and is consistent with, Government Code Sections 65864 through 65869.5, and City Council Resolution No. 2370.
- G. All notices have been given at the time and in the manner required by State Law and the Folsom Municipal Code.
- H. The Amendment No. 2 is consistent with the Environmental Impact Report/Environmental Impact Statement for the Folsom Plan Area Specific Plan certified by the City Council on June 11, 2011 and the 2021 Folsom Ranch Medical Center Addendum, which are incorporated herein by reference. None of the events in Sections 15162 and 15163 of the CEQA Guidelines exists which warrant the preparation of a subsequent EIR or supplemental EIR.

SECTION 2 APPROVAL OF AMENDMENT TO DEVELOPMENT AGREEMENT

The Mayor is hereby authorized and directed to execute the Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement by and between the City of Folsom and Dignity Health on behalf of the City after the effective date of this Ordinance.

SECTION 3 SEVERABILITY

If any section, subsection, sentence, clause, or phrase in this Ordinance or any part thereof is for any reason held to be unconstitutional, invalid, or ineffective by any court of competent jurisdiction, such decision shall not affect the validity or effectiveness of the remaining portions of this Ordinance or any part thereof. The City Council declares that it would have passed each section irrespective of the fact that any one or more section, subsection, sentence, clause, or phrase be declared unconstitutional, invalid, or ineffective.

SECTION 4 EFFECTIVE DATE

This Ordinance shall become effective thirty (30) days from and after its passage and adoption, provided it is published in full or in summary within twenty (20) days after its adoption in a newspaper of general circulation in the City.

This Ordinance was introduced and the title thereof read at the regular meeting of the City Council on June 8, 2021 and the second reading occurred at the regular meeting of the City Council on June 22, 2021.

On a motion by Council Member _____ seconded by Council Member _____, the foregoing ordinance was passed and adopted by the City Council of the City of Folsom, State of California, this 22nd day of June 2021, by the following roll-call vote:

- AYES:** Councilmember(s):
- NOES:** Councilmember(s):
- ABSENT:** Councilmember(s):
- ABSTAIN:** Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

Attachment 2

Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement between the City of Folsom and Dignity Health relative to the Folsom Ranch Medical Center Project

FOR THE BENEFIT OF THE CITY OF FOLSOM
PURSUANT TO GOVERNMENT CODE §6103

RECORDING REQUESTED BY CITY CLERK

WHEN RECORDED MAIL TO:

City Clerk
City of Folsom
50 Natoma Street
Folsom, CA 95630

(SPACE ABOVE THIS LINE RESERVED FOR RECORDER'S USE)

**AMENDMENT NO. 2 TO
FIRST AMENDED AND RESTATED TIER 1 DEVELOPMENT
AGREEMENT
RELATIVE TO FOLSOM SOUTH SPECIFIC PLAN
(DIGNITY HEALTH)**

**AMENDMENT NO. 2 TO
FIRST AMENDED AND RESTATED TIER 1 DEVELOPMENT AGREEMENT
RELATIVE TO FOLSOM SOUTH SPECIFIC PLAN
(DIGNITY HEALTH)**

This Amendment No. 2 to First Amended and Restated Development Agreement (“Amendment No. 2”) is entered into this ___ day of _____, 2021, by and between the City of Folsom (“City”) and Dignity Health, a California nonprofit public benefit corporation (“Landowner”) pursuant to the authority of Sections 65864 through 65869.5 of the Government Code of California. All capitalized terms used herein and not otherwise defined herein shall mean and refer to those terms as defined in Section 1.3 of the Restated Development Agreement and Amendment No. 1 to the Restated Development Agreement, described below between the Predecessor in Interest to Landowner and the City.

RECITALS

A. Restated Development Agreement and Amendments Thereto. The City and Landowner’s predecessor in interest Eagle Commercial Partners, LLC (referred to herein as the “Predecessor in Interest”) previously entered into that certain First Amended and Restated Tier 1 Development Agreement By and Between the City of Folsom and Landowner Relative to the Folsom South Specific Plan, recorded on July 15, 2014, in the Official Records of the County Recorder of Sacramento County in Book 20140715, Page 0517 (the “Restated Development Agreement”). Section 1.5 of the Restated Development Agreement allows the Restated Development Agreement to be amended from time to time by mutual written consent of the parties. On November 12, 2015, Eagle Commercial Partners, LLC and the City entered into Amendment No. 1 to First Amended and Restated Tier 1 Development Agreement Relative to the Folsom South Specific Plan, recorded on January 29, 2016, in the Official Records of the County Recorder of Sacramento County in Book 0160129, Page No. 0385 (“Amendment No. 1”). The Restated Development Agreement and Amendment No. 1 are collectively referred to herein as the “Development Agreement.”

B. Conveyance of Property to Landowner and Assignment of Development Agreement. Predecessor in Interest conveyed the Property to its affiliate, Enclave at Folsom Ranch, LLC (“Predecessor Affiliate”) which in turn conveyed the Property identified by legal description on **Exhibit A-1** and depicted on **Exhibit A-2** to Landowner on June 5, 2020. Concurrently with such conveyance, Predecessor in Interest entered into an Assignment and Assumption Agreement Relative to The Folsom South Specific Plan Amended and Restated Tier 1 Development Agreement, pursuant to a form approved and required by the City, recorded on June 5, 2020, in the Official Records of the County Recorder of Sacramento County as Document Number 202006050658 (“Assignment and Assumption Agreement”). The Assignment and Assumption Agreement transferred all rights, title, interest, burdens and obligations of the

Predecessor in Interest under the Development Agreement with respect to the Property to Landowner.

C. Prior City Determinations and Approvals Relative to the Property. The Property is identified as "Parcel 1" on a Parcel Map approved by the City Planning Commission on December 4, 2019 (PN 19-389). Parcel 1 is one of four parcels created by the subdivision of the property identified as Parcel 85A in the Specific Plan. The Final Parcel Map including Parcel 1 was approved by the City Council and thereafter filed for record on May 22, 2020 in Book 240, Page 13 of Parcel Maps, Sacramento County. Prior to approval of the subdivision of Parcel 85A, on March 17, 2020, the Community Development Director for the City issued approval of a Minor Administrative Modification ("MAM") associated with Parcels 61, 77, 78, and 85A (PN 20-003). The MAM provided for the transfer of certain residential units and gross square footage within the four parcels and remains in effect. The Community Development Director thereafter issued a second letter on April 1, 2020, confirming that Table A-7 of the Specific Plan contained a clerical error regarding allowed land uses for General Commercial (GC) and Regional Commercial (RC) for several parcels, including Parcel 85A (hereafter the "Table A-7 Correction"). The allowed land uses in the Table A-7 Correction remain in effect.

D. Subsequent Entitlements. On May 19, 2021, the City Planning Commission, in a duly noticed and conducted public hearing, approved the Subsequent Entitlements for the Development of the Property as follows:

1. Planned Development Permit (including the Site Improvements and all Amendments to the Planned Development Permit submitted and considered as of the date of the hearing).
2. Planned Development Guidelines.
3. Conditional Use Permit.
4. Site Plan, as depicted on **Exhibit B** (Except parcels marked "Future Multifamily Housing" and "Future Hotel." Other than the Site Improvements, development on Parcels marked "Future Multifamily Housing" and "Future Hotel" (Parcels 2-4 on Exhibit A-2) are not part of the Subsequent Entitlements and shall not be vested).

The Planning Commission further recommended for approval by the City Council of this Amendment No. 2 to the Development Agreement.

E. Purpose of Amendment; Findings Related to Medical Center. Landowner is processing Subsequent Entitlements for the Development of the Property for comprehensive medical uses, specifically two medical office buildings, an acute care hospital and related structures (as further defined in Section 1.3 of this Amendment No.

2 and hereafter the "Medical Center"). Landowner has requested that the Subsequent Entitlements, including the Conditions of Approval related thereto, as approved by the City, be included in the definition of Entitlements as that term is used throughout the Development Agreement, pursuant to Section 1.5.3 of the Development Agreement. The City and Landowner also intend to amend certain provisions of the Development Agreement to allow for flexibility in the approval of and term for the Subsequent Entitlements, specifically in relation to the development of a Medical Center. The City Council has determined that flexibility is required for the Subsequent Entitlements because development of the Medical Center presents significant benefits to the City and the region and unique characteristics in the buildout of the Medical Center buildings and related structures, as follows:

1. Development of the Medical Center will occur over a long period of time, with a phased timeline for construction and potential adjustments to physical structures as medical delivery systems change over time;
2. The California Office of Statewide Health Planning and Development ("OSHPD") is required to approve the hospital building design which may result in required changes to the design of the Medical Center buildings and related structures;
3. The Medical Center will generate significant employment and other economic benefits to the City;
4. The Medical Center will provide needed expansion of access to health care services for the City and other jurisdictions in the region;
5. A significant capital investment is required for the Medical Center buildings and related structures; and
6. The status of Landowner as a nonprofit public benefit corporation.

F. Property. The subject of this Amendment No. 2 is the Development of the Property, as defined in Section 1.3. Landowner owns the Property and represents that all persons holding legal or equitable interests in the Property shall be bound by this Amendment No. 2 and the Development Agreement.

G. Hearings. On May 19, 2021, the City Planning Commission, designated as the planning agency for purposes of development agreement review pursuant to Government Code section 65867, in a duly noticed and conducted public hearing, considered this Amendment No. 2 and recommended that the City Council approve this Amendment No. 2 to the Development Agreement. On June 8, 2021, the City Council, in a duly noticed and conducted public hearing, conducted the first reading of Ordinance No. 1314 and approved this Amendment No. 2, and thereafter conducted the second

reading of Ordinance No. 1314 at a duly noticed regular meeting of the City Council on _____, 2021 and adopted the Ordinance approving this Amendment No. 2.

H. Environmental Review. On May 19, 2021, the Planning Commission considered the Environmental Checklist and Addendum Dignity Health Folsom Ranch Medical Center (the "Addendum") to the Specific Plan EIR/EIS for Development of the Property consistent with the Specific Plan. An Initial Study prepared in support of the Addendum identified mitigation measures to reduce environmental impacts to less than significant, and those mitigation measures have been incorporated into the Project and the Subsequent Entitlements, as reflected by the findings adopted by the Planning Commission in connection with the approval of the Subsequent Entitlements and the City Council's consideration, adoption of findings, and approval of this Amendment No. 2.

I. No New Impacts Associated with Approval of Amendment. The City Council has determined that the adoption of this Amendment No. 2 involves no new impacts not considered in the Specific Plan EIR, the Previous Environmental Analyses listed in Section 1.2 of the Addendum, and the Addendum; therefore, no further environmental documents relating to the adoption of this Amendment No. 2 are required.

J. Consistency with General Plan and Specific Plan. Having duly examined and considered this Amendment No. 2, the City finds and declares that this Amendment No. 2 is consistent with the General Plan and the Specific Plan.

NOW, THEREFORE, the parties hereto, in consideration of the mutual covenants, promises, and agreements herein contained, and for other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged and agreed, the parties agree to hereby amend the Development Agreement as follows:

AMENDMENTS

1. Incorporation of Recitals. The Recitals above are true and correct and constitute enforceable provisions of this Amendment No. 2.
2. Definition and Section 1.5.3 – Subsequent Entitlements. The term "Subsequent Entitlements" in the Definitions Section of the Restated Development Agreement and also referenced in Section 1.5.3 of the Restated Development Agreement and later updated by Amendment No. 1 is amended to include the following:
 - a. The MAM approved by the Community Development Director on March 17, 2020.
 - b. The Table A-7 Correction issued by the Community Development Director on April 1, 2020.

- c. The Planned Development Permit (including the Site Plan for Parcel 1 and all amendments to the Planned Development Permit and Planned Development Guidelines); the Conditional Use Permit; the Planned Development Guidelines and this Amendment No. 2 approved by the Planning Commission and the City Council on May 19, 2021 and June 22, 2021 respectively.
- d. Landowner acknowledges and agrees that, in addition to design features described in the Planned Development Guidelines and approved by the Planning Commission as described in Recital D, supplementary design detail for individual buildings, related structures and the heliport will be presented to the City as required by the Specific Plan and the Folsom Municipal Code by Landowner for review and approval by the City. As this additional design detail is presented to the City, Landowner shall prepare Design Guidelines for the Medical Center for review and approval by the City, which may be updated as phases of Development of the Project continue. Upon approval of the Design Guidelines by the City (or sections thereof being amended and approved by the City from time to time), such Design Guidelines shall be a Subsequent Entitlement without the necessity of further amendment to this Amendment No. 2 or the Development Agreement.

3. **Section 1.3 – Definitions.** The following Definitions are added:

“Adopting Ordinance” means Ordinance No. _____, dated _____, approving this Amendment No.2.

“Addendum” means the Environmental Checklist and Addendum, Dignity Health Folsom Ranch Medical Center, dated April 2021.

“Conditional Use Permit” means the Conditional Use Permit approved by the City pertaining to the Project.

“Design Guidelines” means the design guidelines for the Medical Center approved by the City, and thereafter to be applied by the City to guide and evaluate the design of the Medical Center.

“Effective Date” means the date which is thirty (30) calendar days after the date of the Ordinance approving this Amendment No. 2.

“Environmental Analysis” means the Addendum and the Previous Environmental Analyses listed in Section 1.2 of the Addendum.

“Landowner” means Dignity Health, a California nonprofit public benefit corporation.

“Planned Development Permit” and “Planned Development Guidelines” include the Planned Development Permit and Planned Development Guidelines approved by the Planning Commission pertaining to the Project.

“Medical Center” means the buildings and related structures, including but not limited to the hospital and two medical office buildings, in which health care, research, staff support and ancillary services are provided (including the heliport) both temporary or permanent buildings or structures, and whether considered supportive or ancillary that are identified on the Planned Development Permit and described in the Planned Development Guidelines and the Conditional Use Permit.

“Predecessor in Interest” shall mean the prior owner of the Property and party to the Restated Development Agreement and Amendment No. 1, i.e., Eagle Commercial Partners, LLC.

“Predecessor Affiliate” shall mean the affiliate of Predecessor in Interest, Enclave at Folsom Ranch, LLC.

“Project” means development of the Medical Center and Site Improvements included in the Planned Development Permit consistent with the Development Agreement and the Subsequent Entitlements.

“Property” means the land identified by legal description on **Exhibit A-1** and depicted on **Exhibit A-2**.

“Site Improvements” means the grading, infrastructure and off-site improvements identified in the Planned Development Permit Guidelines.

“Square Footage” for purposes of the Medical Center shall mean “Occupied Square Footage,” as that term is defined and described in the Planned Development Permit and the Planned Development Guidelines, as distinct from building gross square footage.

“Subsequent Entitlements” shall have the amended definition set forth in paragraph 2, subsections (a)-(d), inclusive, of this Amendment No. 2.

“Technical Memorandum” shall mean the PA Parcel 85A Zone Supplemental Analysis dated April 28, 2021 prepared in conjunction with the Environmental Analysis that determined the appropriate water infrastructure connection for water services to the Property.

“Traffic Study” means the traffic analysis prepared and included in the Addendum titled Final Local Transportation Analysis & CEQA Impact Study, dated April 23, 2021 and the Local Transportation Analysis & CEQA Impact Study Errata dated April 29, 2021.

“Vested Rights” means the rights to Develop the Property consistent with the terms and provisions of the Restated Development Agreement, Amendment No. 1, this Amendment No. 2 and the provisions of the Subsequent Entitlements.

4. **Section 1.4.1 – Commencement, Extension, Expiration.** In light of the unique circumstances identified in Recital E of this Amendment No. 2, the Term of the Development Agreement and this Amendment No. 2 are extended to June 30, 2056, or until 530,000 occupied square feet has been built out, whichever is later. All other provisions of Section 1.4.1 of the Restated Development Agreement remain in full force and effect. Landowner may request an additional extension pursuant to Section 1.4.1 of the Restated Development Agreement.

5. **Section 1.6 – Changes to the City’s Inclusionary Housing Ordinance** in Amendment No. 1 is hereby revised to read as follows: “The City has amended the Inclusionary Housing Ordinance (i.e., Folsom Municipal Code Chapter 17.104) by Ordinance No. 1243, to eliminate Second Dwelling Units (also referred to as “granny flats”) as an alternative means of meeting the City’s inclusionary housing requirements. Both Parties acknowledge that the Project is for medical uses and not residential housing. However, in light of the allowed uses under the Specific Plan, City has requested that Landowner acknowledge, and Landowner hereby acknowledges, that there is no vested right to use Second Dwelling Units as an alternative means for meeting the City’s inclusionary housing requirements and that this alternative shall not be available to Landowner from and after the date of Ordinance No. 1243. Landowner further acknowledges that the State adopted amendments to Section 65850 of the California Government Code (specifically Section 65850(g)), effective January 1, 2018, to allow for the implementation of inclusionary housing requirements in residential rental units, upon adoption of an ordinance by the City. The Landowner is not currently contemplating any residential rental projects within the Property; however, in the event the City amends its Inclusionary Housing Ordinance with respect to rental housing pursuant to Section 65850(g), Landowner (or a successor in interest) agrees that the Property shall be subject to said City Ordinance, as amended, should any residential rental project be proposed within the Property. Other than the elimination of the “granny flat” option and the possible future application of an inclusionary housing requirement on residential rental properties (upon the conditions stated herein), the Parties agree that all other alternatives for meeting the City’s inclusionary housing requirements remain vested to the full extent provided for in the Restated Agreement.”

6. **Section 2.1 – Permitted Uses.** The permitted uses of the Property, the density and intensity of use, the maximum height and size of proposed buildings and related structures, set backs, Square Footage (as defined in Section 1.3 and calculated based on Occupied Square Footage and not building gross square footage), heliport location and approach, all signage approved in the Subsequent Entitlements (inclusive of spiritual symbolism), provisions for reservation or dedication of land for public purposes and location of public utilities and public improvements shall be those set forth in the Entitlements, the Subsequent Entitlements, the Development Agreement and this

Amendment No. 2. The permitted uses of the Property shall also include the types of buildings and related structures within the definition of Medical Center and as identified in the Subsequent Entitlements that are ancillary to the development of the Medical Center or the provision of medical services, including but not limited to concrete pads, trailers and structures or facilities that may be necessary for emergency (e.g., pandemic) services (including, but not limited to, tents), as long as they are identified in the Site Plan attached hereto as **Exhibit B**. Any temporary structures that are not identified in the Site Plan shall be processed through City administrative processes, with recognition given to the expedited review that may be necessary for temporary structures to address emergency purposes (including but not limited to pandemics).

7. **Section 2.2 - Vested Rights.** The City agrees that, except as otherwise provided in and as may be amended in accordance with the Exceptions to Vested Rights set forth in Section 2.2.3 of the Restated Development Agreement, Amendment No. 1, and this Amendment No. 2, the City is granting, and grants herewith, Vested Rights to Development for the Term of this Amendment No. 2 in accordance with the terms and conditions set forth herein. The City acknowledges that the rights vested by the Restated Development Agreement, Amendment No. 1 and this Amendment No. 2 include the land uses, utility connections and water (subject to the provisions of Sections 4.6 of the Restated Agreement and Section 4.6.1 of this Amendment No. 2), approximate acreages and Site Plan for the Property as shown and described in **Exhibits A-2 and B** attached hereto. Nothing in this Amendment No. 2 shall impair or affect the rights of Landowner under a vesting tentative map or the City's rights to condition such maps. (Govt. Code Sec. 66498.1, et seq.)

Such uses shall be developed in accordance with the Subsequent Entitlements, as the Subsequent Entitlements are described in Section 1.5.3 of this Amendment No. 2 and as approved by the City on the Effective Date.

Section 2.2.1 – Vested Provisions of the PFFP. No changes.

Section 2.2.2 - Vested Provisions of the Specific Plan. In addition to the provisions of Section 2.2.2(A)-(C) of the Restated Development Agreement, as modified by Amendment No. 1, the following shall apply to the Property:

- D. Notwithstanding the provisions of Section 2.2.2(C) of Amendment No. 1, the street width and roadway sections for McCarthy Way and Mercy Way as approved in the Planned Development Permit and the Planned Development Guidelines, as well as all internal road widths for the Medical Center shall be vested for the Term of this Amendment No. 2, except that the City reserves all rights to add turn lanes, deceleration tapers and other necessary traffic improvements to accommodate safe vehicular access to the Medical Center in the future that have been analyzed and approved in the Traffic Study or in a technical traffic analysis approved by the

Planning Commission or City Council in connection with another project (either public or private).

- E. The Specific Plan identifies a route for Bus Rapid Transit (“BRT”), which as of the Effective Date would be constructed and operated by Sacramento Regional Transit (“Sac RT”). The route identified for BRT has been considered in the Traffic Study, and the Subsequent Entitlements, including the Conditions of Approval, take into account the location for BRT identified in the Specific Plan. If Sac RT proposes any modifications to the location of BRT or location of a bus stop on the Property, the City agrees to (a) notify Landowner, (b) work in good faith with Landowner on any modifications to the location of BRT or a bus stop that may impact the Medical Center, and (c) notify Sac RT that the Subsequent Entitlements are vested pursuant to the terms of this Amendment No. 2.

Sections 2.2.3 – 2.2.8 Exceptions to Vested Rights. No changes, except for the addition of Section 2.2.9 as an Exception to Vested Rights, as follows:

Section 2.2.9 – Intersection Controls at Alder Creek Parkway and McCarthy Way. The Planned Development Permit proposes a non-standard traffic signal (i.e., 700-foot signal spacing) at the left turn access to the Medical Center at the intersection of Alder Creek Parkway and McCarthy Way as depicted on **Exhibit C** to this Amendment No. 2. The Environmental Analysis concluded, based on the Traffic Study, that no significant environmental impacts would result from 700-foot signal spacing proposed by Landowner for a traffic signal at this intersection, although this spacing does not meet City design standards and is not reflected in the Specific Plan. City acknowledges that Landowner proposes to install the proposed signal concurrent with development of Phase 4 (second addition of 100 beds) of Project development and no sooner (unless traffic conditions require earlier installation), and City requires an updated analysis prior to final approval of installation of the traffic signal to identify any impacts to the Specific Plan transportation system. Subject to the specific requirements of Condition of Approval Nos. 41 and 42, Landowner will prepare a supplemental traffic study no later than one year before the proposed installation of the traffic signal for City review and evaluation. The supplemental traffic study shall evaluate the traffic impacts associated with the installation of a traffic signal at Alder Creek Parkway and McCarthy Way. If the supplemental traffic study concludes that there is no significant traffic operational impact, the traffic signal may be installed by Landowner pursuant to the Conditions of Approval and consistent with technical specifications as approved by the City Engineer. For purposes of this section, “traffic operational impact” shall mean and include an environmental impact under CEQA, a safety impact, an impact to BRT, or an impact that results in unacceptable delays to an adjacent street. If the Traffic Study identifies any significant traffic operational impacts that can be mitigated and the Landowner desires to install the traffic signal, the City will identify the required mitigation

and the Landowner shall install the traffic signal along with the required mitigation. In the event the supplemental traffic study identifies any significant traffic operational impact that cannot be mitigated, the City will advise the Landowner and the traffic signal shall not be installed, and the intersection configuration approved as part of the Subsequent Entitlements shall remain in place. Any other modifications proposed by Landowner shall be subject to appropriate environmental review, City approval, and compliance with applicable City standards. If City determines that the left turn access to the Medical Center at the intersection of Alder Creek Parkway and McCarthy Way must be discontinued for any reason, City shall (a) advise Landowner of the City's intention to discontinue access, (b) provide Landowner with a reasonable opportunity to comment on the City's determination to discontinue access, including an appeal to the City Council, and (c) consider the impact on safe and efficient public access to the hospital and emergency services, including any evidence Landowner may provide to City with respect to increased driving times.

Section 2.2.10 – Class 1 Bicycle Path. Landowner acknowledges that it is responsible for rough grading and installation of the necessary retaining wall at its sole cost and expense to accommodate the Class 1 Bicycle Path on the northern portion of the Property as shown in Figure 7.32 of the Specific Plan, and that said work shall be completed no later than the issuance of a building permit on the second expansion of the hospital, identified as Phase 4 and is anticipated to occur in approximately 2034. The Landowner and the City will cooperate on timing of the grading for the proposed Class 1 Bicycle Path and construction of the retaining wall to coincide with phased construction of the Project, which may occur earlier than Phase 4 if the City has identified funding (as defined below) for the Class I Bicycle Path prior to Phase 4. The City agrees that the design of the Class 1 Bicycle Path shall impact no more than five (5) parking spaces, and further that Landowner shall be relieved of the aforementioned obligation should the City amend the Specific Plan to relocate the Class 1 Bicycle Path, or if the City shall not have identified funding for construction of said the Class 1 Bicycle Path across the Property and the connection to East Bidwell Street at the time of issuance of a building permit on the second expansion of the hospital for Phase 4. For purpose of this section, "identified funding" shall mean either: (1) the Class 1 Bicycle Path is incorporated into a subsequent project to widen the East Bidwell Overcrossing structure of US Highway 50 or (2) the submission or application for federal, state or other grants which, together with the City's available matching funds, would be sufficient to construct the Class 1 Bicycle Path across the Property and the connection to East Bidwell Street.

8. **Section 3.5 – EIR Mitigation Measures.** Notwithstanding any other provision in the Restated Development Agreement or Amendment No. 1, as amended hereby, as and when Landowner elects to Develop the Property, or any portion of the Property, Landowner shall be bound by, and shall perform, or cause to be performed, all mitigation measures contained in the Specific Plan EIR/EIS, the Addendum, the

Backbone Infrastructure IS/MND, and any environmental mitigation measures referenced therein applicable to the Development of the Property. The City acknowledges that Landowner has entered into a contractual agreement with Predecessor's Affiliate, which requires that the Site Improvements and related mitigation measures identified in **Exhibit D** attached hereto shall be completed by the Predecessor's Affiliate. Landowner acknowledges that the Site Improvements are Conditions of Approval and, as such, are incorporated into the Subsequent Entitlements and are required for completion pursuant to the timing identified in the Conditions of Approval.

9. **Additions to Development Agreement.** Landowner acknowledges and confirms the Additions to Restated Development Agreement provided for in Section 2 of Amendment No. 1. The following Sections are also added to the Development Agreement as follows:

a. **Section 3.9.2.1 - Phasing of In-Tract Improvements.** The City acknowledges that the Medical Center will be constructed in numerous phases, with the timing and order of phases to be at the discretion of Landowner. The required in-tract improvements for development of the Medical Center shall be phased along with the specific building phases that trigger the need for in-tract improvements, as provided for in the Conditions of Approval for the Subsequent Entitlements.

b. **Section 3.9.3 – Design Review.** Landowner has provided some information related to design of the Medical Center in the Planned Development Guidelines, but acknowledges that additional design review for the two medical office buildings and the hospital is required, and that Landowner shall submit to the City for review and approval by the Planning Commission the design of the buildings and related structures that comprise the medical office buildings and/or hospital prior to construction of any permanent building. The approved building design(s) may be phased, but each phase shall be incorporated into the Design Guidelines that govern the design of the Medical Center.

c. **Section 4.1.1 – City Cooperation in Connection with State OSHPD Approval of Hospital Building Design and Review by Other Agencies.** In addition to the requirements of good faith cooperation and other provisions of Section 4.1 of the Development Agreement, the City acknowledges that Landowner will be required to obtain approval of the design of the hospital building by State OSHPD and will also be required to obtain approvals for components of the Medical Center (including but not limited to the heliport) from Caltrans, Sacramento County and other federal, state or regional agencies. This approval may result in required changes to, among other things, building structure, fenestration, awnings, set backs, and other physical features of the hospital building and/or layout of the Project on the Property. The City shall evaluate and process any such modifications pursuant to the MAM procedure in the Specific Plan if such modification qualifies to be processed through the MAM procedure, but reserves the right to process any such changes required by such agencies through

the Planning Commission and/or the City Council, with appropriate notification to the approval body of the mandatory requirements imposed by such agencies.

d. **Section 4.6.1 – Water Supply.** The City, through approval of the Subsequent Entitlements, has made a finding pursuant to Folsom Municipal Code section 17.38.100(D) of the “availability of necessary public facilities including, but not limited to, water, sewage and drainage and the adequacy of the provision which the development makes for the furnishing of such facilities.” While the City retains the ability pursuant to Section 4.6 of the Restated Development Agreement to “address water shortages on a citywide basis,” the City acknowledges Landowner’s reliance upon the finding of the availability of necessary water to serve the Medical Center, as confirmed in the Addendum for the Project. The City based this determination upon the technical analysis supporting the Addendum regarding water demand for the Project and the conclusion from this technical analysis that the increased water demand estimated at 126 acre feet per year for the Project would remain within the 5,600 acre-feet per year available for the Specific Plan. A graphic included in the Addendum depicting the water supply for the Project is attached hereto as **Exhibit E.** City acknowledges that the estimate of water supply for the Project is now included in the baseline analysis of water usage for the Specific Plan. The City further acknowledges that Landowner will provide essential medical services throughout the Medical Center and that, in the event of a water shortage, the City will take into account the water requirements for essential medical services in any future action that may be necessary to address water shortages. Any disruption in water supply imposed by the State or the City that prevents Landowner from constructing any portion of the Medical Center shall provide a basis for an extension to the Term of this Amendment No. 2 for the same period that such disruption in water supply exists, subject to approval by the City Council.

e. **Section 4.6.2 – Water Infrastructure.** The City, through approval of the Subsequent Entitlements and consideration of a Technical Memorandum titled “PA Parcel 85A Zone Supplemental Analysis” dated April 28, 2021, provided in support of the Addendum, has determined that the water infrastructure for the Medical Center will be provided through “Zone 4,” as that Zone is identified in Figure 12.1 of the Specific Plan, and the landowner shall pay all costs and expenses for piping, and tank size expansion from 2 million gallons to 2.6 million gallons, and construction to connect water from Zone 4 to the Medical Center. On a temporary basis, until the completion of the Zone 4 infrastructure, the water infrastructure for the Medical Center will be provided through Zone 5, as that Zone is identified in Figure 12.1 of the Specific Plan. Landowner bears all costs and expenses to connect water from Zone 5 to the Medical Center. Unless improvements or connections to Zone 4 are solely attributable to the hospital uses included in the Project, Landowner shall only be required to provide a fair-share contribution towards the construction of the Zone 4 infrastructure and may elect to participate in any special assessment/special tax funding mechanisms that are established by the City. Landowner shall coordinate with developers of other projects

that also use Zone 4 infrastructure to determine the amounts of their respective fair share contribution toward Zone 4 infrastructure.

f. **Section 4.10 – Commencement of Construction and Term of Planned Development Permit and Conditional Use Permit.** In light of the extended Term of this Amendment No. 2, the City agrees that the provisions of Folsom Municipal Code sections 17.38.110 and 17.60.060 related to expiration, revocation or abandonment of a Planned Development Permit or a Conditional Use Permit shall have no effect, and that the term of the Planned Development Permit and Conditional Use Permit are equal to the Term of this Amendment No. 2.

g. **Section 4.11 – Maintenance of Landscaping Frontage, Primary Gateway and Street Lights on Public Streets.** Subject to receipt of funding through a mutually agreeable financing mechanism, the City agrees to maintain the frontage landscaping improvements identified on **Exhibit F** and the streetlights on McCarthy Way and East Bidwell Street along frontage of the Property as identified on **Exhibit F**. Landowner acknowledges the identification of a “Primary Gateway” on Figure 2.2 of the Folsom Plan Area Community Design Guidelines and as further described in Section 2.2.1 of that document . City acknowledges that the installation of the Primary Gateway and maintenance of the Primary Gateway is included as an amenity in the Folsom Plan Area Community Design Guidelines and, as such, is not an individual obligation assigned to Landowner. City shall not require Landowner to bear more than a fair share of the cost of such design, installation and maintenance, and Landowner agrees to coordinate with developers of other projects in the Specific Plan to determine the amounts of their respective fair share contribution toward the design, installation and maintenance of the Primary Gateway. Notwithstanding the foregoing, City shall not be responsible for any costs to design, install, or maintain said Primary Gateway.

10. **Effect of Amendment.** This Amendment No. 2 amends, but does not replace or supersede, the Restated Development Agreement and Amendment No. 1. In the event of any conflict, the language of this Amendment No. 2 shall be controlling in all events or circumstances. Except as modified hereby, all other terms and provisions of the Restated Development Agreement and Amendment No. 1 shall remain in full force and effect.

11. **Section 7.8 – Notices.** All notices required by the Development Agreement or this Amendment No. 2 as such requirements relate to the Property or the Subsequent Entitlements, or the enabling legislation or the procedure adopted pursuant to Government Code section 65865 shall be as provided for in Section 7.5 of the Development Agreement, with the substitution for Landowner as follows:

CommonSpirit Health
3200 N. Central Avenue, 23rd Floor
Phoenix, AZ 85012
Attention: System Senior Vice President, National Real Estate Services

With copies to:

CommonSpirit Health
3400 Data Drive
Rancho Cordova, CA 95670
Attention: National Real Estate Services

And

CommonSpirit Health
3200 N. Central Avenue, 23rd Floor
Phoenix, AZ 85012
Attention: Legal Team

12. **Form of Amendment – Execution in Counterparts.** This Amendment No. 2 is executed in duplicate originals, each of which is deemed to be an original, and may be executed in counterparts.

IN WITNESS WHEREOF, the City of Folsom has authorized the execution of this Amendment No. 2 in duplicate by its Mayor and attested to by the City Clerk under the authority of Ordinance No. [] adopted by the City Council on the ___ day of _____, 2021.

CITY:

CITY OF FOLSOM
a municipal corporation

Michael Kozlowski, Mayor

APPROVED AS TO CONTENT:

Elaine Andersen, City Manager

APPROVED AS TO FORM:

Steven Wang, City Attorney

ATTEST:

Christa Freemantle, City Clerk
[Notary Pages to be Added]

LANDOWNER:

DIGNITY HEALTH
a California nonprofit public
benefit corporation

By: _____
Its: _____

APPROVED AS TO FORM:

Martha Clark Lofgren, Brewer
Lofgren LLP

EXHIBIT LIST

- A-1** Legal Description of the Property
- A-2** Depiction of Parcel 1 on Map
- B** Approved Site Plan
- C** Map Depicting Potential Future Signalized Access to Medical Center
- D** Improvements and Mitigation Measures to be Completed by Predecessor's Affiliate
- E** Project Water Supply
- F** Map Depicting McCarthy Way Lighting, Landscaping Frontage and Tentative Location of Gateway and Monument Signs

EXHIBIT A-1

LEGAL DESCRIPTION OF PROPERTY

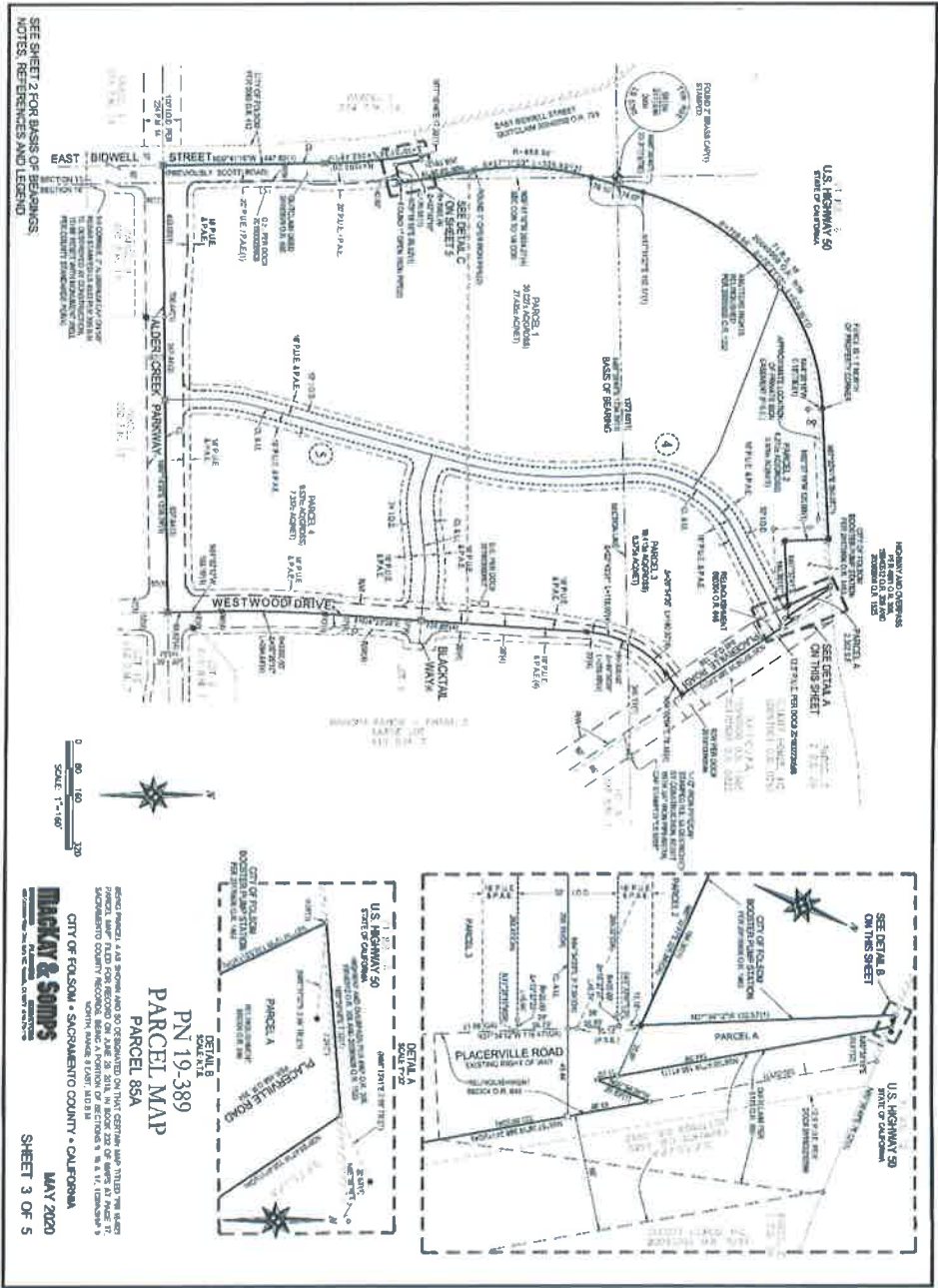
Situated in the City of Folsom, County of Sacramento, State of California and more particularly described as follows:

Parcel 1 of "PN 19-389 Parcel Map Parcel 85A", filed for record May 22, 2020, in Book 240 Page 13 of Parcel Maps, Sacramento County Records.

APN: 072-3190-046 (portion)

EXHIBIT A-2

DEPICTION OF PARCEL 1 ON RECORDED PARCEL MAP



240-13-3

EXHIBIT B SITE PLAN

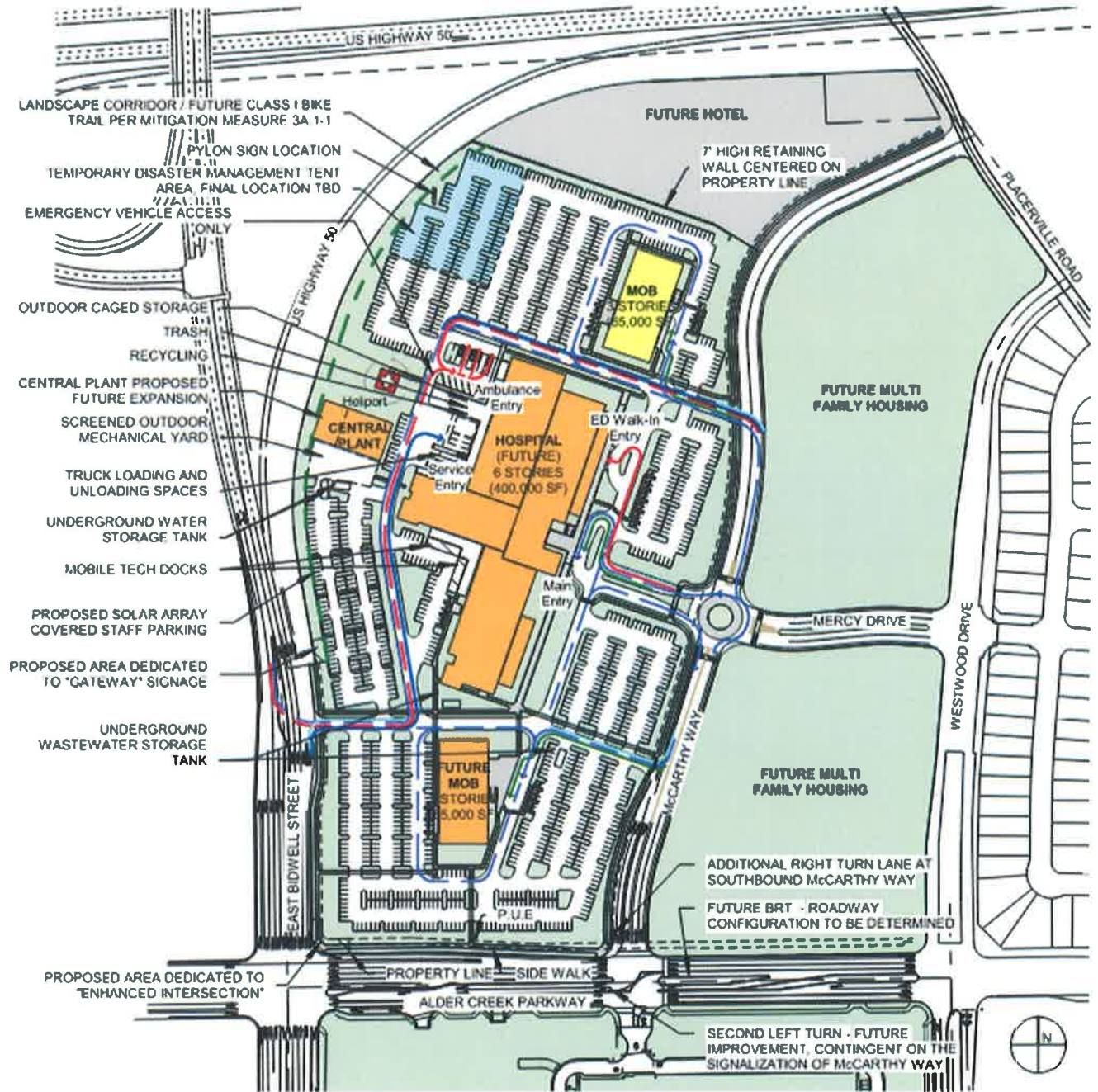




EXHIBIT B - SITE PLAN

Legend Follows on Next Page







Exhibit B

EXHIBIT B
SITE PLAN, CONTINUED

PHASE LEGEND

-  PHASE 1
-  FUTURE PHASE

CIRCULATION PATHS

-  FIRE ACCESS
-  SERVICES / MATERIALS
-  ED WALK-IN
-  AMBULANCE
-  VISITORS / PATIENT
-  EMPLOYEES

PROJECT INFORMATION

FUTURE: Acute Care Hospital = 400,000 SQFT
Medical Office Building = 130,000 SQFT

FUTURE TOTAL SQFT: 530,000 SQFT (OCCUPIED)

TOTAL DEVELOPMENT ACREAGE: 27.44 acres

OVERALL SITE ACREAGE: 31 acres

TOTAL INPATIENT BEDS: 300+ beds

PROPOSED BUILDING HEIGHT = 120' (6 LEVELS)

PROPOSED MAX FAR: .55

EXHIBIT B – SITE PLAN LEGEND

EXHIBIT C
FUTURE POSSIBLE SIGNALIZED ACCESS



EXHIBIT C - FUTURE POSSIBLE SIGNALIZED ACCESS

EXHIBIT D
IMPROVEMENTS AND MITIGATION
MEASURES TO BE COMPLETED BY
PREDECESSOR'S AFFILIATE

1. Mass grading of Parcel 85A and adjacent roadways resulting in contour graded developable parcels and subgrade along roadway alignments.
2. Rough grading of a portion of Hydromodification Basin #8 (HMB#8) including the access road along the Savannah Parkway alignment and perimeter of the basin.
3. Rough Grading of a storm drain outfall swale from the western terminus of Alder Creek Parkway to the existing downstream waterway.
4. Excavation at a borrow site west of East Bidwell.
5. Roadway and Utility improvements along East Bidwell, Alder Creek Parkway, Westwood Drive, Placerville Road, McCarthy Drive and Mercy Way, including storm drain, sanitary sewer, potable and non-potable water, and dry utility infrastructure.
6. HMB#8 improvements including a paved access road to East Bidwell and basin outlet control structure and spillway.

RELATED CONDITIONS OF APPROVAL AND MITIGATION MEASURES

The following Conditions of Approval pertain to the Site Improvements referenced above; in some cases, the applicable Conditions of Approval also pertain to construction of the Medical Center and this list is intended to apply to those conditions of approval related to Site Improvements, only:

Conditions of Approval 1, 2, 4, 6, 9, 10, 12 (as applicable at Improvement Plans), 13, 14, 15-25 (inclusive), 26 (as applicable at Improvement Plans), 27-30 (inclusive), 32, 33, 34 (as applicable at Improvement Plans and not including tank size expansion unless otherwise required for development of Parcels 2-4 of Parcel 85a), 35 (as applicable at Improvement Plans), 36 (medians only and only to the extent applicable at Improvement Plans), 41 (Phase 1 and Phase 2, only), 43 (as applicable at Improvement Plans), 50 and 51 (including all applicable mitigation measures as identified by reference in this Condition of Approval No. 51).

EXHIBIT E PROJECT WATER SUPPLY, INCLUDING PARCEL 1 OF PARCEL 85A

Folsom Plan Area
Folsom Ranch Medical Center
Potable Water Demand Chart

06/08/2021

Water Supply Agreement - 5,800 AFY

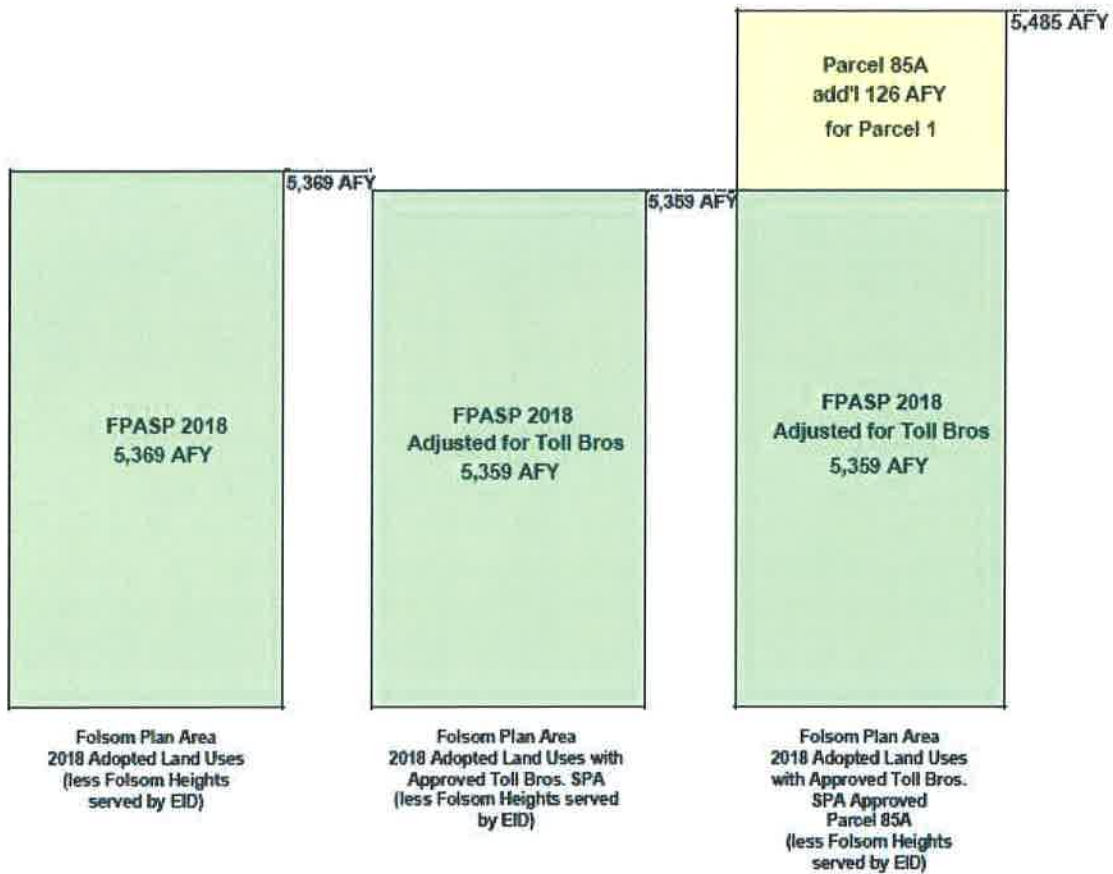


EXHIBIT F
STREET LIGHTING AND LANDSCAPING FRONTAGE



- ▲ 1 PRIMARY GATEWAY BY OTHERS (MAINTENANCE TO BE ADDRESSED BY OWNER'S GROUP)
 - 4- MONUMENT SIGN (LANDOWNER RESPONSIBILITY)
 - 12 STREET LIGHTING (MAINTENANCE TO BE ADDRESSED BY SEPARATE AGREEMENT)
 - 12 FLOOD LIGHTING FOR SITE SIGNAGE (LANDOWNER RESPONSIBILITY)
 - 32 LIGHT POLE / AREA LIGHT (LANDOWNER RESPONSIBILITY)
 - LANDSCAPE AREA = TBD (MAINTENANCE TO BE ADDRESSED BY SEPARATE AGREEMENT)
 - () ENHANCED INTERSECTION (MAINTENANCE TO BE ADDRESSED BY SEPARATE AGREEMENT)
- NOTE: PLANT TYPES AND QUANTITIES ARE SHOWN IN THE PD GUIDELINES



EXHIBIT F - STREET LIGHTING AND LANDSCAPING FRONTAGE

Attachment 3
Planning Commission Staff Report
Dated May 19, 2021



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

AGENDA ITEM NO. 1
Type: Public Hearing
Date: May 19, 2021

Planning Commission Staff Report
50 Natoma Street, Council Chambers
Folsom, CA 95630

Project: Folsom Ranch Medical Center
File #: PN-20-193
Requests: Addendum to Final EIR
 Planned Development Permit
 Conditional Use Permit
 Development Agreement Amendment
Location: The proposed Folsom Ranch Medical Center project is located at the northeast corner of the intersection of East Bidwell Street and Alder Creek Parkway within the Folsom Plan Area
Staff Contact: Steve Banks, Principal Planner, 916-461-6207
 sbanks@folsom.ca.us

Property Owner/Applicant
Name: Dignity Health/Robert O'Hare
Address: 10901 Gold Center Drive, Suite 300
 Rancho Cordova CA 95630

Recommendation: Conduct a public hearing and upon conclusion recommend that the Planning Commission adopt an Addendum to the Final Environmental Impact Report for the Folsom Plan Area Specific Plan prepared for the Folsom Ranch Medical Center project (PN 20-193), approve a Planned Development Permit and a Conditional Use Permit, and recommend that the City Council approve a Development Agreement Amendment for the Folsom Ranch Medical Center project, subject to the findings (Findings A-W) and conditions of approval (Conditions 1-51) attached to this report.

Project Summary: The proposed project includes development of a 530,000-square-foot Medical Center (Folsom Ranch Medical Center) on a 27.44-acre site situated at the northeast corner of the intersection of East Bidwell Street and Alder Creek Parkway within the Folsom Plan Area. The proposed Medical Center project, which will be developed in five phases over the course of approximately 23 years, includes a six-story 400,000-square-foot acute care hospital with 300 beds, two three-story 65,000-square-foot medical office buildings, and a ground-level heliport facility. In addition, the proposed



CITY OF
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DISTINCTIVE BY NATURE

AGENDA ITEM NO. 1
Type: Public Hearing
Date: May 19, 2021

project includes a number of significant off-site improvements including roadway and utility improvements, grading improvements on three adjacent parcels, construction of a hydromodification basin, construction of a storm drain swale, and excavation of a borrow site.

The following are the specific entitlements requested with the proposed project.

- **A Planned Development Permit** which contains detailed development and architectural standards for the proposed 530,000-square-foot Medical Center.
- **A Conditional Use Permit** for the development and operation of a private-use hospital heliport facility at the Medical Center.
- **A Development Agreement Amendment** to the First Amended and Restated Tier 1 Development Agreement to incorporate in the definition of "entitlements" that are vested the entitlements requested by the project applicant including the Planned Development Permit, the Planned Development Guidelines, the Conditional Use Permit, and the Site Plan. The Development Agreement Amendment also seeks to take into account the longer construction timeline for the project, memorialize agreements relative to project site access, memorialize agreements relative to water infrastructure and connections, memorialize agreements relative to street lighting and landscape frontage maintenance, memorialize agreements relative to preparation for the future Class I trail, and acknowledge the potential for future modifications to the Medical Center that may be required by the California Office of Statewide Health Planning and Development.

These proposed actions are described in detail and analyzed later in this report.

Table of Contents:

Attachment 1 - Background and Setting

Attachment 2 - Project Description

- Planned Development Permit
- Conditional Use Permit
- Development Agreement Amendment



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

AGENDA ITEM NO. 1
Type: Public Hearing
Date: May 19, 2021

Attachment 3 - Analysis

- Planned Development Permit
- Conditional Use Permit
- Development Agreement Amendment

Attachment 4 - Conditions of Approval

Attachment 5 - Vicinity Map

Attachment 6 - Preliminary Site Plan, dated May 6, 2021

Attachment 7 - Preliminary Utility Plan, dated May 6, 2021

Attachment 8 - Preliminary Grading and Drainage Plan, dated May 6, 2021

Attachment 9 - Preliminary Landscape Plans, dated May 6, 2021

Attachment 10 - Preliminary Lighting Plan, dated May 6, 2021

Attachment 11 - Preliminary Access and Circulation Plan, dated May 6, 2021

Attachment 12 - Preliminary Off-Site Improvement Plans, dated May 6, 2021

Attachment 13 - Conceptual Building Renderings, dated May 6, 2021

Attachment 14 - Planned Development Guidelines, dated May 12, 2021

Attachment 15 - Folsom Ranch Medical Center Booklet (Separate Bound Document)

Attachment 16 - Amendment No. 2 to First Amended and Restated Development Agreement Relative to Folsom South Specific Plan (Dignity Health)

Attachment 17 - Site Photographs

Attachment 18 - City Memorandum Regarding Folsom Ranch Medical Center Site Access, dated September 30, 2020

Attachment 19 - Devenny Group Traffic Scenarios Memorandum, dated October 26, 2020

Attachment 20 - Transportation Analysis and CEQA Impact Study, dated April 23, 2021 and April 29, 2021 (documents can be found on the City's website at <https://www.folsom.ca.us/home/showpublisheddocument/6155/637558120792530000>)



CITY OF
FOLSOM
DISTINCTIVE BY NATURE

AGENDA ITEM NO. 1
Type: Public Hearing
Date: May 19, 2021

Attachment 21 - Arborist Report and Oak Tree Mitigation Strategy, dated April 28, 2021

Attachment 22 - Environmental Checklist and Addendum for the Folsom Ranch Medical Center Project, dated April, 2021 (documents can be found on the City's website at

<https://www.folsom.ca.us/home/showpublisheddocument/6137/637558099893530000>

Attachment 23 - Mitigation Monitoring and Reporting Program for the Folsom Ranch Medical Center Project, dated April, 2021 (documents can be found on the City's website at

<https://www.folsom.ca.us/home/showpublisheddocument/6141/637558120754730000>

Submitted,

A handwritten signature in blue ink, appearing to read "Pam Johns", with a long horizontal flourish extending to the right.

PAM JOHNS
Community Development Director

ATTACHMENT 1 BACKGROUND AND SETTING

Background:

The proposed project site is part of the approved Folsom Plan Area Specific Plan (FPASP), a comprehensively planned community that proposes new development based "Smart Growth" and Transit Oriented Development principles. The FPASP, approved in 2011, is a development plan for over 3,500 acres of previously undeveloped land located south of U.S. Highway 50, north of White Rock Road, east of Prairie City Road, and west of the Sacramento County/El Dorado County line in the southeastern portion of the City.

The FPASP includes a mix of residential, commercial, employment and public uses, complemented by recreational amenities including a significant system of parks and open space, all within close proximity to one another and interconnected by a network of "complete streets", trails and bikeways. The Specific Plan is consistent with the SACOG Blueprint Principles and the requirements of SB 375 (Sustainable Communities and Climate Protection Act).

The FPASP includes 11,461 residential units at various densities on approximately 1,630 acres; 310 acres designated for commercial and industrial use; +/-130 acres designated for public/quasi-public uses, elementary/middle school/high schools, and community/neighborhood parks; and +/-1,110 acres for open-space areas.

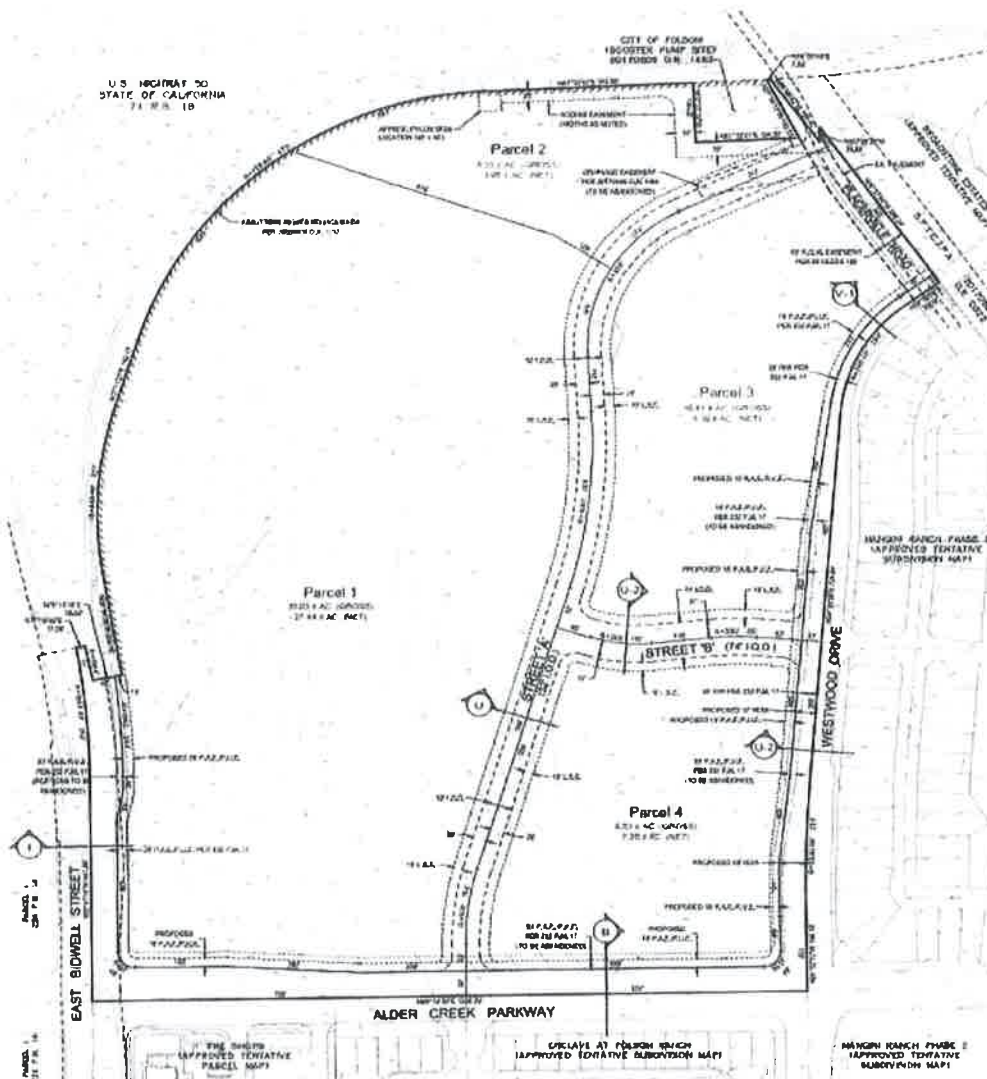
On September 22, 2015, the City Council approved an Addendum to the Folsom Plan Area Specific Plan EIR/EIS, a General Plan Amendment, a Specific Plan Amendment, and Amendment No. 1 to the First Amended and Restated Tier 1 Development Agreement for the Westland-Eagle project. The Westland-Eagle project included a significant reduction in the amount of retail commercial land area and an increase in the number of allowed residential dwelling units within the Folsom Plan Area. The net result of these land use modifications was a decrease of 1,445,710 square feet of commercial building area and an increase of 922 residential units within the Plan Area. In addition, the Westland-Eagle project contained modifications to the FPASP including elimination of the Entertainment Overlay Zone, relocation of more intense land uses toward Alder Creek Parkway, strengthening focus of the town center, relocation of Alder Creek Parkway, and realignment of Old Placerville Road. The proposed project is located within the previously approved Westland-Eagle project area.

On December 4, 2019, the Planning Commission approved a Tentative Parcel Map to subdivide a 54.30-acre property (FPASP Parcel 85A) located at the northeast corner of East Bidwell Street and Alder Creek Parkway into four individual parcels for future sale and development. The City Council approved the Final Map and it was thereafter

Planning Commission
Folsom Ranch Medical Center (PN 20-193)
May 19, 2021

recorded on May 22, 2020. The four new parcels created by the Parcel Map were 27.44 acres (Parcel 1), 3.97 acres (Parcel 2), 8.39 acres (Parcel 3), and 7.35 acres (Parcel 4) in size respectively. The Parcel Map also included dedication of 7.15-acres of land as public right-of-way for future development of public roadways and associated improvements. Dignity Health, a California nonprofit public benefit corporation, acquired Parcel 1 on June 5, 2020. The proposed Medical Center is located on the largest (Parcel 1) of the four parcels that were created by the Tentative Parcel Map as shown in Figure 1 below.

FIGURE 1: PARCEL 85A FINAL MAP



On March 17, 2020, the Community Development Director approved a Minor Administrative Modification (MAM) for the transfer of development rights to shift residential units and commercial gross square feet among four parcels (Parcels 61, 77, 78, and 85A) located within the Folsom Plan Area for the purpose of meeting the development intent of the subject properties. As mentioned previously, the proposed Medical Center project is located on a 27.44-acre portion of the larger parcel known as Parcel 85A. A detailed discussion of the MAM and the modifications affecting this site is included in the General Plan and Specific Plan Consistency section of this staff report.

Physical Setting

The rectangular-shaped 27.44-acre project site, which features gently rolling terrain, contains a variety non-native grasses and a small grove of willow trees. Existing topography on the site ranges from 450-feet to 400-feet in elevation and generally falls in the southwest direction. The project site is bounded by U.S. Highway 50 to the north with the Folsom Pointe shopping center beyond, Alder Creek Parkway to the south with the Enclave Subdivision beyond, East Bidwell Street to the west with future commercial development beyond, and future McCarthy Way and future multi-family development to the east with Westwood Drive beyond. An aerial photograph of the project site is shown in Figure 2 below.

FIGURE 2: AERIAL PHOTOGRAPH OF PROJECT SITE



ATTACHMENT 2 PROJECT DESCRIPTION

APPLICANT'S PROPOSAL

The applicant, Dignity Health, is requesting approval of a Planned Development Permit, Conditional Use Permit, and Development Agreement Amendment for development of a 530,000-square-foot Medical Center (Folsom Ranch Medical Center) on a 27.44-acre site located at the northeast corner of the intersection of East Bidwell Street and Alder Creek Parkway within the Folsom Plan Area.

As noted above, the applicant is requesting approval of three entitlements and adoption of an Addendum to allow for development of the proposed Medical Center. The first entitlement is a request for approval of a Planned Development Permit which contains specific development and architectural standards for the proposed 530,000-square-foot Medical Center. Planned Development Guidelines (Attachment 14) have been submitted which provide the framework for the Planned Development Permit including deviations from the development standards established by the Folsom Plan Area Specific Plan and the Folsom Municipal Code in order to accommodate an acute care hospital, medical office buildings, a heliport, site design and planning, building architecture, landscaping, site lighting, and signage. Specific modifications to the development standards include:

1. Increase Maximum Building Floor Area Ratio (FAR) from 0.25 to 0.55
2. Increase Maximum Building Height from 50 feet to 60 feet (Office Buildings)
3. Increase Maximum Building Height from 50 feet to 120 feet (Hospital Building)
4. Sign Criteria Modifications
 - Increase Number of Wall Signs for Office and Hospital Buildings
 - Increase Maximum Sign Area for Office and Hospital Buildings
 - Increase Maximum Letter Height for Office Buildings and Hospital Buildings
 - Allow Illumination of Office and Hospital Building Wall Signs
 - Increase Maximum Height of Freeway Pylon Sign
 - Increase Maximum Sign Area of Freeway Pylon Sign
5. Apply Folsom Municipal Code Requirements for Required Bicycle Parking Spaces
6. Reduce Amount of Required Vehicle Loading/Unloading Spaces

The second entitlement is a request for approval of a Conditional Use Permit for development and operation of a ground-level heliport within the western portion of the project site, near the hospital's emergency department ambulance entrance. The heliport, which will be designed and constructed with the second phase of the Medical Center (approximately 2028), is intended to accommodate patient transport to the hospital for emergency care and also to transport of patients to other area hospitals where a higher level of emergency care is available. The heliport will feature a broom finish concrete landing area, an eight-foot-wide connecting concrete pathway, and pavement markings, signage, and lighting as required by the California Department of Transportation Aviation

Division and the Federal Aviation Administration.

The third entitlement is a request for approval of a Development Agreement Amendment (Attachment 16) to the First Amended and Restated Tier 1 Development Agreement to incorporate in the definition of "entitlements" that are vested the entitlements requested by the project applicant including the Planned Development Permit, the Planned Development Guidelines, the Conditional Use Permit, and the Site Plan. The Development Agreement Amendment also seeks to account for the longer construction timeline for the project, memorialize agreements relative to project site access, memorialize agreements relative to water infrastructure and connections, memorialize agreements relative to street and landscape frontage maintenance, memorialize agreements relative to preparation for the future Class I trail, and acknowledge the potential for future modifications to the Medical Center that may be required by the California Office of Statewide Health Planning and Development.

The proposed Medical Center includes development of a six-story 400,000-square-foot acute care hospital with 300 beds, two three-story 65,000-square-foot medical office buildings, a 20,000-square-foot central utility plant, and a ground-level heliport facility. With respect to building design, the applicant has submitted Planned Development Guidelines which are intended to provide the architectural framework for development of the Medical Center buildings including guidance relative to building design, building materials, and building colors. Each of the Medical Center buildings will require future Design Review approval by the Planning Commission to ensure consistency with the proposed Folsom Ranch Medical Center Planned Development Guidelines.

The proposed Medical Center, which will be developed in five different phases, is expected to be fully developed within approximately 23 years. The table on the following page lists the specific details regarding each of the five phases of development for the Medical Center:

TABLE 1: FOLSOM RANCH MEDICAL CENTER PHASING EXHIBIT

Phase	Year	Building	Central Plant*	Occupied Area
Phase 1	2023	Medical Office Building 1	NA	65,000 SF
Phase 2	2028	Hospital (100 Beds)	15,000 SF*	160,000 SF
Phase 2	2028	Heliport	NA	NA
Phase 3	2034	Medical Office Building 2	NA	65,000 SF
Phase 4	2030	Hospital Expansion (100 Beds)	NA	120,000 SF
Phase 5	2045	Hospital Expansion (100 Beds)	5,000 SF	120,000 SF
Total Area			20,000 SF*	530,000 SF

*Central Utility Plant areas are estimated and not included in total development area

The proposed development phases and timing are flexible and are based on current estimates and may vary as the Medical Center is developed and population and patient needs dictate. Another factor impacting development timing is the completion of site improvement work and coordination with construction seasons given that the infrastructure and roads needed to serve the project.

The applicant's vision for the Medical Center is to provide a broad range of healthcare services to serve the local community and the greater Sacramento region. Specifically, the Medical Center is expected to provide a variety of medical services including the following:

- Acute-Care Hospital
- Medical Office Buildings
- Outpatient Clinics (including Urgent Care)
- Free-standing Emergency Departments
- Ambulatory Surgery Centers
- Wellness-Related Clinics and Retail Facilities
- Imaging Center and Mobile Imaging and Treatment
- Mobile Trailer Pad(s) for Mobile Prefabricated Modular Structures or Temporary Emergency Management Services

In relation to site design, the six-story hospital building has been centrally positioned in the middle of the project site, with a three-story medical office building located to the north and a three-story medical office building located to the south of the hospital respectively. The central plant building and heliport are located directly to the west of the hospital building adjacent to East Bidwell Street and the U.S. Highway eastbound onramp. The applicant has submitted Planned Development Guidelines for the Medical Center with the goal of creating a medical campus that defines its place within the community, provides an environment that promotes patient safety and accessibility, creates open spaces and safe pedestrian pathways, clearly identifies building entrances and pedestrian pathways, and integrates the project with surrounding development relative to vehicle, pedestrian, and bicycle connectivity. The Planned Development Guidelines also provide specific site design guidance relative to vehicle access, pedestrian access, wayfinding, edge considerations, loading and storage, site furniture, future building pads, and mobile modular structures.

General access to the project site is currently provided by East Bidwell Street and Alder Creek Parkway. Parcel 85A is otherwise undeveloped. The proposed project includes a number of roadway improvements required to serve the project site including the construction of McCarthy Way between Alder Creek Parkway and Placerville Road and the construction of Mercy Drive between McCarthy Way and Westwood Drive. In addition, as part of scheduled Folsom Plan Area Backbone Infrastructure improvements, Westwood Drive will be constructed between Alder Creek Parkway and Placerville Road with a future traffic signal at the intersection of Westwood Drive and Alder Creek Parkway.

Vehicle access to the project site includes three new driveways that will be located on the west side of McCarthy Way and a single driveway that will be located on the east side of East Bidwell Street. A round-a-bout design feature is proposed at the primary driveway entrance at the intersection of McCarthy Way and Mercy Drive. A left-turn pocket is also proposed on southbound East Bidwell Street for sole use by emergency service vehicles to access the East Bidwell Street Driveway.

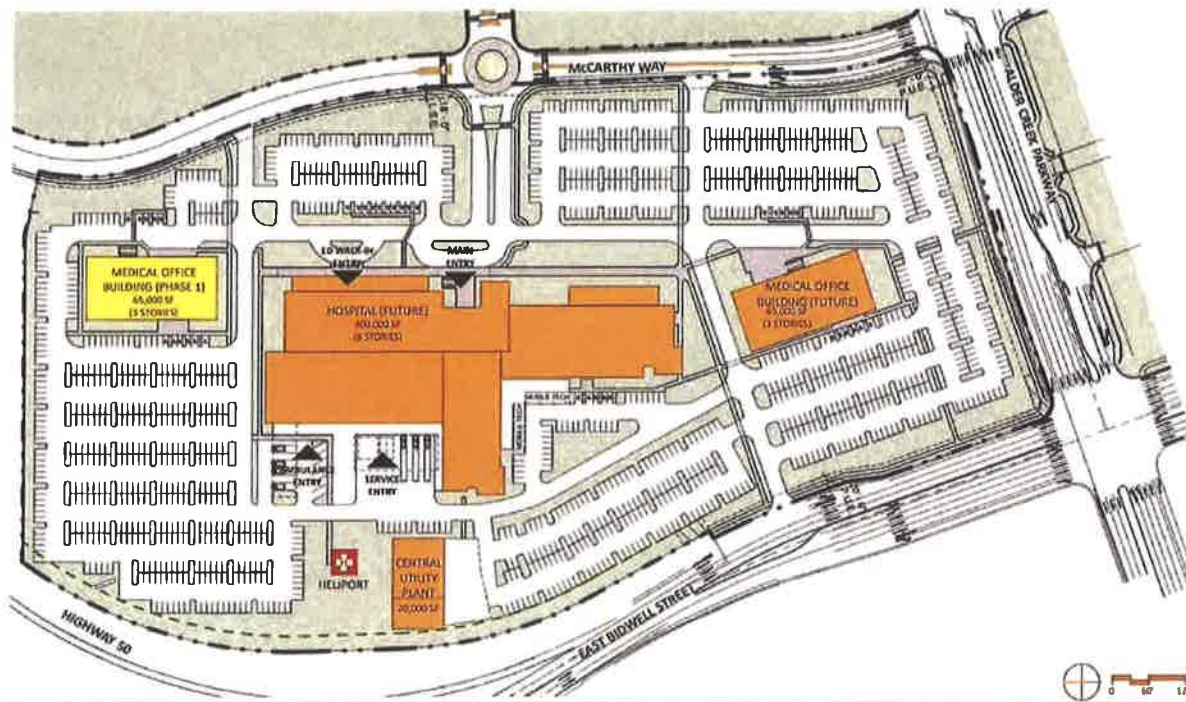
As part of the Planned Development Permit, the applicant requested that three different access scenarios be evaluated for the Medical Center project including: (1) signaling the intersection of Alder Creek Parkway and McCarthy Way; (2) installing a round-a-bout at Alder Creek Parkway and McCarthy Way; and (3) physically restricting left-turn access from eastbound Alder Creek to McCarthy Way with a raised median, with primary eastbound access to McCarthy Way from Alder Creek Parkway being a left-turn movement at Westwood Drive, followed by a left-turn movement at Mercy Drive. The applicant's preferred access to the project site is via the signalized intersection at Alder Creek Parkway and McCarthy Way as they believe this is a critical priority to facilitate both emergency and non-emergency patient and visitor access as well as employee access to the Medical Center.

Upon review of the applicant's preferred access scenario, City staff determined that a traffic signal located at the intersection of Alder Creek Parkway and McCarthy Way would not meet the City design standard for traffic signal spacing (1,320 feet required) as it would only be 700 feet from the intersections of East Bidwell Street/Alder Creek Parkway and Alder Creek Parkway/Westwood Drive respectively. A traffic signal at the intersection of Alder Creek Parkway and McCarthy Way is also not reflected in the Folsom Plan Area Specific Plan. In addition, City staff has concerns regarding potential traffic-related impacts associated with placing a traffic signal at this specific location (discussed in more detail within the Traffic/Access/Circulation portion of this staff report).

It is important to note that the traffic signal at the intersection of Alder Creek Parkway and McCarthy Way would not need to be installed until the Phase 4 (approximately 2034) of the Medical Center project. The configuration of Alder Creek Parkway at the future McCarthy Way (i.e., "S shaped" or "porkchop" median) currently allows for left turn access and the Transportation Impact Study (Attachment 20) concluded that this left turn movement could continue as either an unsignalized or signalized intersection. A microsimulation analysis provided in the appendix of the Transportation Impact Study indicated that, both this intersection along with the East Bidwell Street/Alder Creek Parkway intersection, would operate with acceptable queues and delays if the Alder Creek Parkway/McCarthy Way intersection is signalized. The proposed site plan, which includes the applicant's preferred access scenario, is shown in Figure 3 on the following page.

Planning Commission
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FIGURE 3: PROPOSED SITE PLAN



As shown on the submitted Site Plan above, internal vehicle circulation consists of a series of 25-foot-wide drive aisles that provide access between the four project driveways, the hospital building, and the two office buildings. Pedestrian circulation is provided by new sidewalks located along the street frontages of East Bidwell Street, Alder Creek Parkway, and McCarthy Way as well as by a series of new pedestrian pathways that provide connectivity throughout the project site and to the perimeter sidewalks. A future Class I trail is also located in the northwest corner of the project site adjacent to the U.S. Highway 50 eastbound onramp. Additional on-site improvements include: 1,275 parking spaces (includes combination of uncovered and solar-covered spaces), 56 bicycle parking spaces, electric vehicle charging stations, underground utilities, underground water storage tanks, a heliport, site lighting, site landscaping, retaining walls, and project identification signs.

As noted above, the project site is presently undeveloped. Therefore, the proposed project also includes a number of significant off-site improvements including roadway and utility improvements, grading improvements on three adjacent parcels, construction of a hydromodification basin, construction of a storm drain swale, and excavation of a borrow site. Roadway and utility improvements, which would be constructed along East Bidwell Street, Alder Creek Parkway, Westwood Drive, and Placerville Road include grading of the roadway alignments, and associated storm drain, sewer, water, and dry utilities. Grading improvements include mass grading of three adjacent parcels that are part of the larger Parcel 85A that was recently subdivided; no other development activity is proposed

or approved for these three parcels. Construction of an off-site storm drain outfall swale and an off-site hydromodification basin (Basin No. 8) are proposed to convey and treat storm drainage. The swale is located just west of the Alder Creek Parkway and East Bidwell Street intersection, while the hydromodification basin is southwest of the project site, just north of the proposed Savannah Parkway roadway alignment. Lastly, excavation at a borrow site, which is approximately 400 feet west of East Bidwell Street, is proposed to provide fill material to widen the west side of the East Bidwell Street roadway.

In addition to the off-site improvement referenced above, the proposed project includes modifications to the water delivery system for the Folsom Plan Area in order to provide required emergency water backup storage and sufficient water pressure to serve the proposed project. The 2014 Folsom Plan Area Water System Master Plan indicates that the proposed project site is located within what is identified as the Zone 3 water service area. There are five water zones in the Folsom Plan Area with Zone 5 being furthest east (highest elevation) and Zone 1 being further west (lowest elevation). Based on utility studies prepared for the proposed Medical Center project, it was determined that the required minimum water pressure for the hospital building is 80 psi. In order to achieve the minimum water pressure necessary to serve the project, the applicant is proposing to connect to the Zone 4 water service area. To accommodate shifting the proposed project into the Zone 4 water service area, additional Zone 4 water piping will be required to be installed and the Zone 4 water tank capacity will need to be expanded. A detailed discussion of this topic is covered in the Water Supply and Infrastructure section of this staff report.

ATTACHMENT 3 ANALYSIS

The following sections provide an analysis of the applicant's proposal. Staff's analysis includes:

- A. General Plan and Specific Plan Consistency
- B. Planned Development Permit
 - Development Standards
 - Building Architecture and Design
- C. Conditional Use Permit
- D. Development Agreement Amendment
- E. Traffic/Access/Circulation
- F. Parking and Loading
- G. Noise and Vibration Impacts
- H. Walls
- I. Signage
- J. Site Lighting
- K. Trash/Recycling
- L. Existing and Proposed Landscaping
- M. Oak Tree Impacts
- N. Trails
- O. Grading and Drainage
- P. Frontage Improvements
- Q. Water Supply and Infrastructure
- R. Off-Site Improvements
- S. Conformance with Relevant Folsom General Plan and Folsom Plan Area Specific Plan Objectives and Policies

A. General Plan and Specific Plan Consistency

The 27.44-acre project site has a General Plan land use designation of GC (General Commercial) and a Specific Plan land use designation of SP-GC-PD (Specific Plan-General Commercial-Planned Development Permit District). The project is consistent with both the General Plan land use designation and the Specific Plan land use designation, as hospital and medical offices are identified as a permitted land uses within

the Folsom Plan Area Specific Plan. In addition, the proposed project meets the development requirements established by the Folsom Plan Area Specific Plan (Table A.11) with respect to lot area, building setbacks, landscape coverage, distance between buildings, and parking. However, the proposed project is requesting approval to deviate from the established development standards with respect to maximum building floor area ratio, maximum building height, maximum signage, required bicycle parking, and required vehicle loading/unloading spaces. Development standards for the proposed project are discussed later within the Planned Development Permit section of this staff report.

On March 17, 2020, the City approved a Minor Administrative Modification (MAM) to shift commercial and residential square footage among multiple parcels (Parcels 61, 77, 78, and 85A) located within the Folsom Plan Area including the subject parcel in order to meet the maximum development intent of the properties involved. The resulting Transfer of Development Rights resulted in the subject property being allocated 530,000 gross square feet for a mixture of different commercial land uses (GC-RC, GC-GC, and GC-IND/OP). As discussed in the Project Description, the proposed Medical Center project includes a variety intended land uses including an acute-care hospital, medical office buildings, outpatient clinics, urgent care, free-standing emergency departments, ambulatory surgery centers, wellness related clinics, retail facilities, an imaging center, and mobile imaging and treatment. All the anticipated uses described above are considered permitted land uses on the subject parcel as described within the Folsom Plan Area Specific Plan.

As mentioned in the Project Description section of this staff report, the proposed project includes development and operation of a heliport on the west side of the Medical Center site. The Folsom Plan Area Specific Plan and the Folsom Municipal Code (FMC, Section 17.22.050) do not provide specific guidance as to whether a heliport is considered a non-permitted, permitted, or conditionally permitted land use within a Specific Plan-General Commercial, Planned Development District. However, based upon the nature of the proposed use and its potential impacts, the Community Development Director determined that as part of the proposed project, a heliport would be a permitted use within a Specific Plan-General Commercial, Planned Development District (SP-GC-PD) upon issuance of a Conditional Use Permit by the Planning Commission. A full discussion of the planned heliport is contained within the Conditional Use Permit section of this staff report.

B. Planned Development Permit

The purpose of the Planned Development Permit process is to allow greater flexibility in the design of integrated developments than otherwise possible through strict application of land use regulations. The Planned Development Permit process is also designed to encourage creative and efficient uses of land. The following are proposed as part of the applicant's Planned Development Permit:

- Development Standards and Planned Development Guidelines
- Building Architecture and Design

Development Standards and Planned Development Guidelines

Planned Development Guidelines (Attachment 14) have been submitted which provide the framework for the Planned Development Permit including deviations from the development standards established by the Folsom Plan Area Specific Plan and the Folsom Municipal Code in order to accommodate an acute care hospital, medical office buildings, a heliport, site design and planning, building architecture, landscaping, site lighting, and signage. Specific changes to the development standards include:

1. Increase Maximum Building Floor Area Ratio (FAR) from 0.25 to 0.55
2. Increase Maximum Building Height from 50 feet to 60 feet (Office Buildings)
3. Increase Maximum Building Height from 50 feet to 120 feet (Hospital Building)
4. Sign Criteria Modifications
 - Increase Number of Wall Signs for Office and Hospital Buildings
 - Increase Maximum Sign Area for Office and Hospital Buildings
 - Increase Maximum Letter Height for Office Buildings and Hospital Buildings
 - Allow Illumination of Office and Hospital Building Wall Signs
 - Increase Maximum Height of Freeway Pylon Sign
 - Increase Maximum Sign Area of Freeway Pylon Sign
5. Apply Folsom Municipal Code Requirements for Required Bicycle Parking Spaces
6. Reduce Amount of Required Vehicle Loading/Unloading Spaces

The proposed project includes a request to increase the maximum building floor area ratio (FAR) established by the Folsom Plan Area Specific Plan (FPASP, Table A.11) from 0.25 to 0.55 for the overall Medical Center project site. The applicant has indicated that the proposed increase in maximum FAR is necessary in order to maximize the site and building efficiency for development of the Medical Center. In particular, the applicant states that the proposed six-story hospital and three-story medical office buildings are appropriately sized to meet the health care needs of the surrounding community and that a FAR of 0.55 is required to achieve this goal.

The maximum FAR in the General Plan for GC land uses is 0.5, while maximum FAR established by the Folsom Plan Area Specific Plan for the GC land use category is 0.25. While allowing medical services land uses in the GC land use category, the FPASP did not envision a hospital land use. The General Plan includes a land use designation of Public and Quasi-Public Facility (PQP) which corresponds with the hospital building. PQP land use is defined as supporting the needs of the community by providing health uses, and the FAR for the PQP land uses in the FPASP ranges from 0.2 up to a maximum of 1.0. According to the applicant, the proposed Medical Center will provide significant public amenities and community benefit not only to the City of Folsom, but also to the surrounding region. In order to provide a full-service hospital, a minimum FAR of 0.55 is

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requested by the applicant for the project site to accommodate the grossing and support space anticipated for a full-service hospital. The applicant states that this FAR increase will translate into appropriate building footprints with efficient program layouts and onsite surface parking with open areas for a positive contribution to the visual environment.

Upon review of the submitted site plan, building renderings, and Planned Development Guidelines, staff has determined that the proposed increase in FAR would not result in any visual or aesthetic impacts as the project site is located in an area planned for intensive commercial development in the future. In addition, staff has determined that the proposed increase in FAR would not result in any traffic, access, or circulation-related impacts based on the results of the Transportation Impact Study (Attachment 20) prepared for the project. As a result, staff is supportive of the applicant's proposed FAR increase from 0.25 to 0.55 for the project site to accommodate development of the Medical Center.

The proposed project includes a request to increase the maximum height established by the Folsom Plan Area Specific Plan (FPASP, Table A.11) for the GC land use category from 50 feet to 120 feet. Specifically, the applicant is proposing to increase the maximum building height for the two medical office buildings from 50 feet to 60 feet and to increase the maximum building height for the hospital building from 50 feet to 120 feet (exclusive of mechanical equipment and roof screens). The applicant states that the increase in building height is necessary in order to maximize the site and building efficiency for medical office buildings and a hospital. The applicant also states that the increase in building height is required because medical office buildings and hospitals typically have higher floor to floor dimensions due to interstitial space requirements for mechanical, plumbing, electrical, low voltage and medical gas systems.

In reviewing the request for an increase to the maximum building height for the medical office buildings and hospital, staff took into consideration existing and future land uses in the vicinity of proposed project. The project site is bounded by an undeveloped commercial property (future hotel site) to the north with U.S. Highway 50 beyond, Alder Creek Parkway to the south with an undeveloped commercial property (Shops at Folsom Ranch) and an under construction single-family residential subdivision (Enclave at Folsom Ranch) beyond, East Bidwell Street to the west with an undeveloped commercial property beyond, and future McCarthy Way to the east with undeveloped multi-family residential properties and Westwood Drive beyond.

The southernmost three-story medical office building is located approximately 550 feet from the nearest future single-family residence to the south across Alder Creek Parkway and approximately 400 feet from the undeveloped multi-family property located across future McCarthy Way to the east. The six-story hospital building is located approximately 700 feet from the nearest future single-family residence to the south across Alder Creek Parkway and approximately 350 feet from the undeveloped multi-family properties located across future McCarthy Way to the east. Based on the planned development of commercial uses in the project area, the substantial distance of the proposed buildings

from residential development, and the community benefits associated with the medical campus, staff has determined that the proposed building height increase for the medical office buildings and hospital is appropriate.

The submitted Planned Development Guidelines include a Sign Criteria that proposes several modifications to the standards established by the Folsom Municipal Code (FMC, Chapter 17.59) for hospital and medical office uses including increasing the number of allowable wall signs, increasing the maximum sign area for wall signs, increasing the maximum letter height for wall signs, increasing the maximum height of the proposed freeway pylon sign, and increasing the maximum sign area for the freeway pylon sign. A detailed discussion of the Sign Criteria and the proposed modifications is included in the Signage section of this staff report.

The proposed project includes a request to apply the Folsom Municipal Code requirements for the number of required bicycle parking spaces for both the medical office buildings and the hospital building. In addition, the proposed project includes a request to reduce the number of required vehicle loading and unloading spaces for the medical office buildings and the hospital building. A thorough review of these proposed modifications is contained with the Parking section of this staff report.

Building Architecture and Design

As detailed in the Project Description section of this report, the proposed Medical Center project includes development of a six-story hospital building, two three-story medical office buildings, a single-story central plant building, and two mobile modular structures. The submitted Planned Development Guidelines contain architectural guidelines which provide the framework for development of the medical campus buildings including guidance relative to building design, building materials, and building colors. Listed below are some of the specific recommendations from the Guidelines:

- Buildings shall be well-proportioned, constructed of high-quality materials, and should demonstrate an attention to care and craftsmanship.
- Building forms shall distinguish main public entries versus emergency and staff entrances.
- Hierarchy and rhythm should be used to create building forms that are cohesive, balanced, and approachable.
- Articulation and naturally cast shadows are important aspects of building facade design that can animate and bring visual interest to built structures.
- Buildings on this campus should exhibit a sound understanding of massing including a base, middle, and top massing, the human scale, and the macro scale.

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- High quality, emphasis on natural materials that demonstrate professional craftsmanship.
- Exterior building materials shall form a complimentary palette of textures and colors. Warm-tone, approachable materials and colors shall establish the primary palette.
- Natural stone of hewn, rough cut, flame honed or polished texture; integrally colored synthetic plaster, textured finish; pre-finished metal panels of composite or plate aluminum construction shall constitute the primary exterior wall finishes and accent materials.

Shown in the figures below and on the following pages are conceptual building renderings of the medical office and hospital buildings.

FIGURE 4: NORTHWESTERN VIEW OF ENTIRE MEDICAL CENTER



FIGURE 5: WESTERN ELEVATION OF HOSPITAL BUILDING

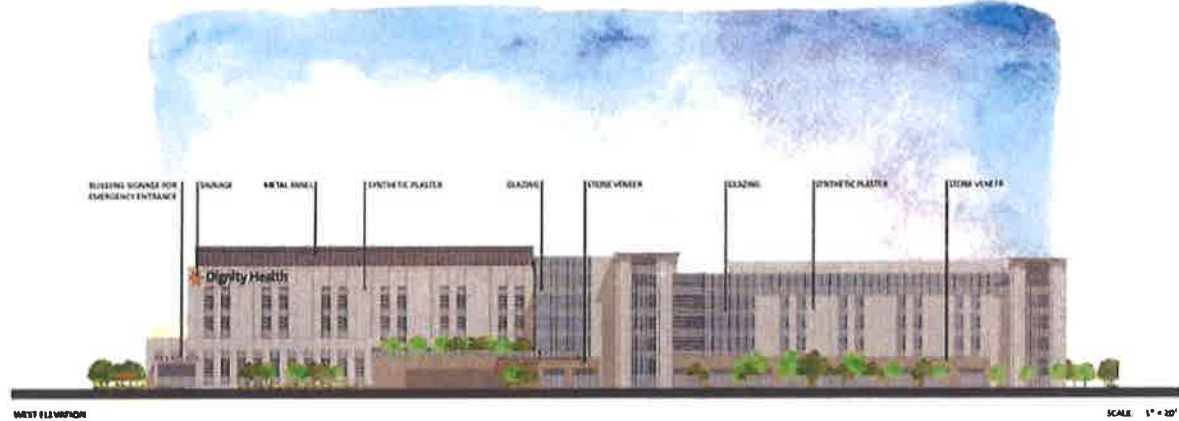


FIGURE 6: EASTERN ELEVATION OF HOSPITAL BUILDING

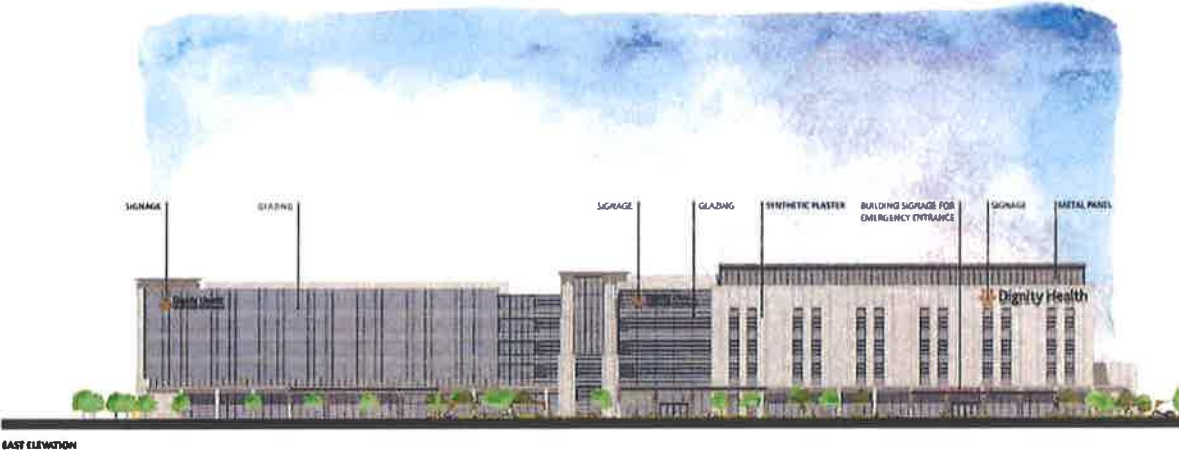


FIGURE 7: NORTHERN ELEVATION OF HOSPITAL BUILDING

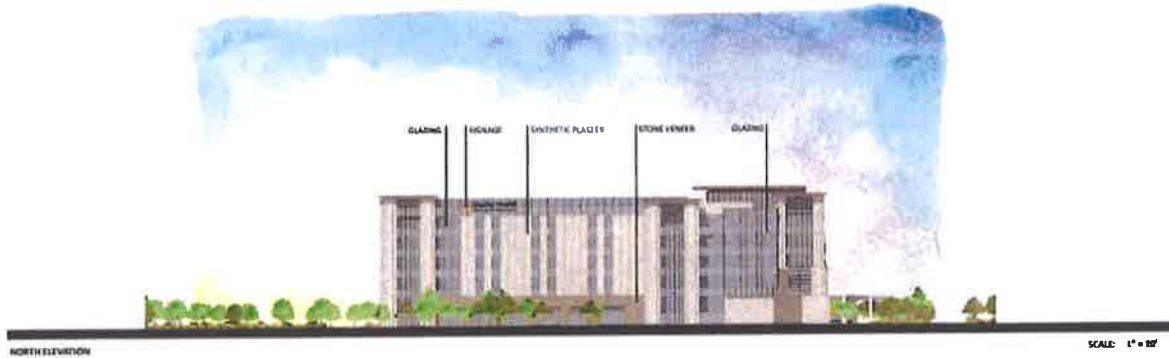


FIGURE 8: SOUTHERN ELEVATION OF HOSPITAL BUILDING



As no specific architectural and design details for the Medical Center were submitted with the subject development application, each of the buildings will require future Design Review approval by the Planning Commission to ensure consistency with the proposed Planned Development Guidelines. Condition No. 47 is included to reflect this requirement.

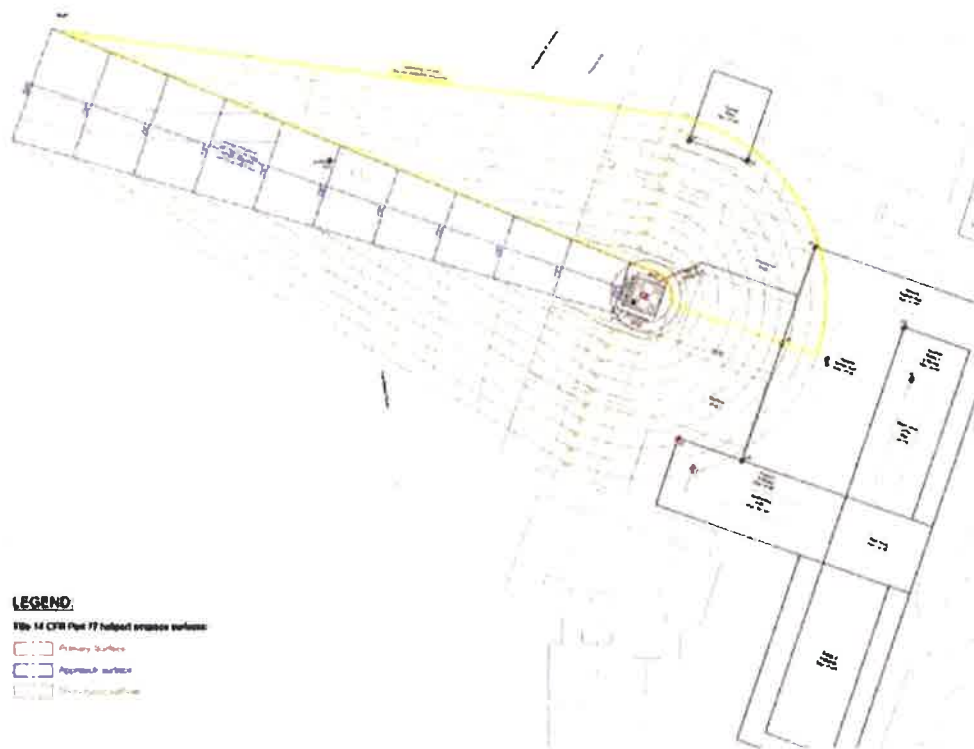
C. Conditional Use Permit

The applicant is requesting approval of a Conditional Use Permit for development and operation of a ground-level heliport within the western portion of the project site. The heliport, which will be constructed with the Phase 2 of the Medical Center (2028), is intended to accommodate patient transport to the hospital for emergency care and also transport of patients to other area hospitals where a higher level of emergency care is available. In terms of operation, the heliport is expected to accommodate 1-2 flights per week, however, there may be fluctuations based on demand. In addition, the heliport will

be available to accommodate flights seven days per week and 24 hours per day. With respect to design, the heliport will feature a broom finish concrete landing area, an 8-foot-wide connecting concrete pathway, and pavement markings, signage, and lighting as required by the California Department of Transportation Aviation Division and the Federal Aviation Administration.

The heliport will be designed to accommodate aircraft similar to the Airbus H145 helicopter model and constructed and operated in accordance with the guidance and requirements of the Federal Aviation Administration (FAA) and the California Department of Transportation (Caltrans). Final approach and takeoff area would be coordinated with FAA and Caltrans and will incorporate required lighting requirements for safe landing and departure of helicopters. The heliport would be lighted in accordance with FAA requirements, including, but not limited to, obstruction lighting, landing pad perimeter lighting, and other related lighting. Helicopters will use typical running lights, which would include red and green right-of-way lights on the sides of the aircraft and a strobe light to indicate the helicopter's position in low-visibility conditions. The preliminary design for the heliport including conceptual standards for landing and takeoff are shown in Figure 9 below:

FIGURE 9: PRELIMINARY HELIPORT DESIGN



In order to approve this request for a Conditional Use Permit to develop and operate a heliport at the Folsom Ranch Medical Center, the Commission must find that the "establishment, maintenance, or operation of the use or building applied for will not, under the circumstances of the particular case, be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood of such proposed use, or be detrimental or injurious to property and improvements in the neighborhood, or to the general welfare of the City."

In reviewing the request for a Conditional Use Permit, staff also took into consideration potential impacts associated with helicopters landing and taking off from the heliport as this represents a new noise source in the project area. The effects of helicopter noise on the surrounding project area were evaluated in a project-specific *Helicopter Noise Technical Report* prepared by Crawford Murphy & Tilly (Appendix G to the Addendum/Attachment 22). The Noise Report utilized the Aviation Environmental Design Tool (AEDT), which is the Federal Aviation Administration (FAA)-approved noise model for quantifying aircraft noise and includes parameters such as the number of anticipated helicopter operations, flight paths used to access the heliport, specific helicopter types, and the time of day at which operations are expected to occur.

The Noise Report determined that the 60 dB contours for helicopter noise would not extend beyond 200 feet from the proposed heliport location, which is primarily within the limits of the proposed Medical Center property and does not include any existing or potential future residential land uses. Therefore, no existing or planned off-site residential uses would be exposed to helicopter noise levels that exceed the City's 60- and 65-dB standards for low-density and high-density residential land uses, respectively. In addition, the Noise Report determined that the proposed heliport operations would not result in any new significant noise-related impacts that were not previously addressed by the Folsom Plan Area Specific Plan EIR/EIS.

Based on this information, staff supports the Conditional Use Permit for development and operation of a heliport at the Folsom Ranch Medical Center as proposed. However, in the event the City finds evidence that the heliport use has resulted in a substantial adverse effect on the health, and/or general welfare of users of adjacent or proximate property or has a substantial adverse impact on public facilities or services, the Community Development Director will refer the use permit to the Planning Commission for review. If, upon such review, the Planning Commission finds that any of the above-stated results have occurred, the Commission may modify or revoke the Conditional Use Permit. Condition No. 8 is included to reflect this requirement.

D. Development Agreement Amendment

The City and Landowner's predecessor (Eagle Commercial Partners, LLC) previously entered into the First Amended and Restated Tier 1 Development Agreement By and Between the City of Folsom and Landowner Relative to the Folsom South Specific Plan on July 15, 2014. Section 1.5 of the Restated Development Agreement allows the

Restated Development Agreement to be amended from time to time by mutual written consent of the parties. On November 12, 2015, Eagle Commercial Partners, LLC and the City entered into Amendment No. 1 to First Amended and Restated Tier 1 Development Agreement Relative to the Folsom South Specific Plan. The applicant is proposing Amendment No. 2 to the First Amended and Restated Development Agreement by and between the City of Folsom and Dignity Health.

As described above, the applicant is requesting approval of a Development Agreement Amendment (Attachment 16) to the First Amended and Restated Tier 1 Development Agreement to incorporate in the definition of "entitlements" that are vested the entitlements requested by the project applicant including the Planned Development Permit, the Planned Development Guidelines, the Conditional Use Permit, and the Site Plan. The Development Agreement Amendment also seeks to take into account the longer construction timeline for the project, memorialize agreements relative to project site access, memorialize agreements relative to water infrastructure and connections, memorialize agreements relative to frontage landscaping and maintenance, memorialize agreements relative to preparation for the future Class I trail, and acknowledge the potential for future modifications to the Medical Center that may be required by the California Office of Statewide Health Planning and Development.

One of the primary purposes of this Development Agreement Amendment is to provide flexibility in the approval and term for subsequent entitlements, specifically in relation to development of the Folsom Ranch Medical Center. This flexibility is required because development of the Medical Center presents significant benefits to the City and the region and there are unique characteristics in the buildout of the Medical Center buildings and related structures including the following:

- Development of the Medical Center will occur over a long period of time, with a phased timeline for construction and potential adjustments to physical structures as medical delivery systems change over time.
- The California Office of Statewide Health Planning and Development ("OSHPD") is required to approve the hospital building design which may result in required changes to the design of the Medical Center buildings and related structures.
- The Medical Center will generate significant employment and other economic benefits to the City.
- The Medical Center will provide needed expansion of access to health care services for the City and other jurisdictions in the region.
- A significant capital investment is required for the Medical Center buildings and related structures; and
- The status of Landowner as a nonprofit public benefit corporation.

In light of the unique circumstances associated with the proposed Medical Center referenced above, the term of the Development Agreement Amendment is proposed to be extended to June 30, 2056 or until the 530,000-square-foot Medical Center has been built out, whichever is later.

The Development Agreement Amendment also address issues related to site access, agreements relative to water infrastructure and connections, agreements relative to street lighting and landscape frontage maintenance, and agreements relative to preparation for the future Class I trail. Each of the specific elements of the Development Agreement Amendment referenced above are discussed in detail within various sections of this staff report. City staff is supportive of the Development Agreement Amendment as proposed. The Planning Commission will be making a recommendation regarding the Development Agreement Amendment to the City Council as Development Agreements require City Council review and approval.

E. Traffic/Access/Circulation

The Folsom Plan Area Specific Plan established a series of plans and policies for the circulation system within the entire Plan Area. The FPASP circulation system was designed with a sustainable community focus on the movement of people and provides a number of mobility alternatives such as walking, cycling, carpooling, and viable forms of public transportation in addition to vehicular circulation. The circulation plan evaluated regional travel, both in terms of connectivity and capacity as well as local internal connections and access. The circulation plan also addressed the concerns of regional traffic, including parallel capacity to U.S. Highway 50, and connectivity with surrounding jurisdictions while considering community-wide connectivity, alternative modes of travel, and the provision of complete streets.

The 2011 Folsom Plan Area Specific Plan Environmental Impact Report/Environmental Impact Statement included not only a detailed analysis of traffic-related impacts within the Plan Area, but also an evaluation of traffic-related impacts on the surrounding communities. In total, there are fifty-five traffic-related mitigation measures associated with development of the FPASP which are included as conditions of approval for the Folsom Ranch Medical Center project. Many of these mitigation measures are expected to reduce traffic impacts to East Bidwell Street. Included among the mitigation measures are requirements to: fund and construct roadway improvements within the Plan Area, pay a fair-share contribution for construction of improvements north of U.S. Highway 50, participate in the City's Transportation System Management Fee Program, and Participate in the U.S. Highway 50 Corridor Transportation Management Association. The Folsom Ranch Medical Center project is subject to all traffic-related mitigation measures required by the 2011 FPASP EIR/EIS (Condition No. 51).

On May 5, 2015, Fehr & Peers completed a Traffic Impact Analysis for the Westland-Eagle Specific Plan Amendment project (an Addendum to the FPASP EIR/EIS was certified in association with the Westland-Eagle Specific Plan Amendment) and

determined that the traffic impacts associated with that project had been adequately addressed in the 2011 Folsom Plan Area Specific Plan EIR/EIS with inclusion of some minor adjustments to account for changes that have occurred since the EIR/EIS was certified. The adjustments include requiring the project to modify the westbound approach to the East Bidwell Street/Iron Point Road intersection to include three left-turn lanes, two through lanes, and one right-turn lane. In addition, the project was required to pay a fair-share contribution towards improvements to the East Bidwell Street/Alder Creek Parkway intersection including the addition of a channelized westbound right-turn lane.

On December 1, 2017, T.KEAR Transportation Planning & Management completed a Transportation Impact Study for the Mangini Ranch Phase 2 Subdivision project to ensure that no additional impacts would occur that were not previously identified and addressed by the 2011 FPASP EIR/EIS and the 2015 Westland-Eagle Specific Plan Addendum to the FPASP EIR/EIS. The Study determined that, with planned street and intersection improvements, the Mangini Ranch Phase 2 Subdivision project would not create any new significant impacts when compared to the FPASP EIR/EIS and the Westland-Eagle Specific Plan Amendment Addendum.

Existing Roadway Network and Conditions

The project site is located at the northeast corner of the intersection of East Bidwell Street and Alder Creek Parkway. Significant roadways in the project vicinity include U.S. Highway 50, East Bidwell Street, Alder Creek Parkway, and Westwood Drive. U.S. Highway 50 is a six-lane east-west highway that passes through Folsom and connects the Sacramento region to Lake Tahoe and points beyond. East Bidwell Street, which is currently a two-lane north-south roadway in the vicinity of the project site, runs from White Rock Road northward until it connects to Riley Street. Alder Creek Parkway is currently a two-lane east-west roadway that travels from East Bidwell Street eastward to Placerville Road and beyond into the Russell Ranch Subdivision. Westwood Drive, which is two-lane north-south roadway that is currently under construction, will eventually run from Mangini Parkway northward up to Placerville Road.

Existing roadway conditions at the McCarthy Way and Alder Creek Parkway intersection include a left-turn pocket on eastbound Alder Creek Parkway at future McCarthy Way and a westbound left-turn pocket on Alder Creek Parkway at the entrance to the Enclave Subdivision. The two existing left-turn pockets are expected to remain in place until such time that they are unsafe or there is a trigger to require modifications to this intersection (e.g., signalization of McCarthy Way). The intersection of East Bidwell Street and Alder Creek Parkway currently features three-way stop-sign control. The westbound approach to East Bidwell Street/Alder Creek Parkway intersection from Alder Creek Parkway includes a right-turn lane and a left-turn pocket. The northbound and southbound approaches to the East Bidwell Street/Alder Creek Parkway intersection both include a through-lane and right-turn and left-turn lanes respectively to provide access to eastbound Alder Creek Parkway. There is currently no traffic control at the intersection of Alder Creek Parkway and Westwood Drive, although there is an eastbound left-turn pocket from Alder Creek Parkway onto future Westwood Drive. An aerial photograph of

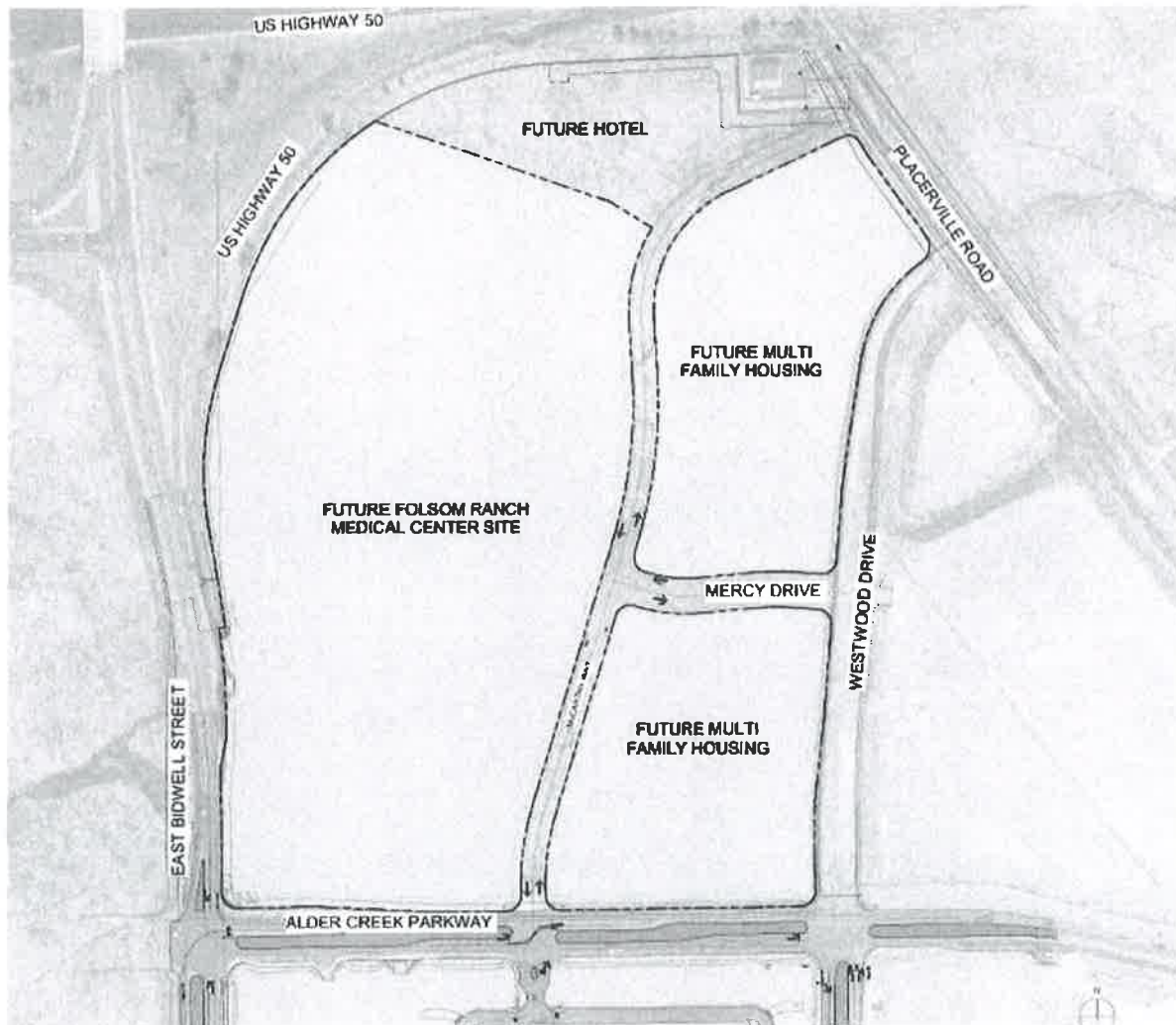
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the existing roadway conditions and an exhibit showing existing conditions plus approved roadways associated with the Parcel 85A Tentative Parcel Map project are shown below and on the following page.

FIGURE 10: AERIAL PHOTOGRAPH OF EXISTING ROADWAY CONDITIONS



FIGURE 11: EXISTING CONDITIONS PLUS APPROVED ROADWAYS



On April 23, 2021, DKS Associates completed a Transportation Analysis (Analysis) and CEQA Impact Study (included as Attachment 20) to evaluate traffic, access, and circulation impacts associated with the proposed project. The Analysis evaluated traffic operations at the following 19 study intersections in the vicinity of the project site:

- East Bidwell Street and Iron Point Road
- East Bidwell Street and Placerville Road
- East Bidwell Street and US 50 WB Ramps
- East Bidwell Street and US 50 EB Ramps
- Placerville Road and Westwood Drive
- Rowberry Way and Alder Creek Parkway
- 1st Street and Alder Creek Parkway

- 3rd Street and Alder Creek Parkway
- East Bidwell Street and Alder Creek Parkway
- McCarthy Way and Alder Creek Parkway
- Westwood Drive and Alder Creek Parkway
- Placerville Road and Alder Creek Parkway
- East Bidwell Street and Old Ranch Road
- East Bidwell Street and Savannah Parkway
- Westwood Drive and Savannah Parkway
- East Bidwell Street and Mangini Parkway
- East Bidwell Street and Regency Parkway
- East Bidwell Street and White Rock Road
- Placerville Road and White Rock Road

Six different scenarios were evaluated in reviewing traffic operations at the 19 study intersections including Existing Conditions (2021) and the five different development phases of the proposed project. The fifth development phase, while planned for 2045 and representing full buildout, was analyzed under Cumulative Conditions (2040) based on the current year travel demand model utilized by the Transportation Analysis.

As discussed in the Project Description, the applicant is proposing the addition of future signalized access control at the intersection of Alder Creek Parkway and McCarthy Way, and City staff is concerned that a signal at this intersection is inconsistent with the approved Specific Plan and City design standards among other things. The Analysis evaluated intersection operations at the intersection of Alder Creek Parkway and McCarthy Way for Cumulative Conditions to determine potential interactions between closely spaced intersections including the potential for queue spillback at upstream adjacent intersections. The Analysis concluded that there would not be a significant impact associated with the future signalization of the Alder Creek Parkway/McCarthy Way intersection, but the Analysis also includes significant assumptions about geometric changes, operational changes and impacts to the proposed Bus Rapid Transit (BRT) service that City staff does not support at this time. A more detailed discussion of project site access as it relates to the proposed traffic signal is provided in the Site Access and On-Site Circulation section of this staff report.

The majority of infrastructure and ultimate roadway cross-sections for the Folsom Plan Area identified in the FPASP have not been constructed yet, however they are critical to handling the expected traffic growth associated with planned development throughout the region including the subject project. The following table shows the programmed infrastructure improvements and expansions identified to correspond with the phased expansion of land use identified for the submitted development application. The table also identifies any improvements either associated with the project, or that become deficient with the addition of project-related trips.

FIGURE 12: PROGRAMMED SCHEDULE OF PLAN AREA INFRASTRUCTURE

IMPROVEMENT	PHASE 1 (2023) ^a	PHASE 2 (2028) ^a	PHASE 3 (2030) ^a	PHASE 4 (2034) ^a	PHASE 5 (2040) ^a
WIDENING OF EAST BIDWELL STREET FROM US 50 TO WHITE ROCK ROAD	C	C	C	C	C
WIDENING OF ALDER CREEK PARKWAY FROM EAST BIDWELL STREET TO PLACERVILLE ROAD	C	C	C	C	C
CONSTRUCTION OF WESTWOOD DRIVE BETWEEN ALDER CREEK PARKWAY AND PLACERVILLE ROAD	C	C	C	C	C
CONSTRUCTION OF ALDER CREEK PARKWAY WEST OF EAST BIDWELL ST INCLUDING INTERSECTIONS OF ALDER CREEK PARKWAY WITH 1 ST STREET AND 3 RD STREET	C	C	C	C	C
CONSTRUCTION OF MCCARTHY WAY BETWEEN ALDER CREEK PARKWAY TO PLACERVILLE ROAD	P	P	P	P	P
CONSTRUCTION OF EMERGENCY ACCESS MIDBLOCK HALF SIGNAL ON EAST BIDWELL STREET	P	P	P	P	P
CONSTRUCTION OF EMPIRE RANCH ROAD US 50 INTERCHANGE	D	C	C	C	C
CONSTRUCTION OF SAVANNAH PARKWAY WEST OF EAST BIDWELL ST	-	C	C	C	C
CONSTRUCTION OF ROWBERRY ROAD US 50 OVERCROSSING	-	C	C	C	C
WIDENING OF WHITE ROCK ROAD FOR CAPITAL SOUTHEAST CORRIDOR	-	-	-	C	C
SIGNALIZATION OF ALDER CREEK PARKWAY AND WESTWOOD DRIVE	-	-	-	-	C

- ^a 'C' indicates an improvement programmed by the City;
 'P' indicates an improvement associated with the Project;
 'D' indicates an improvement required due to deficiency associated with Project trips
 '-' indicates an improvement that is not programmed nor required due to a deficiency

The Analysis projected that the proposed Folsom Ranch Medical Center project is expected to generate a total of 11,220 daily vehicle-trips on weekend days, with 914 trips occurring during the AM peak hour and 1,017 trips occurring in the PM peak hour. Based on the expected number of project-related vehicle trips, the Analysis concluded that the proposed project would not have a significant impact on vehicle level of service (LOS) at any of the study intersections or freeway segments with following two exceptions:

- Alder Creek Parkway/McCarthy Way:
 The Analysis determined that the intersection of Alder Creek Parkway and McCarthy Way would exceed the General Plan threshold for southbound queuing and delay in the PM peak hour under Cumulative Conditions. To address this deficiency, the Analysis recommends that a 250-foot-long channelized southbound left-turn lane be constructed along McCarthy Way leading into an extended

westbound right-turn lane at the intersection of East Bidwell Street and Alder Creek Parkway. To minimize weaving, the Analysis recommends that the southbound right-turn lane be signed and/or striped for vehicles planning to go north on East Bidwell Street.

- U.S. Highway 50:

The Analysis determined that two merge-diverge influence areas on U.S. Highway 50 in the vicinity of the project site would exceed vehicle density thresholds during Phase 1 (2023) development of the proposed project. The Analysis states that this operational deficiency will be addressed as part of the planned Empire Ranch Road/U.S. Highway 50 Interchange Improvement project that is currently programmed in Sacramento Area Council of Government's Metropolitan Transportation Improvement Program.

To address the identified deficiencies and to provide safe and efficient traffic operations, staff recommends that the following measures associated with each of the five proposed development phases be implemented to the satisfaction of the Community Development Department (Condition No. 41):

- Phase 1 Roadway Improvements (2023)
 - The owner/applicant shall construct McCarthy Way between Alder Creek Parkway and Placerville Road along with multiple access points to the site, consistent with City of Folsom Design and Procedures Manual and Improvement Standards and City of Folsom Standard Construction Specifications and Details.
 - The owner/applicant shall construct an additional 250-foot right-turn lane for southbound McCarthy Way approaching Alder Creek Parkway connecting to a right-turn pocket for westbound Alder Creek Parkway approaching East Bidwell Street. The owner/applicant shall also construct a full extension of the right-turn pocket on westbound Alder Creek Parkway from McCarthy Way to East Bidwell Street. The owner/applicant shall adjust the location of the parking lot curb along the south and east edge of the project site accordingly.
- Phase 2 Roadway Improvements (2028)
 - The owner/applicant shall construct a southbound emergency vehicle-only left-turn movement into the East Bidwell Street access point along with installation of a half-signal that will be triggered through emergency signal preemption to stop northbound traffic for enough time for the vehicle to safely complete the turning maneuver.

- Phase 3 Roadway Improvements (2030)
 - No roadway improvements required.
- Phase 4 Roadway Improvements (2034)
 - Should the owner/applicant desire to construct a traffic signal at Alder Creek Parkway/McCarthy Way intersection as an element of Phase 4 development, the owner/applicant shall first be required to perform a supplemental traffic analysis to assess the operational impacts associated with signalization of the intersection.

The dates identified for each phase are planning estimates. The improvements are tied to the actual commencement of each phase.

As shown in the Folsom Plan Area Specific Plan (Figure 7.29), a future transit corridor is planned to be located adjacent to the project site within the Alder Creek Parkway right-of-way. The transit corridor includes a 38-foot-wide planted median that eventually as transit demand increases, will be reduced to 16-feet to allow for construction of two additional travel lanes for either dedicated or mixed-flow regional "Hi Bus" transit service (high frequency bus service). In addition, it anticipated that BRT stops will eventually be placed along eastbound and westbound Alder Creek Parkway to serve the proposed Medical Center project.

Key considerations associated with the future BRT operations in the Alder Creek Parkway right-of-way include consistency with the planned alignment west of East Bidwell Street, the ease of making a right-turn to and left-turn from the planned alignment along Westwood Drive, potential conflicts between transit and general traffic, and safety and accessibility for transit riders. The westbound center-running alignment provides consistency with the Specific Plan and adjacent design alignments while also providing a high level of transit performance with minimal conflicts with other vehicles. It is the preferred alignment in all scenarios. Eastbound BRT operations have the option of either center-running or right-running. The following are considerations associated with eastbound BRT operations:

- While center-running operations provide similar consistency with plans and adjacent designs, it results in lower transit performance due to resulting conflicts with left-turning vehicles at McCarthy Way and at Westwood Drive and a dedicated transit phase at Westwood Drive to make the extended right-turn movement.
- Right-running operations in a Business Access Transit Lane (driveway and right-turn access only) improves transit operations, minimizes conflicts with heavy left-turn volumes, and results in a lower right-of-way impact. The right-running would however require a transition at East Bidwell Street using a dedicated transit queue jump phase and would increase crossing distance for eastbound passengers.

Based on consistency with the approved Specific Plan and Transit Corridor Plan, and the design of adjacent developments, staff recommends that the both the westbound and eastbound BRT operations be center-running within the Alder Creek Parkway right-of-way. Although it will be Sacramento Regional Transit, in consultation with the City, that will determine the ultimate location of BRT, Condition No. 41 is included to reflect this requirement and the Development Agreement Amendment acknowledges the Specific Plan reference to BRT.

The Governors' Office of Planning and Research (OPR) has published guidance recommending a CEQA threshold for transportation impacts of land use projects of a 15 percent Vehicle Miles Traveled (VMT) reduction per capita, relative to either city or regional averages, based on California's Climate Scoping Plan. Qualitative assessment of VMT reduction was determined to be acceptable to screen projects. Under State Law (SB 743), VMT became the only CEQA threshold of significance for transportation impacts on July 1, 2020.

At this time, the City has not adopted VMT thresholds; therefore, the VMT analysis prepared for the proposed project was based primarily on the technical guidance published by the California Office of Planning and Research (OPR) in the *Technical Advisory on Evaluating Transportation Impacts* (Technical Advisory). The OPR Technical Advisory does not include a recommended significance threshold for the proposed land use (i.e., hospital and medical offices). Therefore, the VMT analysis was conducted by separating the medical offices and hospital services and analyzing them independently.

Consistent with OPR Technical Advisory guidance for office land uses, the significance threshold of 85 percent of the existing baseline regional VMT (2016 SACOG regional VMT) per employee was used to analyze employee work-based trips generated by the medical offices of the project. That Analysis found that the VMT associated with work-based land uses of the proposed project and their employees exceed 85 percent of the VMT per employee regional average. The work-based land uses of the proposed project resulted in 14.33 VMT per employee compared to the work-based VMT threshold of 13.69 VMT per employee; thus, resulting in an exceedance of the VMT significance threshold of 4.5 percent. Mitigation Measure 3A.15-2c identified in the FPASP EIR/EIS would reduce impacts related to VMT through participation in the 50 Corridor Transportation Management Association. However, employee participation in this program is voluntary; and thus, the rate of employee participation and the associated VMT reduction attributed to implementation of this mitigation measure is not fully known at this time. Therefore, consistent with Mitigation Measure 3A.15-2c, the Analysis recommended implementation of the following measure (Condition No. 44) to meet the recommended VMT threshold:

- During project operation, and consistent with Mitigation Measure 3A.15-2c, the owner/applicant shall ensure on-going employer membership and participation by Dignity Health in the SACOG 50 Corridor Transportation Management Association (U.S. 50 TMA). In addition, given that employee participation in the U.S. 50 TMA is voluntary, the owner/applicant shall be required to conduct biennial Dignity

Health employee surveys to ensure that at a minimum a 4.5 percent reduction in VMT (or 1,525 daily VMT) is achieved and maintained as part of project operations. Dignity Health shall be responsible for implementing biennial Dignity Health employee surveys to gauge participation with the various employee benefits offered by the U.S. 50 TMA. In order to ensure that the necessary reduction in VMT is being reported and achieved, the surveys shall include questions from which VMT reduction estimates can be estimated (e.g., how many days per week do you take alternative modes of transportation to work? How far do you live from your site of employment? etc.). Surveys and survey results shall be coordinated through and submitted to the U.S. 50 TMA, SACOG, and the City. If the required level of VMT reduction is not achieved, Dignity Health shall work with the City of Folsom and the TMA to identify other demand management related strategies to increase participation in the program and achieve the required reduction in VMT.

Project Access and On-Site Circulation

As discussed in the Project Description, vehicle access to the project site includes three new driveways that will be located on the west side of McCarthy Way and a single driveway that will be located on the east side of East Bidwell Street. A round-a-bout design feature is proposed at the primary driveway entrance at the intersection of McCarthy Way and Mercy Drive. A left-turn pocket is also proposed on southbound East Bidwell Street for exclusive use by emergency access vehicles to enter to the East Bidwell Street Driveway. A Vehicle Access and Circulation Exhibit and Pedestrian Circulation Exhibit associated with the proposed project are shown in Figures 13 and 14 on the following pages.

FIGURE 13: VEHICLE ACCESS AND CIRCULATION EXHIBIT

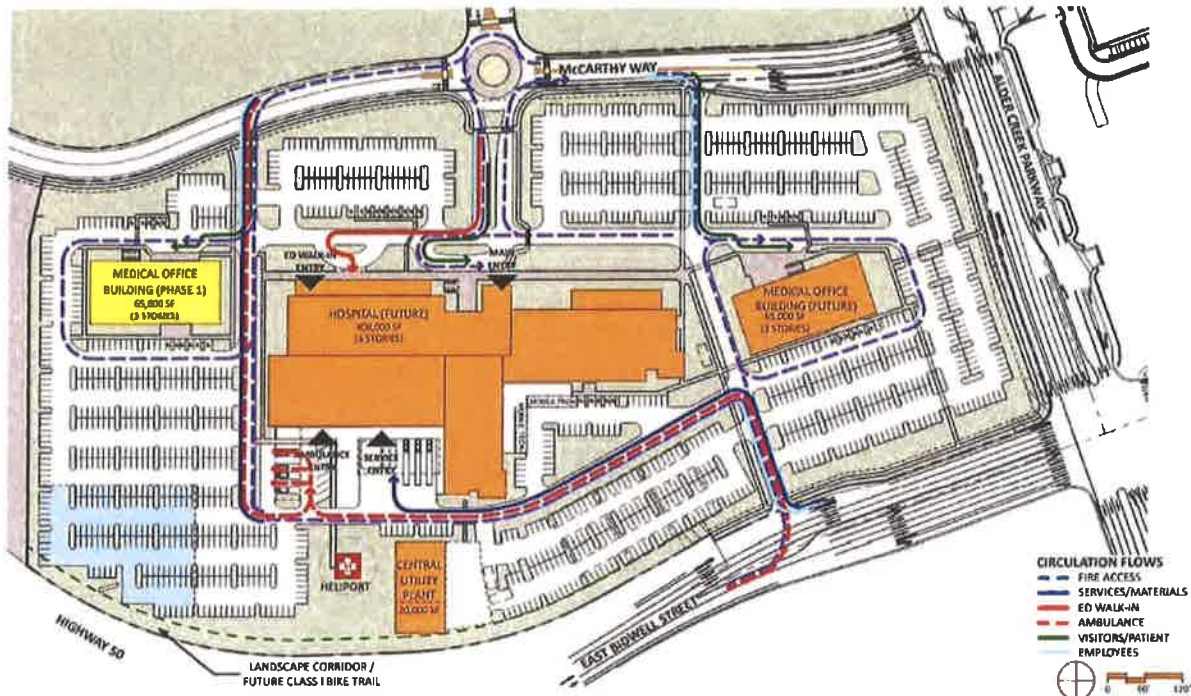
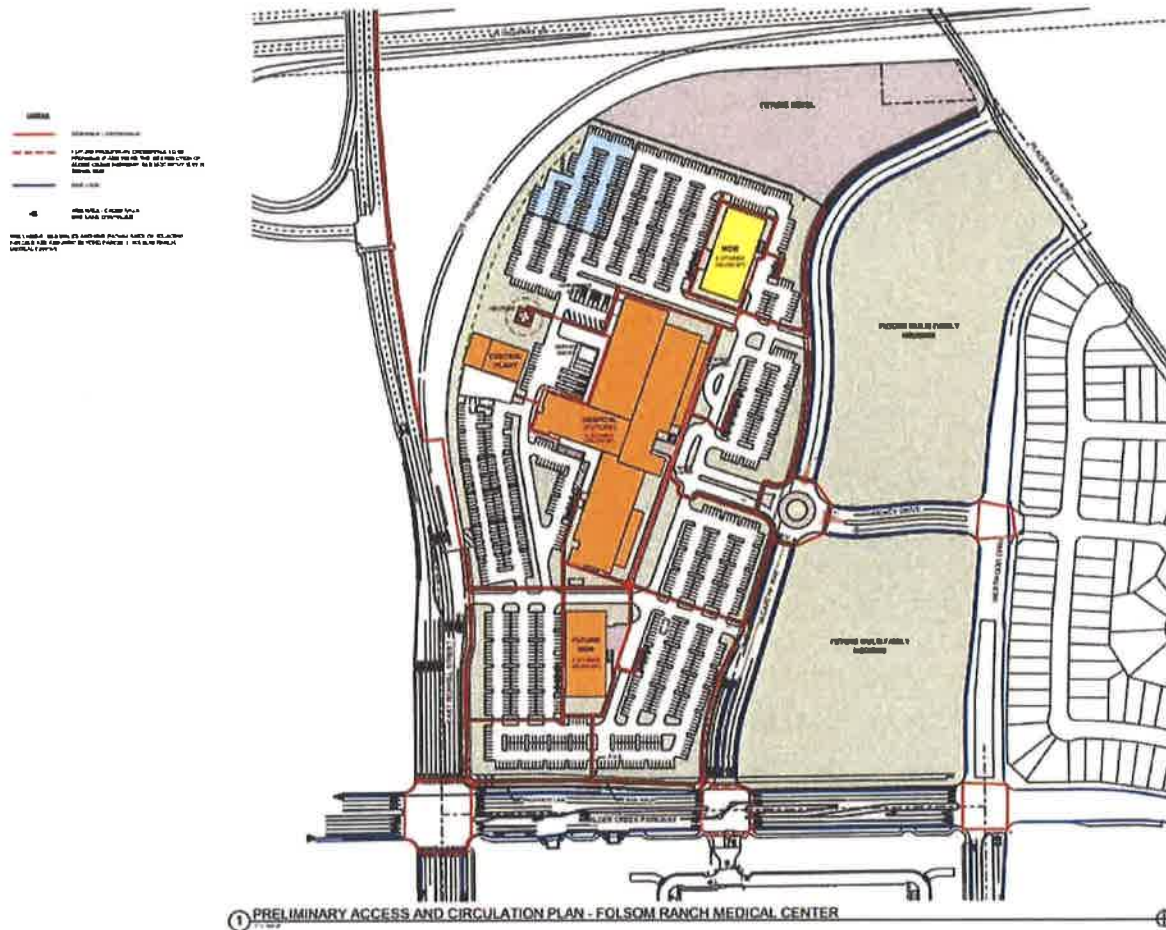


FIGURE 14: PEDESTRIAN CIRCULATION EXHIBIT



As part of the Planned Development Permit, the applicant is requesting that the intersection of Alder Creek Parkway and McCarthy Way be signalized in the future (Phase 4/Approximately 2034) to provide improved access to the project site. The applicant has stated that the signalization of the Alder Creek Parkway/McCarthy Way intersection is an extremely high priority for Dignity Health in order to facilitate both emergency and non-emergency, patient, visitor, and employee access at this location. The applicant has submitted a Memorandum (Attachment 19) which discusses in further detail their preferred access scenario as well as two other alternative access scenarios for the proposed project.

On September 30, 2020, City staff provided a Memorandum (Attachment 18) to the applicant in which staff expressed concerns about the addition of a traffic signal at the intersection of Alder Creek Parkway and McCarthy Way. Given the current level of development in the Folsom Plan Area, staff believes that placing a traffic signal at this location is premature and could significantly limit the City's opportunities to operate and

coordinate the overall traffic signal system in an efficient manner. In addition, staff has concerns regarding design standards and traffic operations, impacts to the future Bus Rapid Transit (BRT) system, and public emergency access that is already planned along East Bidwell Street.

Staff recommends that the intersection of Alder Creek Parkway and McCarthy Way be designed to operate without any traffic controls at this time, other than stop-sign control for vehicles approaching Alder Creek Parkway from southbound McCarthy Way. Staff also recommends that the existing eastbound left-turn pocket on Alder Creek Parkway at McCarthy Way remain in place until such time that traffic conditions become unsafe or excessive delays occur. Condition No. 43 is included to reflect these requirements.

While City staff does not support installation of a traffic signal at the intersection of Alder Creek Parkway and McCarthy Way at this time, staff does acknowledge the operational needs identified by the applicant to install the signal at a future date with development of Phase 4 (2034) of the Medical Center. To facilitate the evaluation and installation of a traffic signal at this intersection in the future, staff recommends the following measures (Condition No. 42):

- The owner/applicant shall submit a Supplemental Traffic Impact Analysis to the City that evaluates the traffic impacts associated with installation of a traffic signal at the intersection of Alder Creek Parkway and McCarthy Way. The scope of the Traffic Impact Analysis shall be subject to review and approval by the Community Development Department. The owner/applicant shall be responsible for all costs associated with preparation of the Supplemental Traffic Impact Analysis.
- If the Supplemental Traffic Impact Analysis concludes that there is no significant traffic operational impact, the traffic signal at the intersection of Alder Creek Parkway and McCarthy Way may be installed by Landowner pursuant to the Conditions of Approval and consistent with technical specifications as approved by the City Engineer.
- If the Supplement Traffic Impact Analysis identifies any significant traffic operational impacts that can be mitigated and the owner/applicant desires to install the traffic signal, the City will identify the required mitigation and the owner/applicant shall install the traffic signal along with the required mitigation.
- In the event the Supplemental Traffic Impact Analysis identifies any significant traffic operational impact that cannot be mitigated, the City will advise the owner/applicant and the traffic signal shall not be installed, and the intersection configuration approved as part of the original entitlements shall remain in place.

- If the City determines that the left-turn access to the Medical Center at the intersection of Alder Creek Parkway and McCarthy Way must be discontinued for any reason, the City shall (a) advise owner/applicant of the City's intention to discontinue access, (b) provide owner/applicant with a reasonable opportunity to comment on the City's determination to discontinue access, including an appeal to the City Council, and (c) consider the impact on safe and efficient public access to the hospital and emergency services, including any evidence owner/applicant may provide to City with respect to increased driving times.

City staff evaluated the operation and configuration of the project access system in terms of driveway spacing, driveway throat depth, and on-site circulation. City staff determined that the three proposed project driveways located on McCarthy Way and the single project driveway on East Bidwell Street provide adequate spacing from the nearest street intersections and meet the City's Design Standards for driveways located on collector streets and major arterials respectively. City staff also determined that the four proposed project driveways provide sufficient throat depth for inbound and outbound vehicles so as to avoid excessive vehicle queuing into the project site and onto adjacent public streets.

City staff also considered on-site circulation and determined that the project features a well-designed parking lot layout which minimizes offset drive aisles and provides adequate drive aisle widths of 25 feet or greater. In addition, City staff determined that there are abundant pedestrian facilities provided by the project including sidewalks, pedestrian walkways, and pedestrian connections which facilitate pedestrian movements in and around the project site.

To ensure implementation of the traffic control measures identified on the submitted site plan, staff recommends the following recommendations be included as conditions of approval for the project (Condition No. 43)

- "Stop" signs and appropriate pavement markings shall be installed at the exits to the four project driveways located on McCarthy Way and East Bidwell Street respectively to the satisfaction of the Community Development Departments.
- Additional warning signs and pavement markings (stamped concrete, brick pavers, etc.) shall be installed at the East Bidwell Street project driveway to alert pedestrians and bicyclists that emergency vehicles will be entering and exiting this driveway on a frequent basis and to be alert for incoming and outgoing vehicles to the satisfaction of the Community Development Department.
- Brick pavers, stamped concrete, or another type of similar colored material (ADA compliant) shall be used to designate pedestrian crosswalks on the project site, in addition to where pedestrian paths cross drive aisles, and shall be incorporated as a design feature at the four project driveway entrances to the satisfaction of the Community Development Department.

- Should the City determined that the existing eastbound left-turn pocket on Alder Creek at McCarthy Way needs to be eliminated, the owner/applicant shall be responsible for the installation of a full concrete median through the intersection and any other improvements associated with elimination of the left-turn pocket. The owner applicant shall also construct a pedestrian-actuated midblock protected crossing to coincide with implementation of BRT service to allow for safe pedestrian access to the BRT stop(s). Lastly, the owner/applicant shall construct a second eastbound left-turn lane at the intersection of Alder Creek Parkway and Westwood Drive. The owner/applicant shall be responsible for a "fair share" contribution for the aforementioned improvements, with reimbursement or fee credit provided for any portion that the City deems above the owner/applicant's responsibility.

F. Parking and Loading

As noted in the Project Description, the proposed project includes development of a 400,000-square-foot (300 beds) hospital building and two, 65,000-square-foot medical office buildings. The Folsom Plan Area Specific Plan (Table A.14) requires that hospital buildings provide one parking space per every two hospital beds and that medical office buildings provide one parking space per every 1,000 square feet of floor area. As shown and described on the submitted site plan, the proposed project includes a total of 1,275 parking spaces including a combination of uncovered and solar-covered spaces. Staff has determined that the proposed project meets the parking requirements prescribed by the Folsom Plan Area Specific Plan by providing 1,275 parking spaces whereas a minimum of 990 parking spaces are required.

The Folsom Plan Area Specific Plan (Table A.14) requires that office uses provide two bicycle parking spaces per 20 required vehicle parking spaces and one additional parking space for every ten additional vehicle parking spaces provided. The Folsom Municipal Code (FMC, Section 17.57.090), requires office uses to provide a minimum of five bicycle parking spaces for up to 25 required vehicle parking spaces and an additional bicycle parking space for every ten additional required vehicle parking spaces, with a maximum of 20 bicycle parking spaces being required. The Folsom Plan Area Specific Plan and the Folsom Municipal Code do not have any bicycle parking requirements for hospital uses.

The submitted site plan indicates that the proposed project will include a total of 56 bicycle parking spaces including 20 bicycle parking spaces at each of the medical office buildings and 16 parking spaces at the hospital building. The applicant is seeking approval through the Planned Development Permit process to utilize the parking requirements established by the Folsom Municipal Code (FMC Section 17.57.090) rather than the parking requirements established by the Folsom Plan Area Specific Plan for the proposed project. The applicant states that the parking requirements established by the Folsom Municipal Code are more applicable to the proposed project as most of the patients, visitors, and employees traveling to the Medical Center will be arriving by vehicle. The proposed

project meets the bicycle parking requirements established by the Folsom Municipal Code by providing 56 bicycle parking spaces whereas 40 bicycle parking spaces are required. Staff supports the applicant's request to utilize the parking requirements of the Folsom Municipal Code based on their experience with bicycle usage at their other medical facilities in the Sacramento region.

As stated above, staff is supportive of the applicant's request to utilize the parking requirements of the Folsom Municipal Code. However, in addition to providing bicycle parking spaces in designated spaces located outside of the medical office and hospital buildings (considered short-term bicycle parking), staff recommends that additional secured locations (bicycle storage room, bicycle storage locker, etc.) within or adjacent to the medical office and hospital buildings be provided to allow for long-term bicycle storage for employees. Condition No. 46 is included to reflect this requirement.

The Folsom Plan Area Specific Plan (Table A.14) requires three loading/unloading spaces for office uses ranging in size from 50,000 to 99,000 square feet and three loading/unloading spaces for hospitals that range between 400,000 to 500,000 square feet. The applicant is seeking approval through the Planned Development Permit process to provide a total of six loading/unloading spaces for the entire Medical Center whereas nine loading/unloading spaces are required. The applicant states that the reduction in loading/unloading spaces is requested to minimize anticipated volumes and frequencies of truck traffic based on Dignity Health's "just in time" delivery program. The applicant also states that high volumes of deliveries and truck traffic will be discouraged at the Medical Center as they stress the hospital system with the need for excessive storage areas. Staff supports the applicant's request to reduce the number of required loading/unloading spaces at the Medical Center based on their experience with loading and unloading facilities at their other medical centers in the Sacramento area.

G. Noise and Vibration Impacts

Potential noise impacts that might result from development of the Folsom Ranch Medical Center project are construction-related activities and operational activities. Construction-related noise would have a short-term effect, while operational noise would continue throughout the lifetime of the project. Construction activities under the project would require similar types and numbers of equipment operating at similar levels of intensity as already contemplated in the FPASP EIR/EIS. The closest sensitive receptors to the project are single-family residences (Enclave at Folsom Ranch) currently being constructed south of Alder Creek Parkway, approximately 150 feet away from the nearest project site boundary. If these residences are occupied during project construction, residents would experience a temporary increase in ambient noise level resulting from construction activities.

The City's Noise Ordinance excludes construction activities from meeting the General Plan Noise Element standards, provided that all phases of construction are limited to the hours between 7:00 a.m. and 6:00 p.m. on weekdays, and between 8:00 a.m. and 5:00

p.m. on Saturdays. To ensure compliance with the City's Noise Control Ordinance and General Plan Noise Element, staff recommends that hours of construction operation be limited from 7:00 a.m. to 6:00 p.m. on weekdays and 8:00 a.m. to 5:00 p.m. on Saturdays with no construction permitted on Sundays or holidays. In addition, staff recommends that construction equipment be muffled and shrouded to minimize noise levels. Condition No. 32 is included to reflect these requirements.

In compliance with FPASP EIR/EIS Mitigation Measure 3A.11-4, a site-specific Noise Analysis (Appendix F of Addendum) was conducted by Bollard Acoustical Consultants, Inc. in 2021 for the proposed project to evaluate future traffic noise levels and stationary operational noise levels associated with the proposed project. The Noise Analysis determined that the project would comply with the City's interior noise standard of 45 dB for hospitals as well as California Building Code interior noise standard for hospitals of 45 dBL with respect to traffic noise levels. The Noise Analysis also evaluated potential noise impacts associated with truck movement throughout the project site. The results of the truck-related noise measurements show that on-site truck circulation noise would exceed the City's interior noise level standard within the northern portion of the hospital building during nighttime hours. To address this potential impact, the Analysis recommends that windows on the northern façade of the hospital be upgraded to STC 35 or that heavy trucks be prohibited from utilizing the drive aisle located immediately north of the hospital during nighttime hours. Condition No. 51 is included to reflect this requirement.

The primary stationary noise sources associated with operations of the hospital and medical office buildings would include parking lots, loading dock operations, central plant equipment, and heating, ventilating, and air conditioning (HVAC) equipment. Ambulances would also generate noise while arriving and departing the site. However, noise generated by ambulance sirens is exempt from City noise standards. The Noise Analysis determined that noise associated with loading dock activity would exceed the City's interior noise level standard during nighttime hours along the western portion of the hospital building. To address this potential impact, the Analysis recommends that for noise sensitive areas of the hospital that are located directly adjacent to the loading dock area, windows on the western façade of the hospital within 100 feet of the loading docks should be upgraded to STC 35. Condition No. 51 is included to reflect this requirement.

Noise-generating mechanical equipment associated with hospital operations (pumps, boilers, compressors, generators, etc.) would be housed within the central plant building located along the western edge of the project site. The central plant building would have to provide at least 50 dB of sound attenuation in order to comply with the City's noise standard for hospitals. Because the specific interior configuration and proposed construction of the central plant building is unknown at this time, the Noise Analysis recommends that the central plan building provide at least 50 dB of sound attenuation in the 63 to 4,000 Hertz frequency bands through building design/configuration and/or by using certain construction materials. Condition No. 51 is included to reflect this requirement.

Impacts from potential exposure of sensitive receptors to construction-related short-term groundborne noise and vibration were analyzed under Impact 3A.11-3 of the FPASP EIR/EIS. The FPASP EIR/EIS identified bulldozing and blasting activities as the sources of maximum groundborne noise and vibration levels that would result from the construction of the FPASP. The project would require some blasting and bulldozing as part of site preparation activities. As mentioned previously, the closest sensitive receptors to the project are single-family residences currently being constructed south of Alder Creek Parkway. These residences are located approximately 150 feet away from the nearest project site boundary, which is close enough to result in structural damage and human annoyance from blasting activities, according to the analysis in the FPASP EIR/EIS. However, bulldozing activities would not result in either structural damage nor human annoyance at this distance. To mitigate the impact, the project would be required to implement FPASP EIR/EIS Mitigation Measure 3A.11-3, which would mitigate groundborne noise and vibration impacts associated with blasting and bulldozing activities. Condition No. 51 is included to reflect this requirement.

H. Walls

The proposed project includes a combination of retaining walls and screen walls. A retaining wall, which ranges from 3 feet to 7 feet in height is proposed along the project's northern boundary. A screen wall, whose height is not known at this time, is proposed to shield the outdoor mechanical yard located adjacent to the central plant building in the western portion of the project. The submitted Planned Development Guidelines recommend that all proposed screen walls, retaining walls, and fences be designed to have consistent materials, styles, and colors to compliment the buildings within the Medical Center. Staff recommends that the final location, design, height, materials, and colors of the retaining walls and screen walls be subject to review and approval by the Community Development Department to ensure consistency with the Folsom Ranch Medical Center Planned Development Guidelines. Condition No. 17 is included to reflect this requirement.

I. Signage

A Sign Criteria has been submitted as part of the Planned Development Guidelines for the purpose of ensuring appropriately designed signage and environmental graphics to provide a clear, comforting, and welcoming experience to visitors. The applicant states that the application of these signage guidelines is a key component in creating an initial impression which conveys the quality of care, professionalism, and promise of excellence provided at the Folsom Ranch Medical Center. The Sign Criteria includes a variety of different sign types to provide proper identification for the Folsom Ranch Medical Center including monument signs, porte cochere signs, freestanding pylon signs, skyline signs, distant read skyline signs, monument directional signs, two-post direction signs, and two-bladed signs. In addition, the Sign Criteria includes provision of alternative types of identification for the Medical Center including sign pageantry and religious symbols.

The applicant indicates that the selection of sign types and placement of signs are dependent on the viewer distance from the sign, rate of motion of the viewer, and critical wayfinding decision points. Freestanding Pylon and Distant Read Skyline, sign types are intended to be viewed from U.S. Highway 50, while Skyline sign types are used to reinforce the wayfinding sequence at connector streets. Monument signs are best placed at entry points to the campus, while Monument Directional signs are viewed from a vehicle traveling at lower speed within the campus boundaries. Onsite digital informational signage is proposed at Freestanding Pylon, Monument and Monument Directional sign types. As the proposed project is phased, Distant Read Skyline signage may be installed at one building and later be moved to another building as wayfinding sequences and criteria may be altered with each successive phase. The exhibits below and on the following page show the conceptual locations of the major signs and the different types of signs proposed for the Medical Center.

FIGURE 15: CONCEPTUAL LOCATIONS OF MAJOR FREESTANDING SIGNS

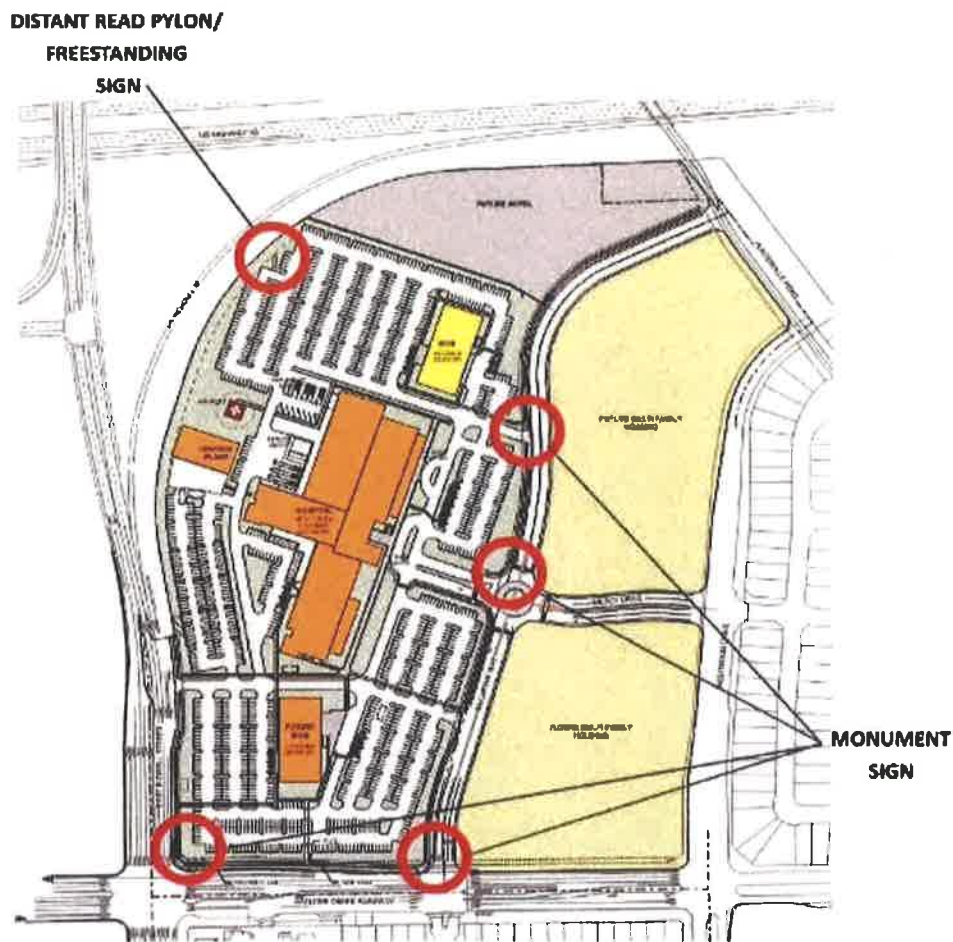
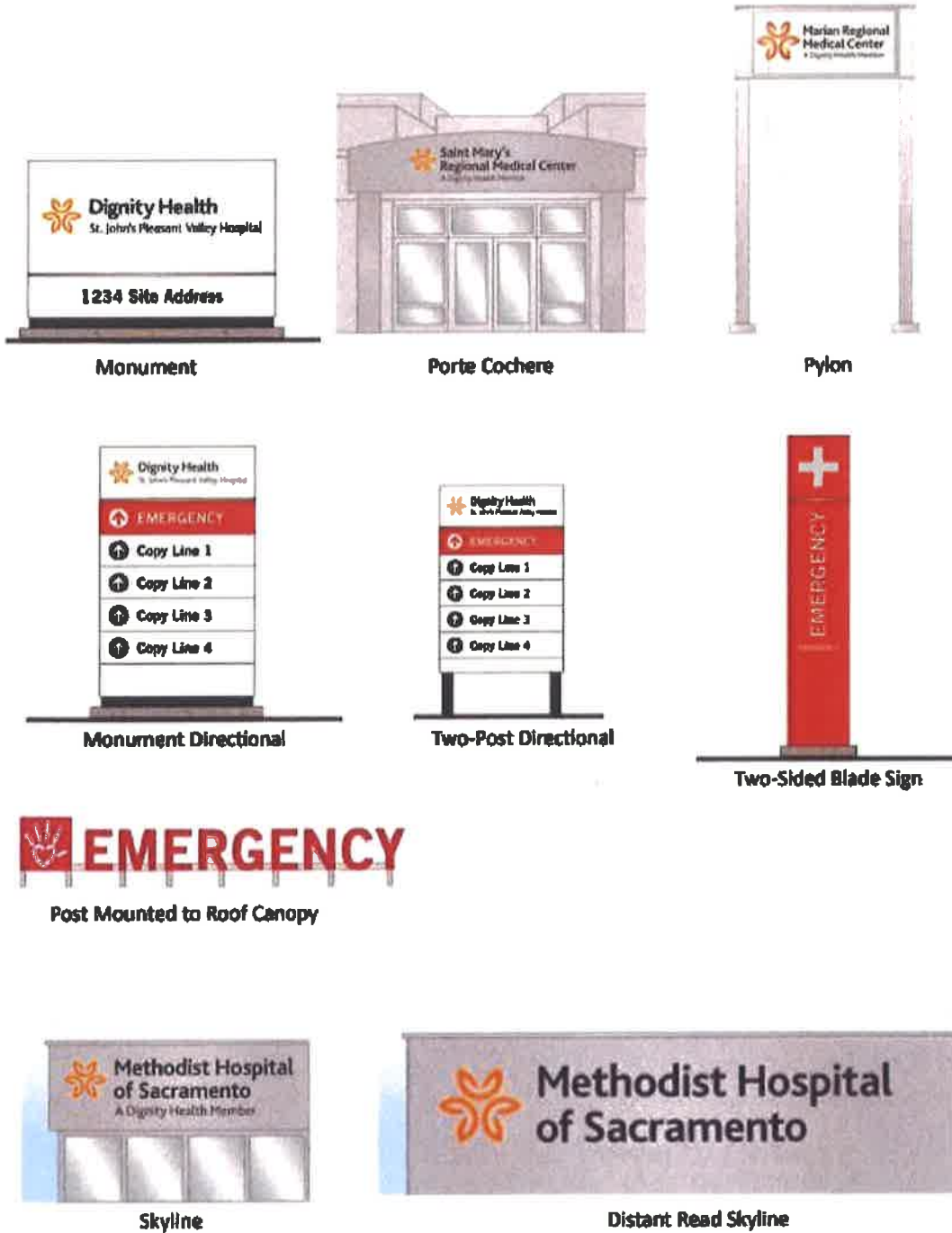


FIGURE 16: CONCEPTUAL SIGN TYPES



As mentioned in the Project Description, the applicant is seeking approval through the Planned Development Permit process to deviate from the sign requirements of the Folsom Municipal Code (FMC, Section 17.59) relative to medical office and hospital uses. The applicant states that the medical campus requires maximum visualization from U.S. Highway 50 and other major roadways in the project vicinity to ensure that patients and families can have a safe wayfinding experience as they negotiate their way to the site. To achieve this goal, the applicant is requesting approval to make the following modifications as part of their Sign Criteria for the Medical Center:

- Increase Number of Wall Signs for Office and Hospital Buildings
- Increase Maximum Sign Area for Office and Hospital Buildings
- Increase Maximum Letter Height for Office Buildings and Hospital Buildings
- Allow Illumination of Office and Hospital Building Wall Signs
- Increase Maximum Height of Freeway Pylon Sign
- Increase Maximum Sign Area of Freeway Pylon Sign

With respect to wall signs, the Folsom Municipal Code (FMC Section 17.59.040 B) states that medical and hospital uses are permitted one non-illuminated wall sign with a maximum sign area of 50 square feet. As stated in the Sign Criteria, the applicant is proposing that a total of 3,750 square feet of wall sign area be provided for all wall signs on the medical campus buildings. The applicant is also requesting flexibility to place more than one sign on each building, although the actual number of signs that would be installed on each building is not known at this time and will be presented with each building design. In addition, the applicant is requesting that the wall signs be permitted to be illuminated with internal LED lighting to ensure that the signs are clearly visible from nearby roadways. Lastly, the applicant is proposing to increase the maximum allowable letter height for the wall signs facing the freeway from 60 inches to 120 inches to allow for the signs to be more legible for motorists traveling on U.S. Highway 50.

With respect to freeway pylon signs, the Folsom Municipal Code (FMC Section 17.59.040 E) states that medical and hospital uses located adjacent to the freeway are permitted one freestanding pylon sign with a maximum height of 18 feet (measured from crown of adjacent highway) and a maximum sign area of 300 square feet. As described in the Sign Criteria, the applicant is proposing one freestanding pylon sign that is 80 feet tall with a maximum sign area of 750 square feet. The exhibits shown on the following pages illustrate the size and scale of the proposed signs.

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FIGURE 17: SIGNAGE SCALE EXHIBIT

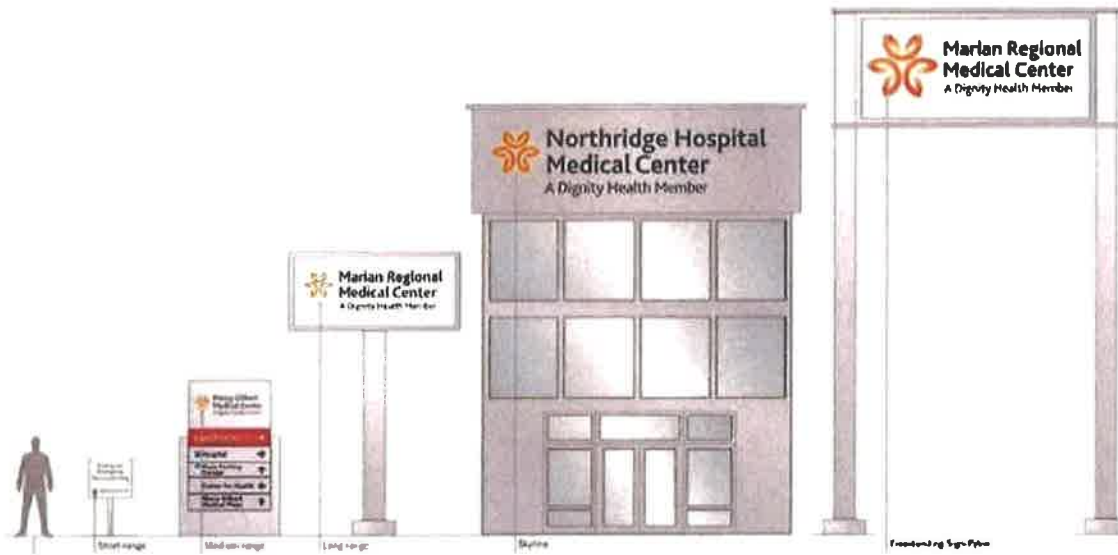


FIGURE 18: DISTANT READ FREEWAY SIGNS (WESTBOUND HWY 50)



FIGURE 19: DISTANT READ FREEWAY SIGNS (EASTBOUND HWY 50)



In reviewing the applicant's request to deviate from multiple sign requirements of the Folsom Municipal Code, staff took into consideration a variety of factors including signage provided for similar medical centers in the Sacramento region and the signage and identification needs for medical centers in general. City staff visited a number of medical centers in the region to observe their respective signage and wayfinding programs including Sutter General Hospital in Sacramento, U.C. Davis Children's Hospital in Sacramento, and Kaiser Permanente Medical Center in Roseville. Based on observations from visits to the aforementioned medical centers, staff concluded that the proposed Sign Criteria for the Folsom Ranch Medical Center is similar (number of signs, size of signs, illumination of signs) to existing signage programs for other medical centers in the region. City staff also recognizes that medical centers provide vital and essential services to communities and that providing adequate signage and wayfinding is a critical operational element. To ensure design and scale consistency with the future medical office and hospital buildings, staff recommends that the location of all wall signs be shown on the building elevations and renderings that are submitted for future Design Review Approval by the Planning Commission. In addition, staff recommends that the owner/applicant obtain a sign permit prior to installation of any signs that require a building permit. Condition No. 49 is included to reflect this requirement.

J. Site Lighting

As described in the Planned Development Guidelines, the applicant is proposing to use a combination of pole-mounted parking lot lighting, building-attached lighting (architecturally integrated down lighting and building-mounted sconce lighting), ground-mounted up lighting, and bollard lights along walkways and landscaped areas on the project site. All lighting would be designed to minimize light/glare impacts to the adjacent properties by ensuring that all exterior lighting is shielded and directed downward. Staff recommends that the final exterior building and site lighting plans be submitted for review

and approval by Community Development Department to ensure consistency with the Folsom Ranch Medical Center Planned Development Guidelines and for evaluation of location, height, aesthetics, level of illumination, glare and trespass prior to the issuance of any building permits. In addition, staff recommends all lighting is designed to be shielded and directed downward onto the project site and away from adjacent properties and public rights-of-way. Condition No. 22 is included to reflect these requirements.

The heliport facility would be lit in accordance with FAA Advisory Circular 150/5390-2C (Heliport Design), Chapter 4 (Hospital Heliports), Section 415 (Heliport Lighting), respectively, including, but not limited to, obstruction lighting, landing pad perimeter lighting, and other related lighting. Helicopters would use typical running lights, which would include red and green right-of-way lights on the sides of the aircraft and a strobe light to indicate the helicopter's position in low-visibility conditions.

K. Trash/Recycling

The applicant is proposing to utilize a combination of public (City of Folsom) and private trash, recycling, and medical waste collection services for the needs of the Medical Center. The proposed project includes a large trash and recycling area located on the west side of the hospital building. The proposed project also may include trash and recycling enclosures to serve the two medical office buildings. Staff recommends that the final trash and recycling collection plan associated with any City required services be reviewed and approved by the Solid Waste Division and the Community Development Department. In addition, staff recommends that the trash and recycling area and trash and recycling enclosures be screened to the satisfaction of the Community Development Department. Condition No. 48 is included to reflect these requirements.

L. Existing and Proposed Landscaping

The project site, which consists primarily of undeveloped grassland with a few scattered non-protected trees and was previously used for cattle grazing. Alder Creek and some minor tributaries/drainage ditches run through the site. The topography of the site consists of gently rolling hills with slopes varying between 0 and 15 percent and surface elevations ranging from 405 to 470 feet above mean sea level. There are a number of protected oak trees located around the off-site hydromodification basin that will be constructed as part of the proposed project.

The proposed project will include landscaping along the project's three street frontages and also landscaping interior to the project site. A landscape buffer (includes sidewalk) is proposed along each street frontage including a 30-foot-wide buffer along Alder Creek Parkway, a 20-foot-wide landscape buffer along East Bidwell Street, and an 18-foot-wide landscape buffer along McCarthy Way.

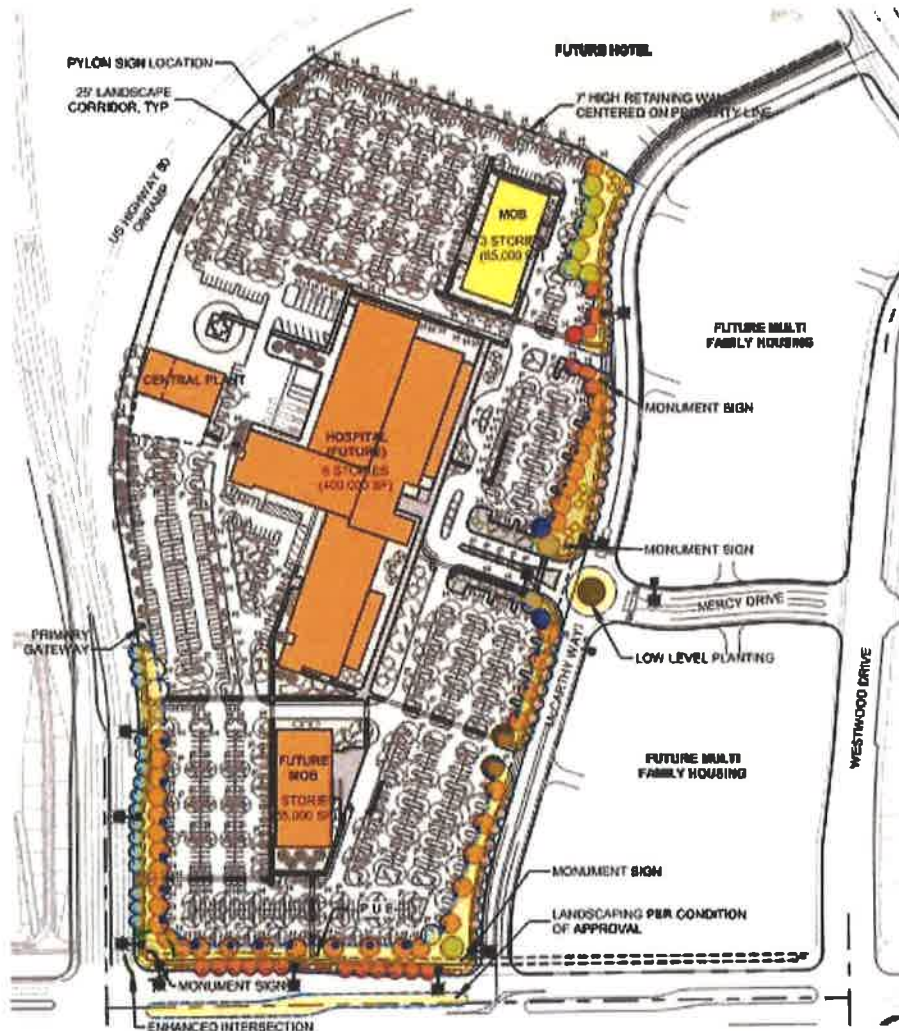
The submitted Planned Development Guidelines include Landscape Guidelines which are intended to provide recommendations relative to hardscape materials, irrigation systems, landscape plantings, and plant palettes. According to the applicant, landscape design plays an essential role in supporting Folsom Ranch Medical Center's overall site planning goals of safe and easy navigation, comfortable pedestrian circulation, and a positive user experience for patients, staff, and visitors. One of the unique landscape features proposed for the project includes a healing garden, which features fragrant plants, cool and calming colors, naturalistic massings and foliage with soft texture. In addition, the proposed landscape plan includes pollinator friendly plants that attract bees, hummingbirds, and butterflies and provide a habitat that will sustain healthy communities of beneficial insects and pollinators.

As shown on the landscape plans (Attachment 9), the applicant is proposing to install landscaping that features California-native and low water-use trees, shrubs, and groundcover selections intended to comply with the requirements of the Model Water Efficiency Landscape Ordinance (MWELO). Proposed landscape improvements include a variety of drought-tolerant trees, shrubs, and groundcover. Among the proposed trees are; Blue Oak, Calabrian Pine, Chinese Pistache, Golden Rain Tree, Interior Live Oak, Pink Crape Myrtle, Purple Leaf Plum, Strawberry Tree, and Valley Oak. Proposed shrubs and groundcover include; Alpine Strawberry, Berkeley Sedge, California Coffeeberry, Coast Rosemary, Creeping Fig, Deer Grass, Fortnight Lily, New Zealand Flax, Rockrose, and Star Jasmine. The preliminary landscape plan meets the City shade requirement (50%) by providing 50% shade in the parking lot area within fifteen (15) years. Staff recommends that the final landscape plans be reviewed and approved by the Community Development Department. Condition No. 36 is included to reflect this requirement.

To ensure a proper level of maintenance for the frontage landscaping located along East Bidwell Street, Alder Creek Parkway, and McCarthy Way, the applicant is seeking to enter into an agreement with the City in which the City agrees to maintain these frontage landscape improvements subject to receipt of funding through a mutually agreement financing mechanism. As part of this agreement, the applicant is also requesting that the City maintain the streetlights located along the frontage of McCarthy Way and East Bidwell Street. The exhibit on the following page shows the specific location of the frontage landscape and lighting improvements to be maintained by the City.

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FIGURE 20: FRONTAGE LANDSCAPING AND LIGHTING EXHIBIT



M. Oak Tree Impacts

As discussed previously within this report, the development of the proposed project includes a number of off-site improvements including construction of a hydromodification basin (Basin No. 8) which is located west of East Bidwell Street (in the location as approved in the FPA Storm Drainage Master Plan). A 1.28-acre area of Blue Oak Woodland would be impacted by construction of the hydromodification basin. The impacts to Oak Woodland in general, and to this particular Blue Oak Woodland area, were previously analyzed as part of the FPASP EIR/EIS, the Westland Eagle SPA Addendum to the FPASP EIR/EIS, and the FPASP Backbone Infrastructure Mitigated Negative Declaration. The proposed project is subject to all mitigation measures identified by the prior environmental documents to address impacts to the Blue Oak Woodland area

caused by construction of hydromodification basin.

An Arborist Report and Oak Tree Mitigation Strategy was prepared for the proposed project by ECORP Consulting on April 28, 2021 (Attachment 21) in order to identify, map, and assess the condition of the oak trees located in the 1.28-acre Blue Oak Woodland area where the proposed hydromodification basin will be constructed. The Arborist Report identified 17 Blue Oaks Trees within the Blue Oak Woodland area, with 11 of the trees being in Fair Condition, five trees being in Good Condition, and one tree being in Poor Condition. The Arborist Report concluded that all 17 Oak Trees will need to be removed due to direct and indirect impacts (grading, trenching paving, soil disturbance, and pruning) associated with construction of the hydromodification basin.

As required by the FPASP EIR/EIS (Mitigation Measure 3A.3-5), the applicant is required to submit an Oak Tree Mitigation Plan consistent with the approved Oak Tree Mitigation and Monitoring Plan for the FPASP to mitigate for impacts to the 1.28-acre Blue Oak Woodland area located in the vicinity of the off-site hydromodification basin. Potential oak tree mitigation options include preserving off-site woodland habitat, use of the in-lieu fee program, and on-site oak tree plantings. Condition No. 51 is included to reflect this requirement.

N. Trails

The Folsom Plan Area Specific Plan (Figure 7.32) indicates that a future Class I Bicycle Path is planned along a portion of the project site in the northwest corner of the property adjacent to the U.S. Highway 50 eastbound onramp. When completed, the Class I Bicycle Path is expected to run from Prairie City Road eastward to the El Dorado County line. The owner/applicant is responsible for site preparation for the future Bicycle Path, while the City is responsible for the construction and maintenance of the actual Bicycle Path. The owner/applicant will be responsible for rough grading and installation of the necessary retaining wall to accommodate the Class I Bicycle Path on the northern portion of the subject property as shown in Figure 7.32 of the Specific Plan prior to issuance of a building permit on the second expansion of the hospital (Phase 4/Anticipated 2034). In the proposed Amendment to the Development Agreement, the City agrees that the owner/applicant shall be relieved of the aforementioned obligation should the City amend the Specific Plan to re-locate the Class 1 Bicycle Path, or if the City has not identified funding for construction of the Class 1 Bicycle Path across the Property and the connection to East Bidwell Street at the time of issuance of a building permit on the second expansion of the hospital for Phase 4. "Identified funding" means either: (1) the Class 1 Bicycle Path is incorporated into a subsequent project to widen the East Bidwell Overcrossing structure of US Highway 50, or (2) the submission or application for federal, state or other grants which, together with the City's available matching funds, would be sufficient to construct the Class 1 Bicycle Path across the Property and the connection to East Bidwell Street. Condition No. 38 is included to reflect this requirement.

O. Grading and Drainage

Development of the Folsom Ranch Medical Center project site necessitates the mass grading of the larger FPASP Parcel 85A and construction of on-site and off-site roadway and utility infrastructure. Existing topography on the overall site ranges from approximately 470 feet to 400 feet in elevation and generally falls in the southwest direction. The overall site will be mass graded to provide developable areas and achieve earthwork balance. On-site retaining walls (approximately 2-6 feet height) are anticipated to maintain maximum developable areas and intended road grades. Excavation at a borrow site located approximately 400 feet west of East Bidwell Street will provide fill material to widen the west side of East Bidwell Street. Fill material will be moved from the borrow site to the fill location by way of ground-disturbing equipment.

At build-out, surface runoff will generally flow to the southwest where it will be conveyed by off-site storm drain infrastructure to an off-site hydromodification basin (Basin No. 8) located west of East Bidwell Street in the location as approved in the FPA Storm Drainage Master Plan (Exhibit M, page 47) and FPA Specific Plan (Figure 12.4 Stormwater Plan, page 248). The first phase of the basin will be constructed with the development of the Folsom Ranch Medical Center, including the basin outlet control structure and spillway, a paved access road along the Savannah Parkway alignment and perimeter of the basin, and a storm drain outfall swale from the western terminus of Alder Creek Parkway to the existing downstream waterway.

P. Frontage Improvements

Existing improvements to Alder Creek Parkway include underground utilities, two travel lanes, a westbound turn-pocket (Enclave at Folsom Ranch Subdivision), an eastbound left-turn pocket (future McCarthy Way), a westbound left-turn lane at East Bidwell Street, bicycle lanes, and a raised median for landscaping. Existing improvements to East Bidwell Street include underground utilities, two travel lanes, a sidewalk, bicycle lanes, and a southbound left-turn lane at Alder Creek Parkway. Backbone improvements associated with the proposed project include widening of East Bidwell Street and providing a northbound right-turn pocket on the east side of East Bidwell Street. The owner/applicant will be required to install curbs, gutters, landscaping, and streetlights along the street frontages of Alder Creek Parkway and East Bidwell Street. (Condition No. 18). The recommended conditions of approval require the applicant to submit detailed plans for all curbs, gutters, bicycle lanes, sidewalks, landscaping, and streetlights prior to construction to ensure compliance with the Folsom Ranch Central District Design Guidelines, the City of Folsom Standard Construction Specifications and Details, and the City of Folsom Design and Procedures Manual and Improvement Standards.

To facilitate development of the proposed Medical Center, the owner/applicant will be required to construct the McCarthy Way roadway from Alder Creek Parkway north to Placerville Road, the Mercy Drive roadway from McCarthy Way east to Westwood Drive, and the Westwood Drive Roadway from Alder Creek Parkway to Placerville Road. In

addition, the owner/applicant will be required to install curbs, gutters, bicycle lanes sidewalks, landscaping (only McCarthy Way), and streetlights along the street frontages of McCarthy Way, Mercy Drive, and Westwood Drive (Condition No. 18). The recommended conditions of approval require the applicant to submit detailed plans for all curbs, gutters, bicycle lanes, sidewalks, landscaping, and streetlights prior to construction to ensure compliance with the Folsom Ranch Central District Design Guidelines, the City of Folsom Standard Construction Specifications and Details, and the City of Folsom Design and Procedures Manual and Improvement Standards.

Q. Water Supply and Infrastructure

Water supply services would be provided to the Medical Center project by the City of Folsom under the Folsom Plan Area Water Supply Agreement. The Folsom Plan Area Water Supply Agreement covers an estimated water demand of 5,600 acre-feet per year, consistent with the water demand analyzed in the FPASP EIR/EIS. The water demand associated with the proposed project is estimated to be 156 acre-feet per year, a 126 acre-feet per year increase above previously considered demand for Parcel 85A. The project would result in a total water demand of 5,485 acre-feet-per-year for the entire FPASP. In addition, the proposed project would include two on-site underground water storage tanks. On-site storage tanks are required to meet California Plumbing Code requirements for emergency potable water supply to support 72 hours of continuing operation in the event of an emergency.

The Folsom Plan Area Water System Master Plan (Brown and Caldwell 2014) identifies five water pressure zones in the FPASP area. The proposed project is currently located within the Zone 3 water service area. Based on a hydraulic analysis (Appendix I to the Addendum/Attachment 22) that was prepared for the proposed Medical Center project, it was determined that the project would need to connect to the Zone 4 water service area in order to achieve the required minimum water pressure needed for the hospital building (80 psi). To accommodate shifting the proposed project into the Zone 4 water service area, additional Zone 4 water piping will be required to be installed and the Zone 4 water tank capacity will need to be expanded from 2 million gallons (MG) to 2.6 MG. The owner/applicant shall pay all costs and expenses for piping, tank size expansion from 2.0 MG to 2.6 MG, and construction to connect from Zone 4 to the Medical Center. Condition No. 34 is included to reflect this requirement.

On a temporary basis, until the completion of the Zone 4 infrastructure, the water infrastructure for the Medical Center will be provided through Zone 5. The owner/applicant will be responsible for all costs and expenses to connect water from Zone 5 to the Medical Center. Unless improvements or connections to Zone 4 are solely attributable to the proposed Medical Center, the owner/applicant will only be required to provide a fair-share contribution towards the construction of the Zone 4 infrastructure and may elect to participate in any special assessment/special tax funding mechanisms that are established by the City. The owner/applicant will also be required to coordinate with developers of other projects that also use Zone 4 infrastructure to determine the amounts

of their respective fair share contribution toward Zone 4 infrastructure. Condition No. 34 is included to reflect these requirements.

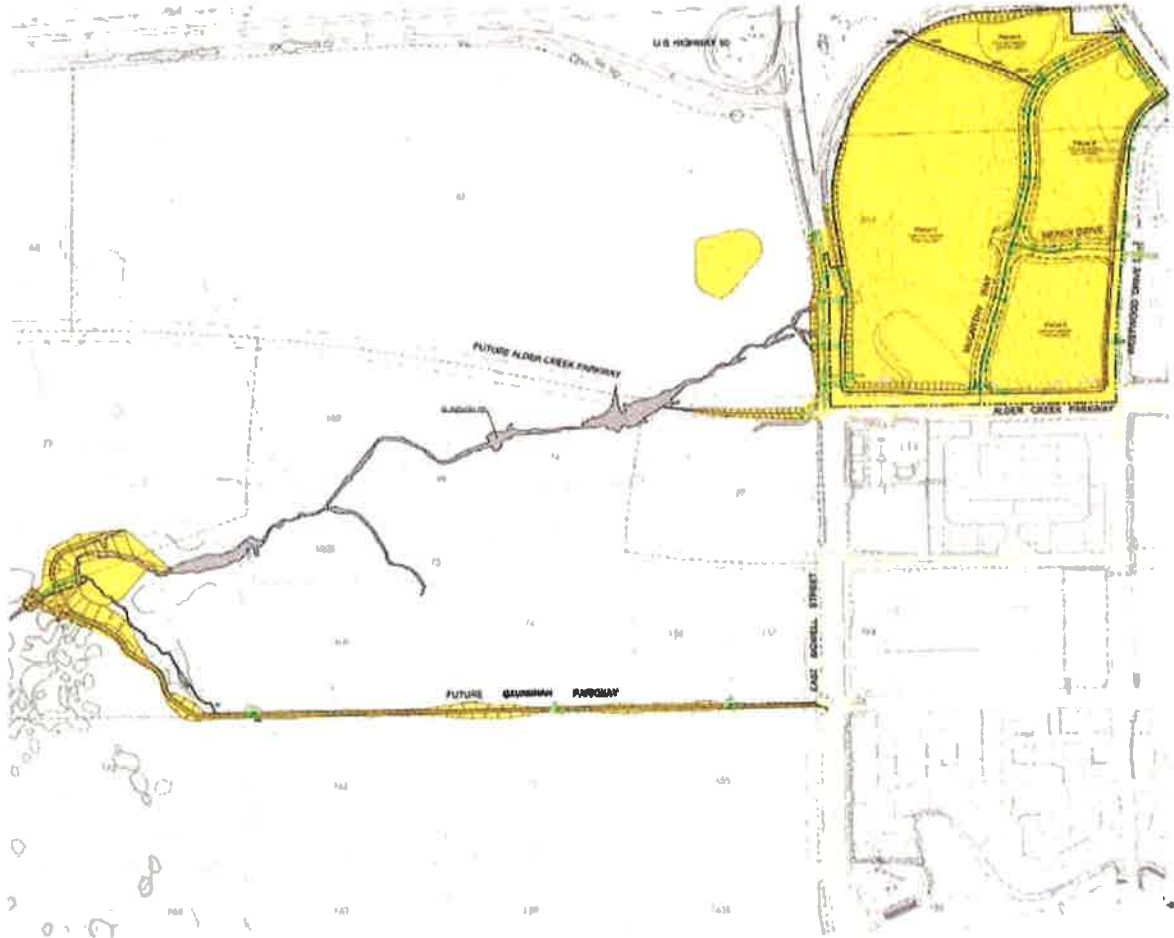
R. Off-Site Improvements

The proposed project also includes a number of significant off-site improvements (Attachment 12) including roadway and utility improvements, grading improvements on three adjacent parcels, construction of a hydromodification basin, construction of a storm drain swale, and excavation of a borrow site. The following is a detailed description of each of the proposed off-site improvements:

- Mass grading of Parcel 85A (includes subject parcel) and adjacent roadways resulting in contour graded developable parcels and subgrade along roadway alignments.
- Rough grading of a portion of Hydromodification Basin No. 8 including the access road along the Savannah Parkway alignment and perimeter of the basin.
- Rough grading of a storm drain outfall swale from the western terminus of Alder Creek Parkway to the existing downstream waterway.
- Excavation at a borrow site west of East Bidwell Street.
- Roadway and Utility improvements along East Bidwell, Alder Creek Parkway, Westwood Drive, Placerville Road, McCarthy Drive and Mercy Way including storm drain, sanitary sewer, potable and non-potable water, and dry utility infrastructure.
- Hydromodification Basin No. 8 improvements including a paved access road to East Bidwell Street and basin outlet control structure and spillway.

The extent of the proposed off-site improvements is shown in Figure 21 on the following page.

FIGURE 21: PRELIMINARY OFF-SITE GRADING AND DRAINAGE PLAN



S. Conformance with Relevant Folsom General Plan and Folsom Plan Area Specific Plan Objectives and Policies

The recently approved City of Folsom 2035 General Plan outlines a number of goals, policies, and implementation programs designed to guide the physical, economic, and environmental growth of the City. In addition, the Folsom Plan Area Specific Plan includes goals and policies intended to ensure successful development within the Folsom Plan Area. Staff has determined that the proposed project is consistent with both the General Plan and Specific Plan goals and policies. The following is a summary analysis of the project's consistency with the Folsom General Plan and with key policies of the Folsom Plan Area Specific Plan.

APPLICABLE GENERAL PLAN GOALS AND POLICIES

GP GOAL LU 1.1 (Land Use/Growth and Change)

Retain and enhance Folsom's quality of life, unique identity, and sense of community while continuing to grow and change.

GP POLICY LU 1.1.12-1 (Infill Development)

Respect the local context: New development should improve the character and connectivity of the neighborhood in which it occurs. Physical design should respond to the scale and features of the surrounding community, while improving critical elements such as transparency and permeability.

Analysis: The proposed project is consistent with this policy in that the project features significant site and design improvements which will enhance the overall character of the area. In addition, the proposed project includes Planned Development Guidelines that ensure that the design of the Medical Center buildings and associated improvements will be complimentary to the existing and future residential and commercial development in the vicinity.

GP POLICY LU 1.1.15 (SACOG Blueprint Principles)

Strive to adhere to the Sacramento Regional Blueprint Growth Principles.

Analysis: The proposed project is consistent with this policy in that the project has been designed to adhere to the primary SACOG Blueprint Principles including Compact Development, Use of Existing Assets, and Quality Design. Compact Development involves creating environments that are more compactly built and use space in an efficient but attractive manner and helps to encourage more walking, biking, and transit use and shorter auto trips. Use of Existing Assets entails intensification of the existing use or redevelopment in order to make better use of existing public infrastructure, including roads. Quality Design focuses on the design details of any land development (such as relationship to the street, placement of buildings, sidewalks, street widths, landscaping, etc.), which are all factors that influence the attractiveness of living in a compact development and facilitate the ease of walking within and in and out of a community.

GP GOAL M 4.1 (Vehicle Traffic and Parking)

Ensure a safe and efficient network of streets for car and trucks, as well as provide an adequate supply of vehicle parking.

GP POLICY M 4.1.3 (Level of Service)

Strive to achieve a least traffic Level of Service "D" (or better) for local streets and roadways throughout the City. In designing transportation improvements, the City will prioritize use of smart technologies and innovative solutions that maximize efficiencies and safety while minimizing the physical footprint. During the course of Plan buildout, it may occur that temporarily higher Levels of Service result where roadway improvements have not been adequately phased as development proceeds. However, this situation will be minimized based on annual traffic studies and monitoring programs. Staff will report to the City Council at regular intervals via the Capital improvement Program process for the Council to prioritize project integral to achieving Level of Service D or better.

Analysis: The proposed project is consistent with this policy in that the project will not result in a change in the level of service (LOS) at any of the 19 study intersections with programmed roadway infrastructure improvements. In addition, the proposed project would not result in any substantial increase in Vehicle Miles Traveled (VMT) with implementation of mitigation measures identified in the Folsom Plan Area Specific Plan EIR/EIS.

GP GOAL M 4.2 (Vehicle Traffic and Parking)

Provide and manage a balanced approach to parking that meets economic development and sustainability goals.

GP POLICY M 4.2.4 (Electric Vehicle Charging Stations)

Encourage the installation of electric vehicle charging stations in parking spaces throughout the city, prioritizing installations at multi-family residential units.

Analysis: The proposed project is consistent with this policy in that the project includes 100 electric vehicle charging spaces/stations positioned at four strategic locations throughout the project site to serve the two medical office buildings and the hospital building. The number of proposed electric vehicle charging spaces/stations (100) is consistent with the California Green Buildings Standards Code's provisions for multi-family residential development.

GP GOAL EP 2.1 (Employment Development)

Support efforts to increase employment in Folsom by encouraging the retention, attraction, and expansion of private sector business.

GP POLICY EP 2.1.1 (New Employment)

Encourage professional research and development, industrial, and office employers to located in Folsom to provide more job opportunities for Folsom residents.

Analysis: The proposed project is consistent with this policy in that the project is expected to employ approximately 2,662 medical and support services employees at full build-out of the Medical Center. In addition, the proposed project will service approximately 2,631 visitors on a daily basis.

GP GOAL LU 8.1 (Land Use/Employment)

Encourage, facilitate, and support the location of office, creative industry, technology, and industrial uses and retention of existing industry in appropriate locations.

GP POLICY LU 8.1.5 (Transit)

Encourage new employment uses to locate where they can be easily served by public transit. Transit centers should be incorporated into the project, where appropriate.

Analysis: The proposed project is consistent with this policy in that the project is located on a major transit corridor along Alder Creek Parkway as identified by the Folsom Plan Area Specific Plan (Figure 7.29). The transit corridor includes a 38-foot-wide planted median that eventually as transit demand increases, will be reduced to 16-feet to allow for construction of two additional travel lanes for either dedicated or mixed-flow regional "Hi Bus" transit service (high frequency bus service). In addition, it anticipated that Bus Rapid Transit (BRT) stops will eventually be placed along eastbound and westbound Alder Creek Parkway to serve the proposed Medical Center project.

GP GOAL LU 9.1 (Land Use/Community Design)

Encourage community design that results in a distinctive, high-quality built environment with a character that creates memorable places and enriches the quality of life of Folsom's residents.

GP POLICY LU 9.1.10 (Renewable and Alternative Energy Generation Systems)

Require the use of solar, wind, and other on-site renewable energy generation systems as part of the design of new planned developments.

Analysis: The proposed project is consistent with this policy in that the project includes the construction of a solar panel array for the covered employee parking area located on the west of the hospital building. In addition, the proposed project will be implementing energy efficiencies through the design of the medical office and buildings in compliance with the California Building Code and the California Green Code. The proposed project also includes the use of cool paving materials at various locations throughout the project site.

Conformance with Relevant Specific Plan Goals, Objectives, and Policies

The Folsom Plan Area Specific Plan identifies a number of goals, objectives, and policies designed to guide the physical, economic, and environmental growth of the Specific Plan

Area. Staff has determined that the proposed project is consistent with the Specific Plan goals, objectives, and policies as outlined and discussed below:

SP OBJECTIVE 7.1 (Circulation)

Consistent with the California Complete Streets Act of 2008 and the Sustainable Communities and Climate Protection Act (SB 375), create a safe and efficient circulation system for all modes of travel.

SP POLICY 7.1

The roadway network in the Plan Area shall be organized in a grid-like pattern of streets and blocks, except where topography and natural features make it infeasible, for the majority of the Plan Area in order to create neighborhoods that encourage walking, biking, public transit, and other alternative modes of transportation.

Analysis: Consistent with the requirements of the California Complete Streets Act, the FPASP identified and planned for hierarchy of connect “complete streets” to ensure that pedestrian, bike, bus, and automobile modes of travel are designed to have direct and continuous connections throughout the Plan Area. Every option, from regional connector roadways to arterial and local streets, has been carefully planned and designed. Recent California legislation to reduce greenhouse gas emissions (AB 32 and SB 375) has resulted in an increased market demand for public transit and housing located closer to service needs and employment centers. In response to these changes, the FPASP includes a regional transit corridor that will provide public transportation links between the major commercial, public, and multi-family residential land uses in the Plan Area.

The Folsom Ranch Medical Center project has been designed with multiple modes of transportation options (vehicles, bicycle, walking, access to transit) and internal drive aisles organized in a pattern consistent with the approved FPASP circulation plan. In particular, the Medical Center is located adjacent to Alder Creek Parkway, where a future Bus Rapid Transit (BRT) corridor is planned.

ENVIRONMENTAL REVIEW

The City, as the lead agency under the California Environmental Quality Act (CEQA), has determined that, in accordance with Section 15164 of the State CEQA Guidelines, the proposed project constitutes minor changes to the development scenario described in the Final EIR/EIS for the Folsom Plan Area Specific Plan, warranting the preparation of an Addendum. An Addendum is appropriate where a previously certified EIR has been prepared and some changes or revisions to the project are proposed, or the circumstances surrounding the project have changed, but none of the changes or revisions would result in significant new or substantially more severe environmental impacts, consistent with CEQA (Public Resources Code) Section 21166 and State CEQA Guidelines Sections 15162 and 15163.

An Environmental Checklist and Addendum was prepared in accordance with CEQA Guidelines Section 15164 to evaluate whether the proposed project's effects were adequately examined in the FPASP EIR/EIS. The Environmental Checklist and Addendum concluded that no changes associated with the proposed project and no changed circumstances trigger subsequent or supplemental environmental review. The Environmental Checklist and Addendum are included at Attachment 22 to this staff report. In addition, the Mitigation Monitoring and Reporting Program are included as Attachment 23 to this staff report.

RECOMMENDATION/PLANNING COMMISSION ACTION

Staff recommends approval of the proposed project, subject to the Findings and Conditions of Approval attached to this report.

Move to:

- Adopt an Addendum to the Final Environmental Impact Report for the Folsom Plan Area Specific Plan prepared for the Folsom Ranch Medical Center project (PN 20-193) per Attachment 22; and
- Approve a Planned Development Permit which contains detailed development and architectural standards for the proposed 530,000-square-foot Medical Center; and
- Approve a Conditional Use Permit for the development and operation of a private-use hospital heliport facility at the Medical Center; and

Move to recommend that the City Council:

- Approve Amendment No. 2 to the First Amended and Restated Tier 1 Development Agreement Relative to the Folsom South Specific Plan for the Folsom Ranch Medical Center project.

These approvals are subject to the proposed findings below (Findings A-W) and the recommended conditions of approval (Conditions 1-51) attached to this report.

GENERAL FINDINGS

- A. NOTICE OF HEARING HAS BEEN GIVEN AT THE TIME AND IN THE MANNER REQUIRED BY STATE LAW AND CITY CODE.
- B. THE PROJECT IS CONSISTENT WITH THE GENERAL PLAN AS AMENDED, THE FOLSOM PLAN AREA SPECIFIC PLAN AS AMENDED, AND THE FOLSOM RANCH CENTRAL DISTRICT DESIGN GUIDELINES.

CEQA FINDINGS

- C. THE CITY, AS LEAD AGENCY, PREVIOUSLY CERTIFIED AN ENVIRONMENTAL IMPACT REPORT/ENVIRONMENTAL IMPACT STATEMENT FOR THE FOLSOM PLAN AREA SPECIFIC PLAN.
- D. AN ADDENDUM TO THE FOLSOM PLAN AREA SPECIFIC PLAN FINAL ENVIRONMENTAL IMPACT REPORT/ENVIRONMENTAL IMPACT STATEMENT WAS CERTIFIED BY THE CITY IN 2015 FOR THE WESTLAND EAGLE SPECIFIC PLAN AMENDMENT PROJECT IN ACCORDANCE WITH CEQA.
- E. THE CITY HAS DETERMINED THAT THE FOLSOM RANCH MEDICAL CENTER PROJECT IS CONSISTENT WITH THE FOLSOM PLAN AREA SPECIFIC PLAN AS AMENDED BY THE WESTLAND EAGLE SPECIFIC PLAN AMENDMENT.
- F. THE CITY HAS DETERMINED THAT NONE OF THE CIRCUMSTANCES DESCRIBED IN PUBLIC RESOURCES CODE SECTION 21166 OR CEQA GUIDELINES SECTION 15162 GENERALLY REQUIRING THE PREPARATION OF A SUBSEQUENT EIR EXIST IN THIS CASE.
- G. THE CITY HAS PREPARED AN ADDENDUM TO THE FINAL ENVIRONMENTAL IMPACT REPORT FOR THE FOLSOM PLAN AREA SPECIFIC PLAN AND HAS DETERMINED THAT NONE OF THE CHANGES OR REVISIONS PROPOSED BY THE PROJECT WOULD RESULT IN SIGNIFICANT NEW OR SUBSTANTIALLY MORE SEVERE ENVIRONMENTAL IMPACTS AND DOES NOT REQUIRE ANY MITIGATION MEASURES IN ADDITION TO THOSE IN THE FINAL ENVIRONMENTAL IMPACT REPORT AND THE ADDENDUM FOR THE WESTLAND EAGLE SPECIFIC PLAN AMENDMENT PROJECT.
- H. THE CITY HAS DETERMINED THAT THE IMPACTS OF THE FOLSOM RANCH MEDICAL CENTER PROJECT ARE ADEQUATELY ADDRESSED BY THE FINAL ENVIRONMENTAL IMPACT REPORT FOR THE FOLSOM PLAN AREA SPECIFIC PLAN, THE ADDENDUM FOR THE WESTLAND EAGLE SPECIFIC PLAN AMENDMENT PROJECT, AND THE ADDENDUM FOR THE FOLSOM RANCH MEDICAL CENTER PROJECT.
- I. THE PLANNING COMMISSION HAS CONSIDERED THE ADDENDUM WITH THE FINAL EIR BEFORE MAKING A DECISION ON THE PROJECT.

PLANNED DEVELOPMENT PERMIT FINDINGS

- J. THE PROPOSED PROJECT COMPLIES WITH THE INTENT AND PURPOSES OF THE FOLSOM PLAN AREA SPECIFIC PLAN AS AMENDED AND OTHER APPLICABLE ORDINANCES OF THE CITY AND THE GENERAL PLAN AS AMENDED.
- K. THE PROPOSED PROJECT IS GENERALLY CONSISTENT WITH THE OBJECTIVES, POLICIES AND REQUIREMENTS OF THE DEVELOPMENT STANDARDS OF THE CITY. THE MODIFICATION TO THOSE STANDARDS PROPOSED AS PART OF THIS PROJECT WILL RESULT IN A DEVELOPMENT THAT IS SUPERIOR TO THAT OBTAINED BY THE RIGID APPLICATION OF THE STANDARDS.
- L. THE PHYSICAL, FUNCTIONAL AND VISUAL COMPATIBILITY BETWEEN THE PROPOSED PROJECT AND EXISTING AND FUTURE ADJACENT USES AND AREA CHARACTERISTICS IS ACCEPTABLE.
- M. AS CONDITIONED, THE PROJECT WILL MAKE AVAILABLE NECESSARY PUBLIC FACILITIES, INCLUDING BUT NOT LIMITED TO, WATER, SEWER AND DRAINAGE, AND THE PROJECT WILL ADEQUATELY PROVIDE FOR THE FURNISHING OF SUCH FACILITIES.
- N. THE PROPOSED PROJECT WILL NOT CAUSE ADVERSE ENVIRONMENTAL IMPACTS WHICH HAVE NOT BEEN MITIGATED TO AN ACCEPTABLE LEVEL.
- O. THE PROPOSED PROJECT WILL NOT CAUSE UNACCEPTABLE VEHICULAR TRAFFIC LEVELS ON SURROUNDING ROADWAYS, AND THE PROPOSED PROJECT WILL PROVIDE ADEQUATE INTERNAL CIRCULATION, INCLUDING INGRESS AND EGRESS.
- P. THE PROPOSED PROJECT WILL NOT BE DETRIMENTAL TO THE HEALTH, SAFETY AND GENERAL WELFARE OF THE PERSONS OR PROPERTY WITHIN THE VICINITY OF THE PROJECT SITE, AND THE CITY AS A WHOLE.
- Q. ADEQUATE PROVISION IS MADE FOR THE FURNISHING OF SANITATION SERVICES AND EMERGENCY PUBLIC SAFETY SERVICES TO THE DEVELOPMENT.

CONDITIONAL USE PERMIT FINDING

- R. AS CONDITIONED, THE ESTABLISHMENT, MAINTENANCE OR OPERATION OF THE USE APPLIED FOR WILL NOT, UNDER THE CIRCUMSTANCES OF THIS PARTICULAR CASE, BE DETRIMENTAL TO THE HEALTH, SAFETY, PEACE, MORALS, COMFORT, AND GENERAL WELFARE OF PERSONS RESIDING OR WORKING IN THE NEIGHBORHOOD, OR BE DETRIMENTAL OR INJURIOUS TO PROPERTY AND IMPROVEMENTS IN THE NEIGHBORHOOD OR TO THE GENERAL WELFARE OF THE CITY.

DEVELOPMENT AGREEMENT AMENDMENT FINDINGS

- S. THE PROPOSED AMENDMENT NO. 2 TO THE FIRST AMENDED AND RESTATED TIER 1 DEVELOPMENT AGREEMENT IS CONSISTENT WITH THE OBJECTIVES, POLICIES, GENERAL LAND USES AND PROGRAMS SPECIFIED IN THE CITY GENERAL PLAN (AS AMENDED) AND THE FOLSOM PLAN AREA SPECIFIC PLAN (AS AMENDED).
- T. THE PROPOSED AMENDMENT NO. 2 TO THE FIRST AMENDED AND RESTATED TIER 1 DEVELOPMENT AGREEMENT IS IN CONFORMITY WITH PUBLIC CONVENIENCE, GENERAL WELFARE, AND GOOD LAND USE PRACTICES.
- U. THE PROPOSED AMENDMENT NO. 2 TO THE FIRST AMENDED AND RESTATED TIER 1 DEVELOPMENT AGREEMENT WILL NOT BE DETRIMENTAL TO THE HEALTH, SAFETY, AND GENERAL WELFARE OF PERSONS RESIDING IN THE IMMEDIATE AREA, NOR BE DETRIMENTAL OR INJURIOUS TO PROPERTY OR PERSONS IN THE GENERAL NEIGHBORHOOD OR TO THE GENERAL WELFARE OF THE RESIDENTS OF THE CITY AS A WHOLE.
- V. THE PROPOSED AMENDMENT NO. 2 TO THE FIRST AMENDED AND RESTATED TIER 1 DEVELOPMENT AGREEMENT WILL NOT ADVERSELY AFFECT THE ORDERLY DEVELOPMENT OF PROPERTY OR THE PRESERVATION OF PROPERTY VALUES.
- W. THE PROPOSED AMENDMENT NO. 2 TO THE FIRST AMENDED AND RESTATED TIER DEVELOPMENT AGREEMENT IS CONSISTENT WITH THE PROVISIONS OF GOVERNMENT CODE SECTIONS 65864 THROUGH 65869.5.

Attachment 4

Conditions of Approval

CONDITIONS OF APPROVAL FOR THE FOLSOM RANCH MEDICAL CENTER PROJECT (PN 20-193) NORTHEAST CORNER OF THE INTERSECTION OF EAST BIDWELL STREET AND ALDER CREEK PARKWAY PLANNED DEVELOPMENT PERMIT, CONDITIONAL USE PERMIT, AND DEVELOPMENT AGREEMENT AMENDMENT				
Condition No.	Mitigation Measure	Condition of Approval	When Required	Responsible Department
1.		<p>The owner/applicant shall submit final site development plans to the Community Development Department that shall substantially conform to the exhibits referenced below:</p> <ol style="list-style-type: none"> 1. Preliminary Site Plan, dated October 30, 2020 2. Preliminary Utility Plan, dated March 5, 2021 3. Preliminary Grading and Drainage Plan, dated March 5, 2021 4. Preliminary Landscape Plans, dated March 5, 2021 5. Preliminary Access and Circulation Plan, dated March 16, 2021 6. Preliminary Off-Site Improvement Plans, dated March 5, 2021 7. Planned Development Guidelines, dated February 3, 2021 8. Folsom Ranch Medical Center Booklet (Separate Bound Document) 9. Development Agreement Amendment 10. Transportation Analysis and CEQA Impact Study, dated April 23, 2021 11. Arborist Report, dated April 28, 2021 12. Environmental Checklist and Addendum for the Folsom Ranch Medical Center Project, dated April, 2021 13. Mitigation Monitoring and Reporting Program for the Folsom Ranch Medical Center Project, dated April, 2021 <p>The Planned Development Permit and Conditional Use Permit are approved for the development and operation of a 530,000 Medical Center project (Folsom Ranch Medical Center). Implementation of the project shall be consistent with the above referenced items and these conditions of approval. Square footage is determined based upon occupied square footage as detailed in Planned Development Guidelines and Development Agreement Amendment.</p>	G, I, B	CD (P)(E)

Planning Commission
 Folsom Ranch Medical Center
 May 19, 2021

2.		Building plans, and all civil engineering, improvement, landscape and irrigation plans, shall be submitted to the Community Development Department for review and approval to ensure conformance with this approval and with relevant codes, policies, standards and other requirements of the City of Folsom.	G, I, B	CD (P)(E)(B)
3.		The project approvals granted under this staff report (Planned Development Permit and Conditional Use Permit) shall remain in effect for the term of the Development Agreement Amendment (June 30, 2056 or until the 530,000 square foot Medical Center has been built out, whichever is later). Provided that the relevant building (or other) permits for the heliport are obtained within this time period, the Conditional Use Permit will thereafter run with the land (i.e., be valid in perpetuity), subject to the revocation process in Condition 8. Failure to obtain the relevant building (or other) permits within this time period, without the subsequent extension of this approval, shall result in the termination of this approval. The Development Agreement Amendment, which is subject to review and approval by the City Council, is valid until June 30, 2056 or until 530,000 occupied square feet associated with Medical Center has been built out, whichever is later.	B	CD (P)

4.		<p>The owner/applicant shall protect, defend, indemnify, and hold harmless the City and its agents, officers and employees from any claim, action or proceeding against the City or its agents, officers or employees to attack, set aside, void, or annul any approval by the City or any of its agencies, departments, commissions, agents, officers, employees, or legislative body concerning the project, which claim, action or proceeding is brought within the time period provided therefore in Government Code Section 66499.37 or other applicable statutes of limitation. The City will promptly notify the owner/applicant of any such claim, action or proceeding, and will cooperate fully in the defense. If the City should fail to cooperate fully in the defense, the owner owner/applicant shall not thereafter be responsible to defend, indemnify and hold harmless the City or its agents, officers, and employees, pursuant to this condition. The City may, within its unlimited discretion, participate in the defense of any such claim, action or proceeding if both of the following occur:</p> <ul style="list-style-type: none"> • The City bears its own attorney’s fees and costs; and • The City defends the claim, action or proceeding in good faith <p>The owner/applicant shall not be required to pay or perform any settlement of such claim, action or proceeding unless the settlement is approved by the owner/applicant. The owner/applicant’s obligations under this condition shall apply regardless of whether a Final Map is ultimately recorded with respect to this project.</p>	OG	CD (P)(E)(B) PW, PR, FD, PD
5.		<p>The owner/applicant shall comply with all provisions of Amendments No. 1 and 2 to the First Amended and Restated Tier 1 Development Agreement (collectively “Development Agreement”) and any approved amendments thereafter by and between the City and the owner/applicant of the project.</p>	B	CD (E)

6.	✓	<p>The owner/applicant shall participate in a mitigation monitoring and reporting program pursuant to City Council Resolution No. 2634 and Public Resources Code 21081.6. The mitigation monitoring and reporting measures identified in the Folsom Plan Area Specific Plan FEIR/EIS, the South of 50 Backbone Infrastructure Project MND, the Westland/Eagle Specific Plan Amendment to the FPASP and Addendum to the FPASP EIR/EIS, the Folsom South of U.S. Highway 50 Specific Plan Project Revised Proposed Off-Site Water Facility Alternative Amendment to the FPASP and Addendum to the FPASP EIR/EIS, and Folsom Ranch Medical Center Addendum to the Folsom Plan Area Specific Plan FEIR/EIS have been incorporated into these conditions of approval in order to mitigate or avoid significant effects on the environment. These mitigation monitoring and reporting measures are identified in the mitigation measure column. Applicant shall fund on a Time and Materials basis all mitigation monitoring (e.g., staff and consultant time).</p>	OG	CD (P)
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<p>7.</p>	<p>The City has amended the Inclusionary Housing Ordinance (i.e., Folsom Municipal Code Chapter 17.104) by Ordinance No. 1243, to eliminate Second Dwelling Units (also referred to as “granny flats”) as an alternative means of meeting the City’s inclusionary housing requirements. The City and the owner/applicant acknowledge that the Project is for medical uses and not residential housing. However, in light of the allowed uses under the Specific Plan, City has requested that owner/applicant acknowledge and owner/applicant hereby acknowledges that there is no vested right to use Second Dwelling Units as an alternative means for meeting the City’s inclusionary housing requirements and that this alternative shall not be available to Landowner from and after the date of Ordinance No. 1243. The owner/applicant further acknowledges that the State adopted amendments to Section 65850 of the California Government Code (specifically Section 65850(g)), effective January 1, 2018, to allow for the implementation of inclusionary housing requirements in residential rental units, upon adoption of an ordinance by the City. The owner/applicant is not currently contemplating any residential rental projects within the Property; however, in the event the City amends its Inclusionary Housing Ordinance with respect to rental housing pursuant to Section 65850(g), the owner/applicant (or a successor in interest) agrees that the Property shall be subject to said City Ordinance, as amended, should any residential rental project be proposed within the Property. Other than the elimination of the “granny flat” option and the possible future application of an inclusionary housing requirement on residential rental properties (upon the conditions stated herein), the City and the owner/applicant agree that all other alternatives for meeting the City’s inclusionary housing requirements remain vested to the full extent provided for in the Restated Agreement.</p>	<p>B</p>	<p>CD (P)</p>
<p>8.</p>	<p>If the Community Development Director finds evidence that conditions of approval for Folsom Ranch Medical Center heliport have not been fulfilled or that the use has resulted in a substantial adverse effect on the health, and/or general welfare of users of adjacent or proximate property, or has a substantial adverse impact on public facilities or services, the Director will refer the use permit to the Planning Commission for review. If, upon such review, and after a consideration of the public health benefits provided by the availability of a heliport at the Medical Center, the Planning District Commission finds that any of the above-stated results have occurred, the Commission may modify or revoke the Conditional Use Permit.</p>	<p>OG</p>	<p>CD (P)</p>

POLICE/SECURITY REQUIREMENT				
9.		<p>The owner/applicant shall consult with the Police Department in order to incorporate all reasonable crime prevention measures. The following security/safety measures shall be considered:</p> <ul style="list-style-type: none"> • A security guard on-duty at all times at the site or a six-foot security fence shall be constructed around the perimeter of construction areas. • Security measures for the safety of all construction equipment and unit appliances. • Landscaping shall not cover exterior doors or windows, block line-of-sight at intersections or screen overhead lighting. 	G, I, B	PD
DEVELOPMENT COSTS AND FEE REQUIREMENTS				
10.		The owner/applicant shall pay all applicable taxes, fees and charges for the project at the rate and amount required by the Public Facilities Financing Plan and Amendments No. 1 and No. 2 to the Amended and Restated Tier 1 Development Agreement.	B	CD (P)(E)
11.		If applicable, the owner/applicant shall pay off any existing assessments against the property, or file necessary segregation request and pay applicable fees.	B	CD (E)
12.		<p>The owner/applicant shall be subject to all Folsom Plan Area Specific Plan Area development impact fees established at the time of approval consistent with the Public Facilities Financing Plan (PFFP), Development Agreement and amendments thereto, unless exempt by previous agreement. The owner/applicant shall be subject to all applicable Folsom Plan Area plan-wide development impact fees in effect at the time of approval at the rates in effect when a building permit is issued. These fees may include, but are not limited to, the Folsom Plan Area Specific Plan Fee, Specific Plan Infrastructure Fee (SPIF), Solid Waste Fee, Corporation Yard Fee, Transportation Management Fee, Transit Fee, Highway 50 Interchange Fee, General Park Equipment Fee, Housing Trust Fee, etc.</p> <p>Any protest to such for all fees, dedications, reservations or other exactions imposed on this project will begin on the date of final approval, or otherwise shall be governed by the terms of Amendments No. 1 and 2 to the Development Agreement. The fees shall be calculated at the fee rate set forth in the PFFP and the Development Agreement.</p>	B	CD (P), PW, PK

13.		The City, at its sole discretion, may utilize the services of outside legal counsel to assist in the implementation of this project, including, but not limited to, drafting, reviewing and/or revising agreements and/or other documentation for the project. If the City utilizes the services of such outside legal counsel, the City shall provide notice to the owner/applicant of the outside counsel selected, the scope of work and hourly rates, and the owner/applicant shall reimburse the City for all outside legal fees and costs incurred and documented by the City for such services. The owner/applicant may be required, at the sole discretion of the City Attorney, to submit a deposit to the City for these services prior to initiation of the services. The owner/applicant shall be responsible for reimbursement to the City for the services regardless of whether a deposit is required.	OG	CD (P)(E)
14.		If the City utilizes the services of consultants to prepare special studies or provide specialized design review or inspection services for the project, the City shall provide notice to the owner/applicant of the outside consultant selected, the scope of work and hourly rates, and the owner/applicant shall reimburse the City for actual costs incurred and documented in utilizing these services, including administrative costs for City personnel. A deposit for these services shall be provided prior to initiating review of the grading plan, improvement plans, or beginning inspection, whichever is applicable.	G, I, B	CD (P)(E)
GRADING PERMIT REQUIREMENTS				
15.		The owner/applicant shall locate and remediate all antiquated mine shafts, drifts, open cuts, tunnels, and water conveyance or impoundment structures existing on the project site, with specific recommendations for the sealing, filling, or removal of each that meet all applicable health, safety and engineering standards. Recommendations shall be prepared by an appropriately licensed engineer or geologist. All remedial plans shall be reviewed and approved by the City prior to approval of grading plans.	G	CD (E)
16.		The owner/applicant shall obtain all required State and Federal permits and provide evidence that said permits have been obtained, or that the permit is not required, subject to staff review prior to approval of any grading or improvement plan.	G, I	CD (P)(E)
17.		The final location, design, height, materials, and colors of the retaining walls and screen walls shall be subject to review and approval by the Community Development Department to ensure consistency with the Folsom Ranch Medical Center Planned Development Guidelines.	G, I, B	CD (P)(E), FD

IMPROVEMENT PLAN REQUIREMENTS				
18.		The improvement plans for the required public and private improvements (on-site and off-site) necessary to serve the project including but not limited to street and frontage improvements on East Bidwell Street, Alder Creek Parkway, McCarthy Way, Mercy Drive, Westwood Drive, and Placerville Road shall be reviewed and approved by the Community Development Department prior to approval of a building permit for the project.	B	CD (E)
19.		Public and private improvements (on-site and off-site), including roadways, curbs, gutters, sidewalks, bicycle lanes and trails, streetlights, underground infrastructure and all other improvements shall be provided in accordance with the latest edition of the City of Folsom <i>Standard Construction Specifications and Details</i> and the <i>Design and Procedures Manual and Improvement Standards</i> .	I	CD (P)(E)
20.		The on-site water and sewer systems shall be privately owned and maintained. The fire protection system shall be separate from the domestic water system. The fire system shall be constructed to meet the National Fire Protection Association Standard 24. The domestic water and irrigation system shall be metered per City of Folsom <i>Standard Construction Specifications</i> .	I	CD (E)
21.		For any improvements constructed on private property that are not under the ownership or control of the owner/applicant, all rights-of-entry, and if necessary, a permanent easement shall be obtained and provided to the City. All rights of entry, construction easements, either permanent or temporary and other easements shall be obtained as set forth in the Development Agreement, which shall be fully executed by all affected parties and shall be recorded with the Sacramento County Recorder, where applicable, prior to approval of grading and/or improvement plans.	G, I	CD (E)

22.		<p>The owner/applicant of all project phases shall submit a lighting plan for the project to the Community Development Department. The lighting plan shall be consistent with the Folsom Ranch Medical Center Planned Development Guidelines including but not limited to:</p> <ul style="list-style-type: none"> • Shield or screen lighting fixtures to direct the light downward and prevent light spill on adjacent properties; • Place and shield or screen flood and area lighting needed for construction activities, nighttime sporting activities, and/or security so as not to disturb adjacent residential areas and passing motorists; • For public lighting in residential neighborhoods, prohibit the use of light fixtures that are of unusually high intensity or that blink or flash; • Use appropriate building materials (such as low-glare glass, low-glare building glaze or finish, neutral, earth toned colored paint and roofing materials), shielded or screened lighting, and appropriate signage in the office/commercial areas to prevent light and glare from adversely affecting motorists on nearby roadways; and • Design exterior on-site lighting as an integral part of the building and landscaping design in the Specific Plan Area. Lighting fixtures shall be architecturally consistent with the overall site design. Lights used on signage should be directed to light only the sign face with no off-site glare. 	I	CD (P)
23.		<p>The owner/applicant shall coordinate the planning, development and completion of this project with the various utility agencies (i.e., SMUD, PG&E, etc.). The owner/applicant shall provide the City with written confirmation of public utility service prior to approval of the on-site improvement plans.</p>	I	CD (P)(E)
24.		<p>The owner/applicant shall be responsible for replacing any, and all damaged or hazardous public sidewalk, curb, and gutter, and/or bicycle trail facilities along the site frontage and/or boundaries, including pre-existing conditions and construction damage, to the satisfaction of the Community Development Department.</p>	I, OG	CD (E)
25.		<p>All future utility lines lower than 69 KV that are to be built within the project shall be placed underground within and along the perimeter of the project at the developer's cost. The owner/applicant shall dedicate to SMUD all necessary underground easements for the electrical facilities that will be necessary to service development of the project.</p>	I	CD (E)

26.	The owner/applicant shall pay for, furnish, and install all infrastructure associated with the water meter fixed network system for any City-owned and maintained water meter for the project.	I	CD (E), EWR
27.	The owner/applicant shall provide sanitary sewer, water, and storm drainage improvements with corresponding easements, as necessary, in accordance with these studies and the latest edition of the City of Folsom <u>Standard Construction Specifications and Details</u> , and the <u>Design and Procedures Manual and Improvement Standards</u> . The storm drainage design shall provide for no net increase in run-off (applicable to watershed of Hyrdomodification Basin No. 8) under post-development conditions.	G, I	CD(E), EWR, PW
28.	The storm drain improvement plans shall provide for “Best Management Practices” that meet the requirements of the water quality standards of the City’s National Pollutant Discharge Elimination System Permit issued by the State Regional Water Quality Control Board. In addition to compliance with City ordinances, the owner/applicant shall prepare a Stormwater Pollution Prevention Plan (SWPPP) and implement Best Management Practices (BMPs) that comply with the General Construction Stormwater Permit from the Central Valley RWQCB, to reduce water quality effects during construction.	G, I	CD (E)
29.	During Construction, the owner/applicant shall be responsible for litter control and sweeping of all paved surfaces in accordance with City standards. All on-site storm drains shall be cleaned immediately before the official start of the rainy season (October 15).	OG	CD (E)
30.	The owner/applicant shall dedicate public utility easements for underground facilities on properties adjacent to the public streets. A minimum of twelve and one-half-foot (12.5’) wide Public Utility Easements for underground facilities (i.e., SMUD, Pacific Gas and Electric, cable television, telephone) shall be dedicated adjacent to all public rights-of-way. The owner/applicant shall dedicate additional width to accommodate extraordinary facilities as determined by the City. The width of the public utility easements adjacent to public right of way may be reduced with prior approval from public utility companies.	I	CD (E)
31.	The owner/applicant shall dedicate the additional required right-of-way necessary for the construction of an additional 250-foot right-turn lane for southbound McCarthy Way approaching Alder Creek Parkway and the full extension of the right-turn pocket on westbound Alder Creek Parkway from McCarthy Way to East Bidwell Street.	I	CD (E)

NOISE REQUIREMENTS				
32.		Compliance with Noise Control Ordinance and General Plan Noise Element shall be required. Hours of construction operation shall be limited from 7:00 a.m. to 6:00 p.m. on weekdays and 8:00 a.m. to 5:00 p.m. on Saturdays. No construction is permitted on Sundays or holidays. Construction equipment shall be muffled and shrouded to minimize noise levels.	I, B	CD (P)(B)

FIRE DEPARTMENT REQUIREMENTS				
33.		<p>The owner/applicant shall comply with the following Fire Department requirements:</p> <ul style="list-style-type: none"> • The medical office and hospital building(s) shall have illuminated addresses visible from the street(s) or drive(s) fronting the property. Size and location of address identification shall be reviewed and approved by the Fire Marshal. • Prior to the issuance of any improvement plans or building permits, the Community Development and Fire Departments shall review and approve all detailed design plans for accessibility of emergency fire equipment, fire hydrant flow location, and other construction features. • All fire protection devices shall be designed to be located on site: fire hydrants, fire department connections, post indicator valves, etc. cannot be used to serve the building. A water model analysis that proves the minimum fire flow will be required before any permits are issued. The fire sprinkler riser location shall be inside a Fire Control Room (5' X 7' minimum) with a full-sized 3'-0" door. This room can be a shared with other building utilities. The room shall only be accessible from the exterior. • All-weather emergency access roads and fire hydrants (tested and flushed) shall be provided before combustible material or vertical construction is allowed on site. All-weather access is defined as 6" of compacted AB from May 1 to September 30 and 2"AC over 6" AB from October 1 to April 30. 	G, I, B	CD (P), FD

ENVIRONMENTAL AND WATER RESOURCE DEPARTMENT REQUIREMENTS				
34.		<p>The City, through approval of the Subsequent Entitlements and consideration of a Technical Memorandum titled “PA Parcel 85A Zone Supplemental Analysis” dated April 28, 2021, provided in support of the Addendum, has determined that the water infrastructure for the Medical Center will be provided through “Zone 4,” as that Zone is identified in Figure 12.1 of the Specific Plan. As a result, the owner/applicant shall be required to pay all costs and expenses for piping, tank size expansion from 2.0 MG to 2.6 MG, and construction to connect water from Zone 4 to the Medical Center.</p> <p>On a temporary basis, until the completion of the Zone 4 infrastructure, the water infrastructure for the Medical Center will be provided through Zone 5, as that Zone is identified in Figure 12.1 of the Specific Plan. As a result, the owner/applicant shall bear all costs and expenses to connect water from Zone 5 to the Medical Center.</p> <p>Unless improvements or connections to Zone 4 are solely attributable to the hospital uses included in the Project, the owner/applicant shall only be required to provide a fair-share contribution towards the construction of the Zone 4 infrastructure and may elect to participate in any special assessment/special tax funding mechanisms that are established by the City. The owner/applicant shall coordinate with developers of other projects that also use Zone 4 infrastructure to determine the amounts of their respective fair share contribution toward Zone 4 infrastructure.</p>	I, G, B	CD (E), EWR

<p>35.</p>	<p>The owner/applicant shall comply with the following Environmental and Water Resource Department requirements:</p> <ul style="list-style-type: none"> • All water for the Folsom Ranch Medical Center (Medical Office Buildings, Hospital Building, Central Plant, etc.) shall be on the Zone 4 water line. • An on-site looped water system shall be required. • The water system shall be protected with USC Certified and approved RPPA and RPDA devices. • A sewer manhole or cleanout shall be placed at the property line/right-of-way line to distinguish private vs public ownership. • All on-site water and sewer shall be privately owned and maintained. • If there is a kitchen or food preparation area, some type of grease control device shall be required. • All proposed sewer within the right-of-way is to be 8-inch SDR-26 sewer pipe. • Additional in-line valves are required to be added to the fire system. In-line valves shall be incorporated in the improvement plans. The additional valves will be required to be strategically placed in order to help isolate the backflow device if it needs to be repaired or replaced without shutting off the entire water system to the Medical Center. 	<p>I, G, B</p>	<p>CD (E), EWR</p>
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LANDSCAPE/TREE PRESERVATION REQUIREMENTS				
36.		<p>Final landscape plans and specifications shall be prepared by a registered landscape architect and approved by the City prior to the approval of the first building permit. Said plans shall include all on-site and applicable off-site landscape specifications and details including a tree planting exhibit demonstrating sufficient diversity and appropriate species selection to the satisfaction of the Community Development Department. The tree exhibit shall include all street trees, accent trees, parking lot shading trees, and mitigation trees proposed within the development. Said plans shall comply with all State and local rules, regulations, Governor’s declarations and restrictions pertaining to water conservation and outdoor landscaping.</p> <p>Landscaping shall meet shade requirements as outlined in the Folsom Plan Area Specific Plan where applicable. The landscape plans shall comply and implement water efficient requirements as adopted by the State of California (Assembly Bill 1881) (State Model Water Efficient Landscape Ordinance) until such time the City of Folsom adopts its own Water Efficient Landscape Ordinance at which time the owner/applicant shall comply with any new ordinance. Shade and ornamental trees shall be maintained according to the most current American National Standards for Tree Care Operations (ANSI A-300) by qualified tree care professionals. Tree topping for height reduction, view protection, light clearance or any other purpose shall not be allowed. Specialty-style pruning, such as pollarding, shall be specified within the approved landscape plans and shall be implemented during a 5-year establishment and training period. The owner/applicant shall comply with city-wide landscape rules or regulations on water usage. The owner/applicant shall comply with any state or local rules and regulations relating to landscape water usage and landscaping requirements necessitated to mitigate for drought conditions on all landscaping in the Folsom Ranch Medical Center project.</p>	B	CD (P)(E)
37.		<p>The owner/applicant shall be responsible for on-site landscape maintenance throughout the life of the project to the satisfaction of the Community Development Department. Vegetation or planting shall not be less than that depicted on the final landscape plan, unless tree removal is approved by the Community Development Department because the spacing between trees will be too close on center as they mature.</p>	B, OG	CD (P)(E)

PARKS AND RECREATION REQUIREMENTS				
38.		The owner/applicant shall be responsible for rough grading and installation of the necessary retaining wall to accommodate the Class I Bicycle Path on the northern portion of the subject property as shown in Figure 7.32 of the Specific Plan prior to issuance of a building permit on the second expansion of the hospital (Phase 4/2034). However, the City agrees that the owner/applicant shall be relieved of the aforementioned obligation should the City amend the Specific Plan to re-locate the Class 1 Bicycle Path, or if the City has not identified funding for construction of the Class 1 Bicycle Path across the Property and the connection to East Bidwell Street at the time of issuance of a building permit on the second expansion of the hospital for Phase 4. "Identified funding" shall mean either: (1) the Class 1 Bicycle Path is incorporated into a subsequent project to widen the East Bidwell Overcrossing structure of US Highway 50 or (2) the submission or application for federal, state or other grants which, together with the City's available matching funds, would be sufficient to construct the Class 1 Bicycle Path across the Property and the connection to East Bidwell Street.	B	CD (P)(E), PR
39.		The owner/applicant shall enter into an agreement with City to pay for services to maintain the mutually agreed upon private frontage landscaping along East Bidwell Street, Alder Creek Parkway, and McCarthy Way. Included in the agreement shall be a description of the services to be paid by the owner/applicant for the City to maintain the privately owned streetlights along frontages of East Bidwell Street and McCarthy Way. Streetlights located along Alder Creek Parkway, which are included in CFD 18, and are publicly owned and maintained and not considered part of this agreement.	B, OG	CD (P)(E), PR
40.		To accommodate the required width of the Class I Bicycle Path (minimum of 25 feet in width), a number of parking spaces (approximately 4 parking spaces) will need to be eliminated from the northwest corner of the project site to the satisfaction of the Community Development Department and the Parks and Recreation Department.	B, OG	CD (P)(E), PR

TRAFFIC/ACCESS/CIRCULATION/PARKING REQUIREMENTS				
41.		<p>Based on the recommendations of the Transportation Impact Study dated April 23, 2021 and April 29, 2021 (Attachment 20), the following conditions of approval shall be implemented to the satisfaction of the Community Development Department:</p> <ul style="list-style-type: none"> • Phase 1 Roadway Improvements (Anticipated 2023) <ul style="list-style-type: none"> ○ The owner/applicant shall construct McCarthy Way between Alder Creek Parkway and Placerville Road along with multiple access points to the site, consistent with City of Folsom Design and Operational Standards City of Folsom Design and Procedures Manual and Improvement Standards and City of Folsom Standard Construction Specifications and Details . ○ The owner/applicant shall construct an additional 250-foot right-turn lane for southbound McCarthy Way approaching Alder Creek Parkway connecting to a right-turn pocket for westbound Alder Creek Parkway approaching East Bidwell Street. The owner/applicant shall also construct a full extension of the right-turn pocket on westbound Alder Creek Parkway from McCarthy Way to East Bidwell Street. The owner/applicant shall adjust the location of the parking lot curb along the south and east edge of the project site accordingly. • Phase 2 Roadway Improvements (Anticipated 2028) <ul style="list-style-type: none"> ○ The owner/applicant shall construct a southbound emergency vehicle-only left-turn movement into the East Bidwell Street access point along with installation of a half-signal that will be triggered through emergency signal preemption to stop northbound traffic for enough time for the vehicle to safely complete the turning maneuver. • Phase 4 Roadway Improvements (Anticipated 2034) <ul style="list-style-type: none"> ○ Should the owner/applicant desire to construct a traffic signal at the Alder Creek Parkway/McCarthy Way intersection as an element of Phase 4 development, the owner/applicant shall first be required to perform a supplemental traffic analysis to assess the operational impacts associated with signalization of the intersection. 	B	CD (E), PW, FD

41. Cont.		<ul style="list-style-type: none">• Bus Rapid Transit (Anticipated BRT) Alignment<ul style="list-style-type: none">○ The westbound and eastbound BRT operations shall be center-running within the Alder Creek Parkway right-of-way to the satisfaction of the Community Development Department acknowledging that Sacramento Regional Transit, in consultation with the City, make the final determination of the location of BRT.	B	CD (E), PW, FD
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42.	<p>To facilitate the evaluation and installation of a future traffic signal at the intersection of Alder Creek Parkway and McCarthy Way, staff recommends the following measures shall be implemented to the satisfaction of the Community Development Department and the Public Works Department:</p> <ul style="list-style-type: none"> • The owner/applicant shall submit a Supplemental Traffic Impact Analysis to the City that evaluates the traffic impacts associated with installation of a traffic signal at the intersection of Alder Creek Parkway and McCarthy Way. The scope of the Traffic Impact Analysis shall be subject to review and approval by the Community Development Department. The owner/applicant shall be responsible for all costs associated with preparation of the Supplemental Traffic Impact Analysis. • If the Supplemental Traffic Impact Analysis concludes that there is no significant traffic operational impact, the traffic signal at the intersection of Alder Creek Parkway and McCarthy Way may be installed by Landowner pursuant to the Conditions of Approval and consistent with technical specifications as approved by the City Engineer. • If the Supplement Traffic Impact Analysis identifies any significant traffic operational impacts that can be mitigated and the owner/applicant desires to install the traffic signal, the City will identify the required mitigation and the owner/applicant shall install the traffic signal along with the required mitigation. • In the event the Supplemental Traffic Impact Analysis identifies any significant traffic operational impact that cannot be mitigated, the City will advise the owner/applicant and the traffic signal shall not be installed, and the intersection configuration approved as part of the original entitlements shall remain in place. • If the City determines that the left-turn access to the Medical Center at the intersection of Alder Creek Parkway and McCarthy Way must be discontinued for any reason, the City shall (a) advise owner/applicant of the City's intention to discontinue access, (b) provide owner/applicant with a reasonable opportunity to comment on the City's determination to discontinue access, including an appeal to the City Council, and (c) consider the impact on safe and efficient public access to the hospital and emergency services, including any evidence owner/applicant may provide to City with respect to increased driving times. 	OG	CD (P)(E), PW
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43.		<p>To ensure implementation of the traffic control measures identified on the submitted site plan, staff recommends the following measures are included as conditions of approval for the project.</p> <ul style="list-style-type: none"> • “Stop” signs and appropriate pavement markings shall be installed at the exits to the four project driveways located on McCarthy Way and East Bidwell Street respectively to the satisfaction of the Community Development Departments. • Additional warning signs and pavement markings (stamped concrete, brick pavers, etc.) shall be installed at the East Bidwell Street project driveway to alert pedestrians and bicyclists that emergency vehicles will be entering and exiting this driveway on a frequent basis and to be alert for incoming and outgoing vehicles to the satisfaction of the Community Development Department. • Brick pavers, stamped concrete, or another type of similar colored material (ADA compliant) shall be used to designate pedestrian crosswalks on the project site, in addition to where pedestrian paths cross drive aisles, and shall be incorporated as a design feature at the four project driveway entrances to the satisfaction of the Community Development Department. • Should the City determined that the existing eastbound left-turn pocket on Alder Creek at McCarthy Way needs to be eliminated, the owner/applicant shall be responsible for the installation of a full concrete median through the intersection and any other improvements associated with elimination of the left-turn pocket. The owner applicant shall also construct a pedestrian-actuated midblock protected crossing to coincide with implementation of BRT service to allow for safe pedestrian access to the BRT stop(s). Lastly, the owner/applicant shall construct a second eastbound left-turn lane at the intersection of Alder Creek Parkway and Westwood Drive. The owner/applicant shall be responsible for a “fair share” contribution for the aforementioned improvements, with reimbursement or fee credit provided for any portion that the City deems above the owner/applicant’s responsibility. The owner/applicant shall enter into a Deferred Improvement Agreement with the City for construction of the aforementioned improvements. 	B	CD (E), PW, FD
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44.	✓	<p>During project operation, and consistent with Mitigation Measure 3A.15-2c, the owner/applicant shall ensure on-going employer membership and participation by Dignity Health in the SACOG 50 Corridor Transportation Management Association (U.S. 50 TMA). In addition, given that employee participation in the U.S. 50 TMA is voluntary, the owner/applicant shall be required to conduct biennial Dignity Health employee surveys to ensure that at a minimum a 4.5 percent reduction in VMT (or 1,525 daily VMT) is achieved and maintained as part of project operations. Dignity Health shall be responsible for implementing biennial Dignity Health employee surveys to gauge participation with the various employee benefits offered by the U.S. 50 TMA. In order to ensure that the necessary reduction in VMT is being reported and achieved, the surveys shall include questions from which VMT reduction estimates can be estimated (e.g., how many days per week do you take alternative modes of transportation to work? How far do you live from your site of employment? etc.). Surveys and survey results shall be coordinated through and submitted to the U.S. 50 TMA, SACOG, and the City. If the required level of VMT reduction is not achieved, Dignity Health shall work with the City of Folsom and the TMA to identify other demand management related strategies to increase participation in the program and achieve the required reduction in VMT.</p>	OG	CD (P)
45.		A minimum of 990 on-site parking spaces shall be provided for the project.	I, O	CD (P)(E)
46.		A minimum of 56 on-site bicycle parking spaces shall be provided for the project in locations as identified on the preliminary site plan (Attachment 6). In addition, the owner/applicant shall provide additional secured locations (bicycle storage room, bicycle storage locker, etc.) within or adjacent to the medical office and hospital buildings to provide for long-term bicycle storage for employees to the satisfaction of the Community Development Department.	I, O	CD (P)(E)
ARCHITECTURE/SITE DESIGN REQUIREMENTS				
47.		Each of the buildings associated with the Folsom Ranch Medical Center including but not limited to the two medical office buildings, the hospital building, and the central plant building shall require future Design Review approval by the Planning Commission to ensure consistency with the Folsom Ranch Medical Center Planned Development Guidelines. If a proposed building is not in compliance with the Planned Development Guidelines a Planned Development Permit Modification will be required.	B	CD (P)(B)

48.		The final trash and recycling collection plan associated with any City required services shall be reviewed and approved by the Solid Waste Division and the Community Development Department. In addition, the trash and recycling area and the trash and recycling enclosures shall be screened to the satisfaction of the Community Development Department. Lastly, the final location, design, materials, and colors of any future trash/recycling enclosures shall be subject to review and approval by the Community Development Department.	B	CD (P) (E)
49.		The location of all wall signs shall be shown on the building elevations and renderings that are submitted for future Design Review Approval by the Planning Commission. In addition, the owner/applicant shall obtain a sign permit prior to installation of any signs that require a building permit	B	CD (P)
MISCELLANEOUS REQUIREMENTS				
50.		The proposed project shall comply with all State and local rules, regulations, Governor's Declarations, and restrictions relative to water usage and conservation including but not limited to: Executive Order B-29-15 issued by the Governor of California on April 1, 2015 relative to water usage and conservation, requirements relative to water usage and conservation established by the State Water Resources Control Board, and water usage and conservation requirements established within the <u>Folsom Municipal Code, (Chapter 13.26 Water Conservation)</u> , or amended from time to time.	I, B, OG	CD (P)(E)
MITIGATION MEASURES				
51.	✓	<i>Folsom Ranch Medical Center Mitigation Monitoring Reporting Program (MMRP).</i> The owner/applicant shall implement all of the applicable mitigation measures from the FPASP (May 2011) MMRP, as amended by the Revised Proposed Water Supply Facility Alternative (November 2012), the Folsom South of U.S. Highway 50 Backbone Infrastructure Mitigated Negative Declaration (December 2014), the Westland Eagle Specific Plan Amendment (September 2015), and the Folsom Ranch Medical Center Addendum (April 2021). The Mitigation Monitoring and Reporting Program for the Folsom Ranch Medical Center project is included as Attachment 23 to the staff report.	I, G, B, OG	CD (E)(P), PW, FD, EWR, PD, PR

Planning Commission
Folsom Ranch Medical Center
May 19, 2021

CONDITIONS

See attached tables of conditions for which the following legend applies.

RESPONSIBLE DEPARTMENT		WHEN REQUIRED	
CD	Community Development Department Planning Division Engineering Division Building Division Fire Division	I	Prior to approval of Improvement Plans
(P)		M	Prior to approval of Final Map
(E)		B	Prior to issuance of first Building Permit
(B)		O	Prior to approval of Occupancy Permit
(F)		G	Prior to issuance of Grading Permit
PW	Public Works Department	DC	During construction
PR	Park and Recreation Department	OG	On-going requirement
PD	Police Department		

Planning Commission
Folsom Ranch Medical Center
May 19, 2021

Attachment 5

Vicinity Map

Vicinity Map



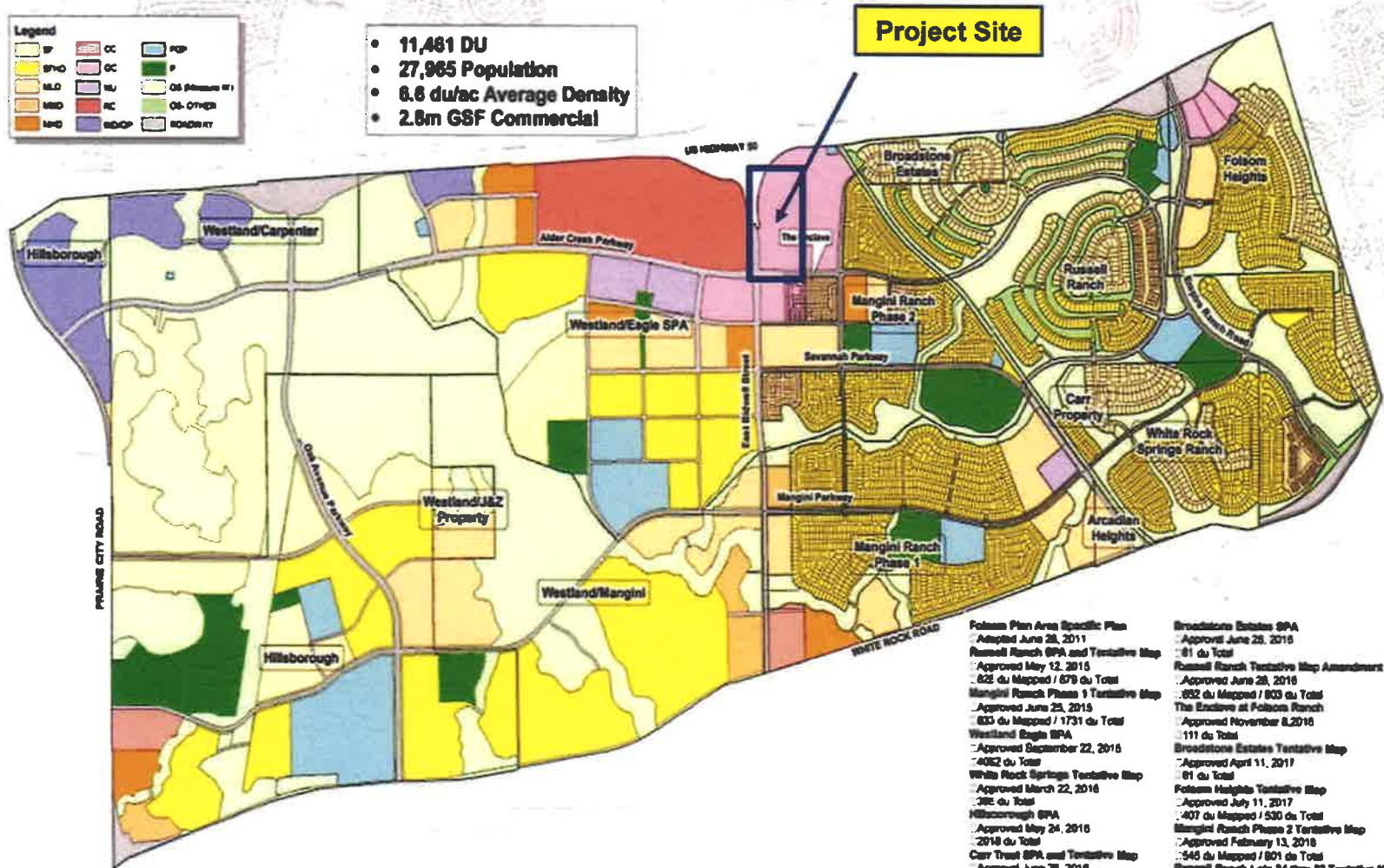
CITY OF
FOLSOM

Legend

SP	CC	PCP
SPHO	CC	P
MLD	ML	OS (Pavement RT)
MWD	RC	OS (Other)
MND	ROD/OP	ROADS RT

- 11,461 DU
- 27,965 Population
- 6.6 du/ac Average Density
- 2.8m GSF Commercial

Project Site

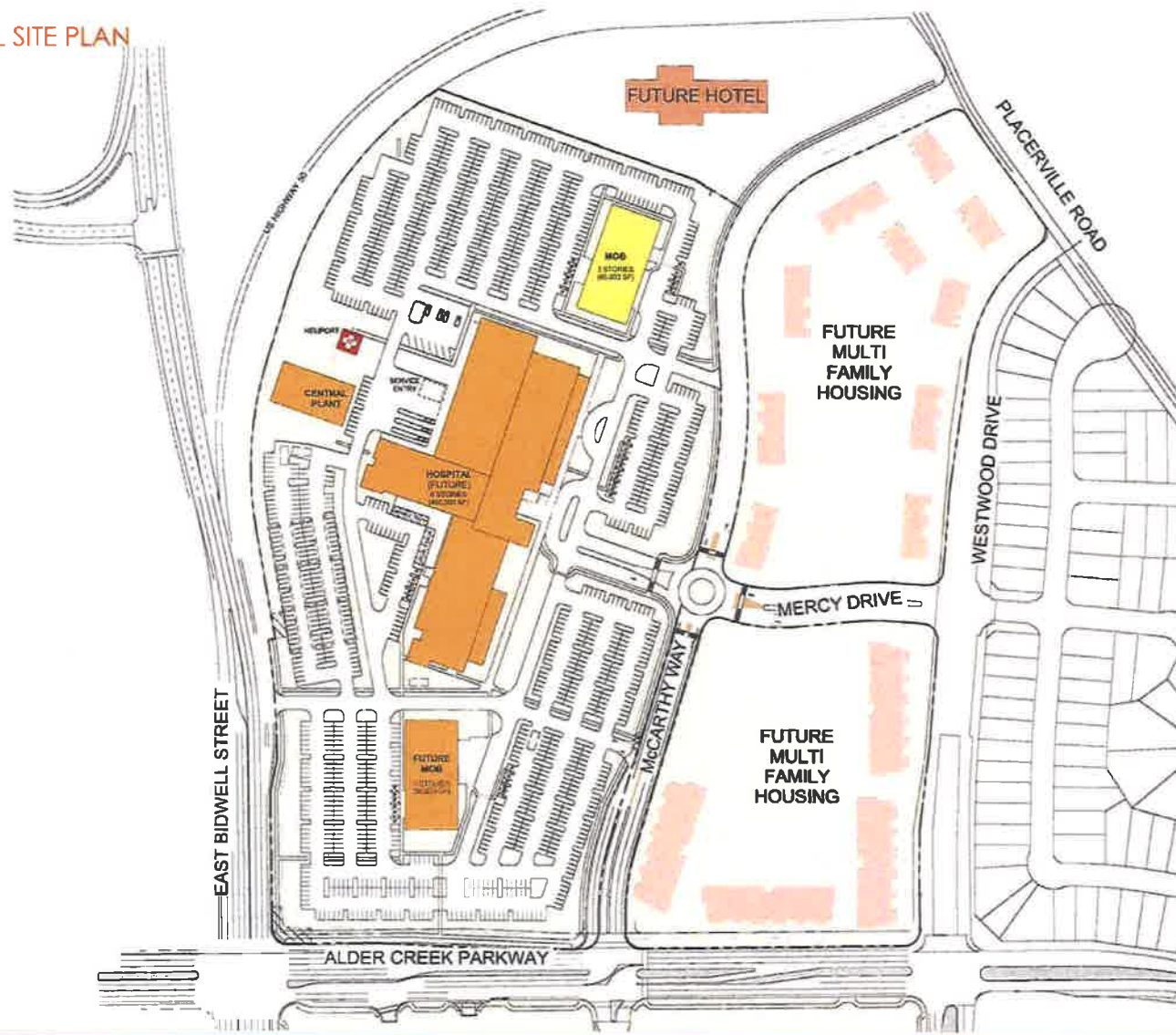


- | | |
|---|---|
| <p>Folsom Plan Area Specific Plan
 - Adopted June 28, 2011
 Russell Ranch SPA and Tentative Map
 - Approved May 12, 2015
 - 623 du Mapped / 679 du Total
 Mangini Ranch Phase 1 Tentative Map
 - Approved June 23, 2015
 - 633 du Mapped / 1731 du Total
 Westland/Eagle SPA
 - Approved December 22, 2016
 - 4062 du Total
 White Rock Springs Tentative Map
 - Approved March 22, 2016
 - 392 du Total
 Hillsborough SPA
 - Approved May 24, 2016
 - 2018 du Total
 Carr Tract SPA and Tentative Map
 - Approved June 28, 2016
 - 29 du Total
 Folsom Heights SPA
 - Approved June 28, 2016
 - 407 du Mapped / 530 du Total</p> | <p>Broadstone Estates SPA
 - Approved June 28, 2016
 - 81 du Total
 Russell Ranch Tentative Map Amendment
 - Approved June 28, 2016
 - 692 du Mapped / 803 du Total
 The Enclave at Folsom Ranch
 - Approved November 8, 2016
 - 111 du Total
 Broadstone Estates Tentative Map
 - Approved April 11, 2017
 - 81 du Total
 Folsom Heights Tentative Map
 - Approved July 11, 2017
 - 407 du Mapped / 530 du Total
 Mangini Ranch Phase 2 Tentative Map
 - Approved February 13, 2018
 - 543 du Mapped / 601 du Total
 Russell Ranch Lots 24 thru 33 Tentative Map
 - Approved March 13, 2018
 - 399 du Total</p> |
|---|---|

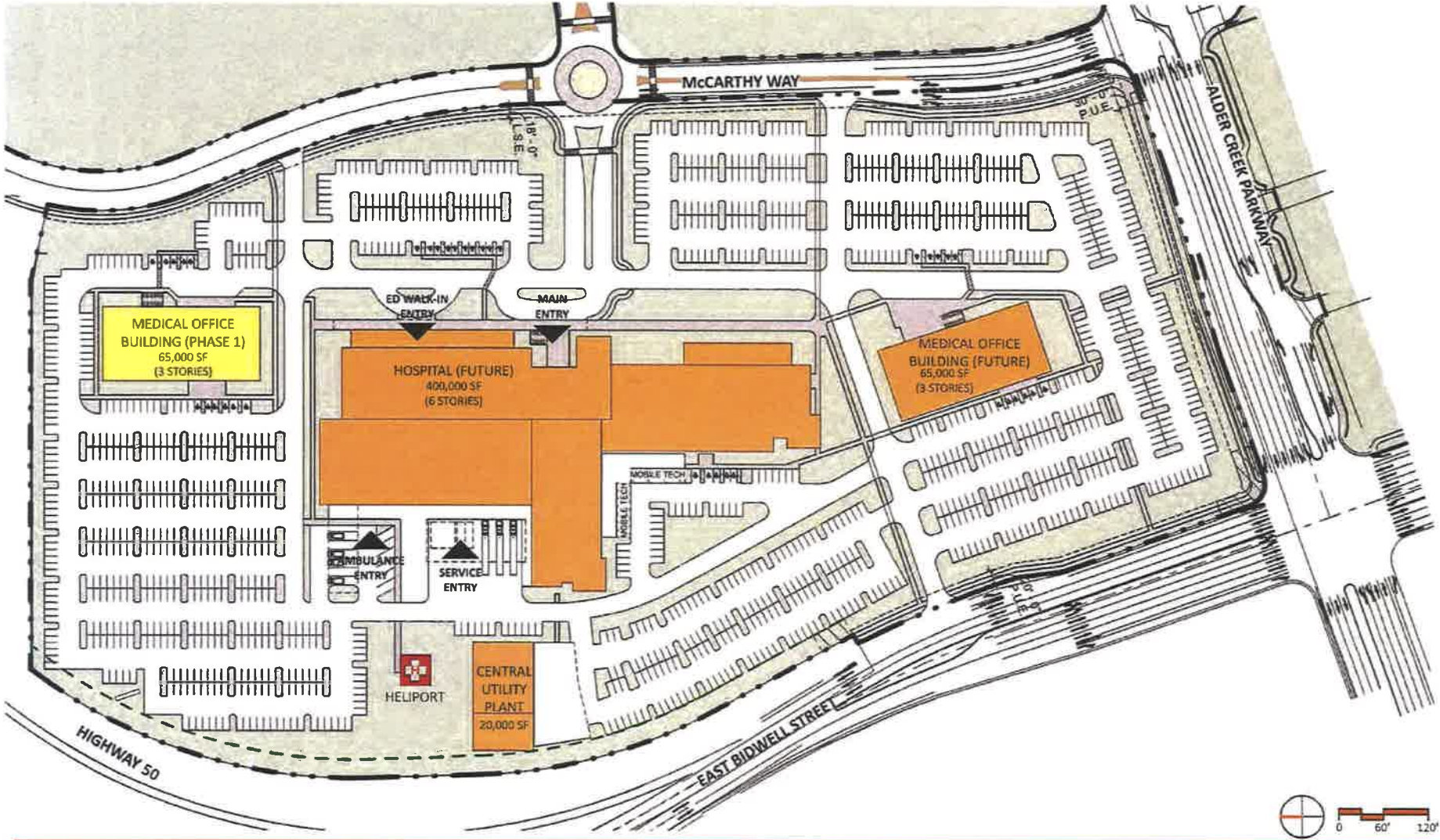
Attachment 6

Preliminary Site Plan, dated May 6, 2021

1. SITE PLAN OVERVIEW
PROPOSED OVERALL SITE PLAN



1. SITE PLAN OVERVIEW
PROPOSED ENLARGED SITE PLAN

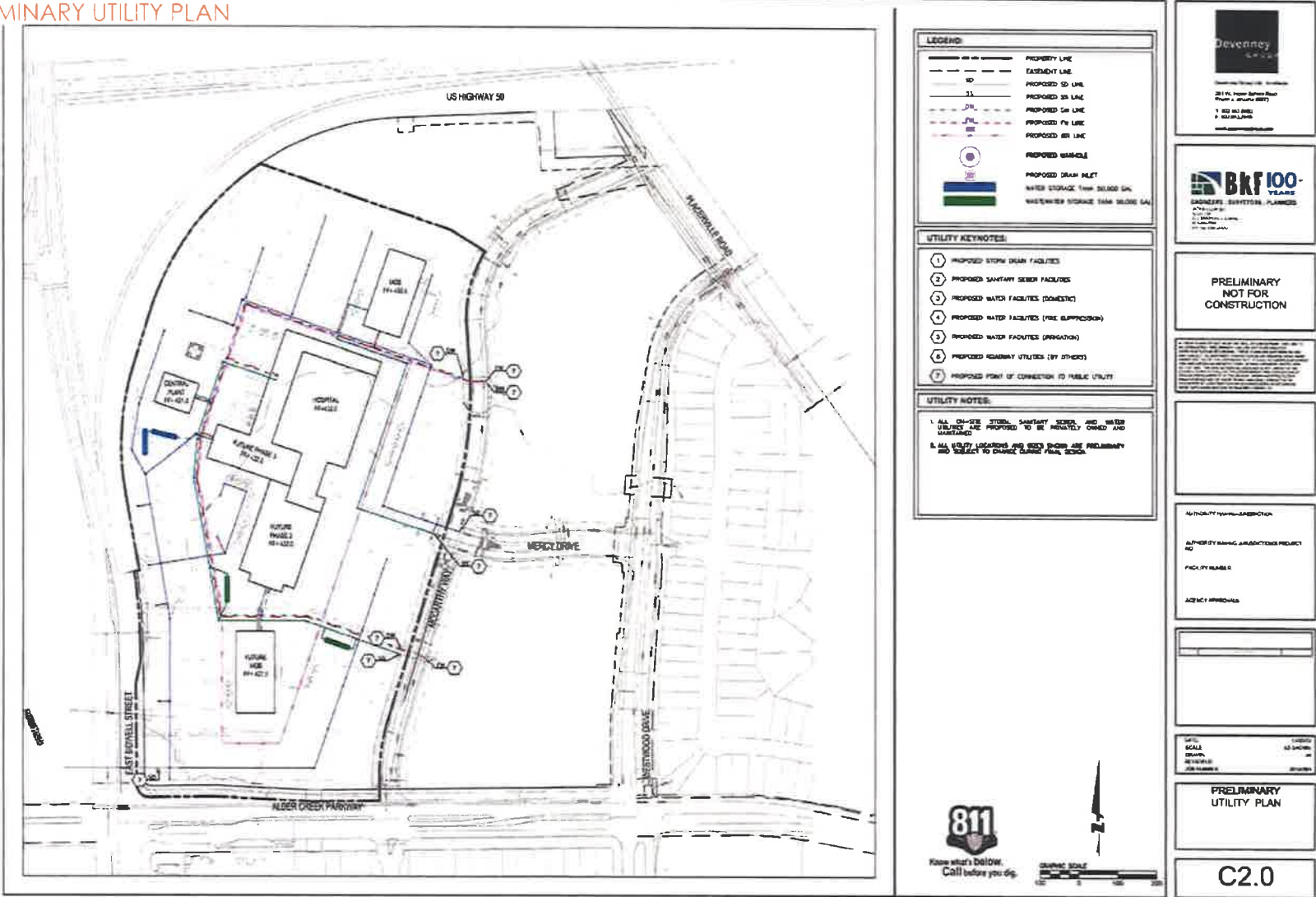


Planning Commission
Folsom Ranch Medical Center
May 19, 2021

Attachment 7

Preliminary Utility Plan, dated May 6, 2021

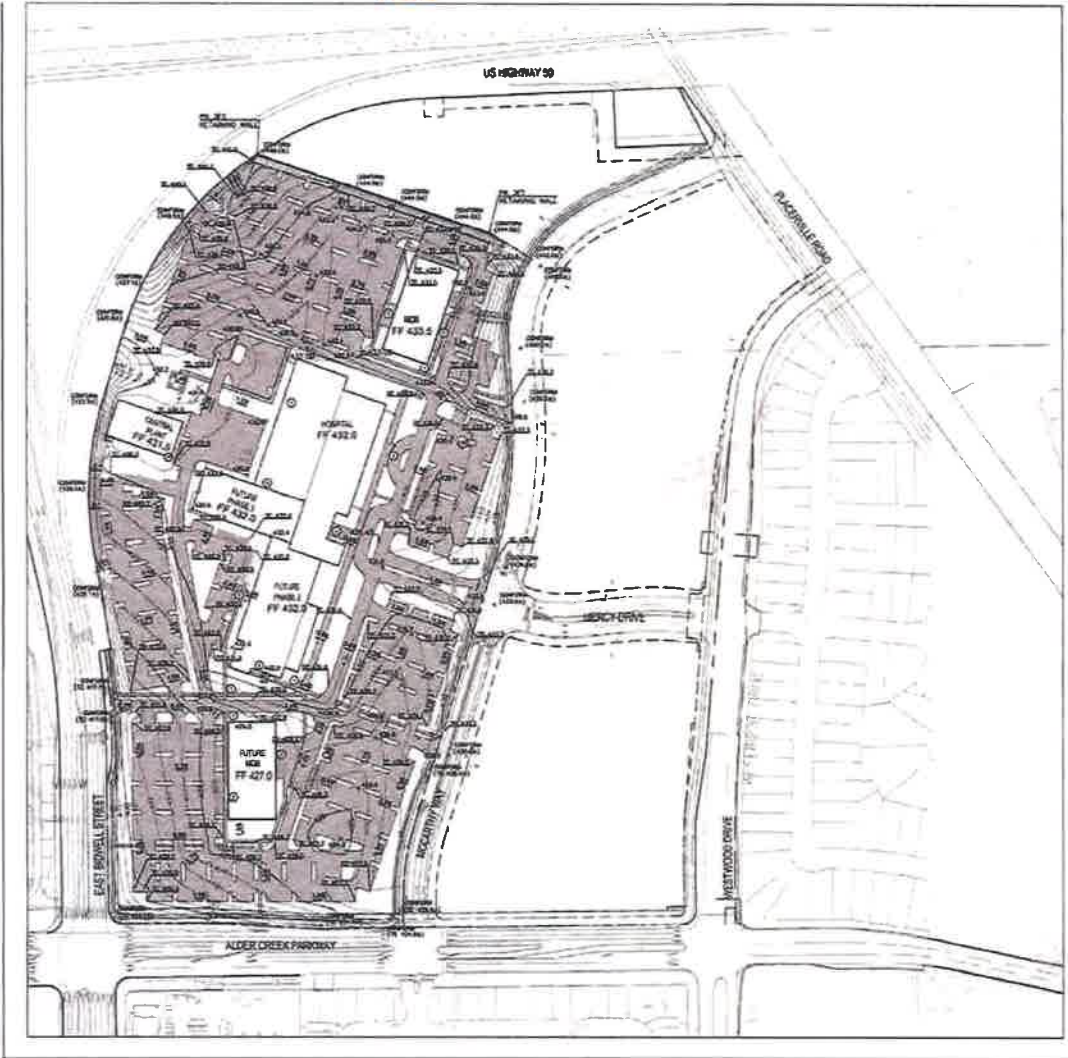
2. SITE PLAN DETAILS
PRELIMINARY UTILITY PLAN



Attachment 8

Preliminary Grading and Drainage Plan Dated May 6, 2021

2. SITE PLAN DETAILS PRELIMINARY GRADING & DRAINAGE PLAN



LEGEND:

	PROPERTY LINE
	GRADE BREAK
	EXISTING CONTOUR
	PROPOSED CONTOUR
	PROPOSED FINISH GRADE
	EXISTING FINISH GRADE
	DRAINAGE SLOPE ARROW
	PROPOSED PAVED AND SIDEWALK AREAS

GRADING KEY NOTES:

	ACCESSIBLE RAMP SYSTEM BETWEEN BUILDINGS
	PRELIMINARY BUILDING ACCESS POINT

811
Know what's below.
Call before you dig.

GRAPHIC SCALE
1" = 100'

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INCORPORATED
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APPROVED FOR THE CITY OF FOLSOM
AUTHORITY: PUBLIC WORKS DEPARTMENT
DATE: 06/08/2021
CITY ENGINEER: [Signature]

DATE: 06/08/2021
SCALE: AS SHOWN
BY: [Signature]

DATE: 06/08/2021
SCALE: AS SHOWN
BY: [Signature]

**PRELIMINARY
GRADING PLAN**

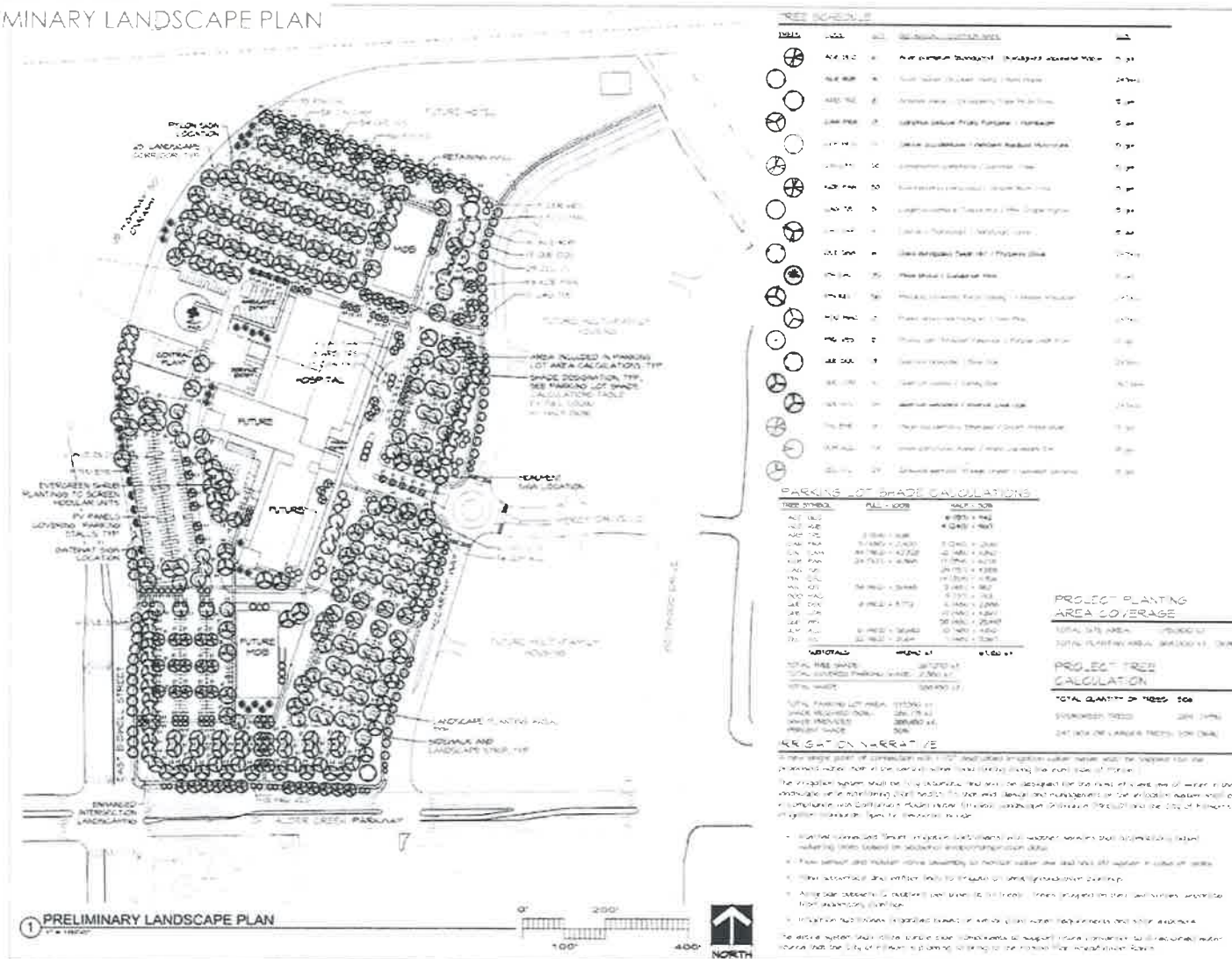
C1.0

Planning Commission
Folsom Ranch Medical Center
May 19, 2021

Attachment 9

Preliminary Landscape Plans
Dated May 6, 2021

2. SITE PLAN DETAILS PRELIMINARY LANDSCAPE PLAN



Devenney Group
 111 St. Johns Street
 Folsom, CA 95630
 Tel: 916.451.1111
 Fax: 916.451.1112
 www.devenneygroup.com

YAMASAKI
 LANDSCAPE ARCHITECTURE
 4110 UNIVERSITY AVENUE
 SUITE 100
 SACRAMENTO, CA 95817
 Tel: 916.442.1111
 Fax: 916.442.1112
 www.yamasakilandscape.com

PRELIMINARY NOT FOR CONSTRUCTION

FOLSOM RANCH FUTURE DEVELOPMENT
 DIGNITY HEALTH

PROJECT PLANTING AREA COVERAGE

PROJECT TREE CALCULATION

TOTAL QUANTITY OF TREES: 506

PRELIMINARY LANDSCAPE PLAN

L1

Attachment 10

Preliminary Lighting Plan
Dated May 6, 2021

2. SITE PLAN DETAILS
LIGHTING & PHOTOMETRIC PLAN



Calculation Summary

Label	Avg	Max	Min	Avg/Min	Max/Min
Ambulance Entry	3.2	6.5	2.9	3.5	7.8
Hospital Canopy	2.0	6.8	0.8	5.5	11.0
Parking Lot	1.8	6.4	0.4	4.6	16.0
Service Entry	2.7	5.8	1.1	2.5	6.3
Walkway_1	1.8	4.4	0.5	3.5	8.8
Walkway_2	2.3	10.9	0.6	3.9	18.2
Walkway_3	1.6	3.7	0.6	2.7	6.2
Walkway_4	1.6	2.4	0.9	1.8	2.7
Walkway_5	1.2	1.8	0.7	1.7	2.6
Walkway_6	2.2	4.4	0.6	3.7	7.3

2 PRELIMINARY PHOTOMETRIC LEVELS

3 PRELIMINARY LIGHTING FIXTURE SCHEDULE

ITEM	DESCRIPTION	QUANTITY	MANUFACTURER	WATTAGE	BEAM ANGLE	HEIGHT	NOTES
1	150W LED Flood Light	10	ACORNS	150W	120°	10'	
2	150W LED Flood Light	10	ACORNS	150W	120°	10'	
3	150W LED Flood Light	10	ACORNS	150W	120°	10'	
4	150W LED Flood Light	10	ACORNS	150W	120°	10'	
5	150W LED Flood Light	10	ACORNS	150W	120°	10'	
6	150W LED Flood Light	10	ACORNS	150W	120°	10'	
7	150W LED Flood Light	10	ACORNS	150W	120°	10'	
8	150W LED Flood Light	10	ACORNS	150W	120°	10'	
9	150W LED Flood Light	10	ACORNS	150W	120°	10'	
10	150W LED Flood Light	10	ACORNS	150W	120°	10'	
11	150W LED Flood Light	10	ACORNS	150W	120°	10'	
12	150W LED Flood Light	10	ACORNS	150W	120°	10'	
13	150W LED Flood Light	10	ACORNS	150W	120°	10'	
14	150W LED Flood Light	10	ACORNS	150W	120°	10'	
15	150W LED Flood Light	10	ACORNS	150W	120°	10'	
16	150W LED Flood Light	10	ACORNS	150W	120°	10'	
17	150W LED Flood Light	10	ACORNS	150W	120°	10'	
18	150W LED Flood Light	10	ACORNS	150W	120°	10'	
19	150W LED Flood Light	10	ACORNS	150W	120°	10'	
20	150W LED Flood Light	10	ACORNS	150W	120°	10'	

Devenney
ARCHITECTURAL
20141 North Spring Road
Folsom, CA 95632
708.453.4400
www.devenney.com

SILVERMAN & LIGHT INC
10000 Folsom Blvd, Suite 100
Folsom, CA 95630
916.453.4400
www.silvermanlight.com

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FOLSOM RANCH
FUTURE DEVELOPMENT
DIGNITY HEALTH

E1.01

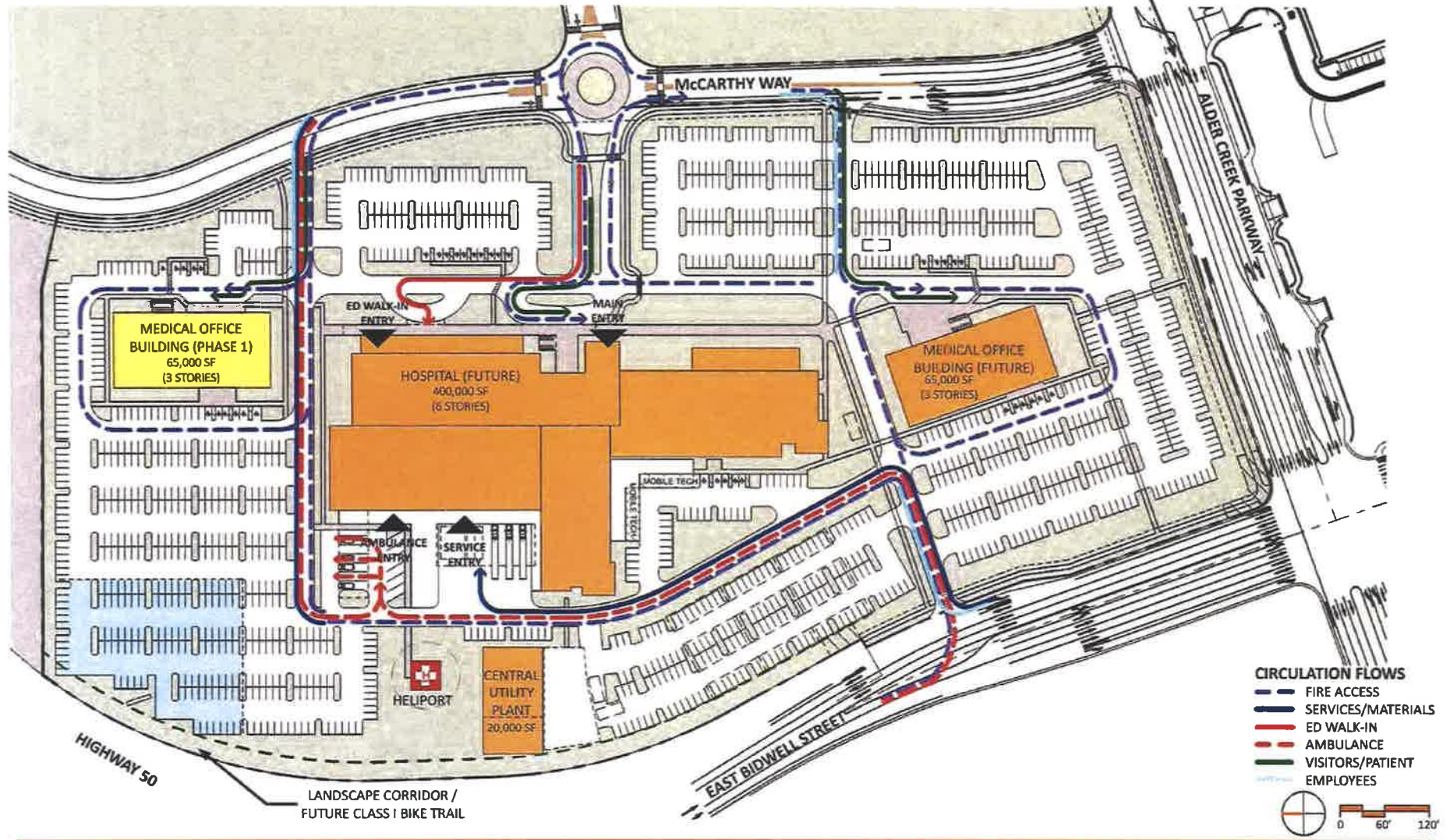
Attachment 11

Preliminary Access and Circulation Plan Dated March 16, 2021

1. SITE PLAN OVERVIEW

PRELIMINARY ACCESS & CIRCULATION PLAN

SECOND LEFT TURN - FUTURE IMPROVEMENT,
CONTINGENT ON THE SIGNALIZATION OF McCARTHY WAY

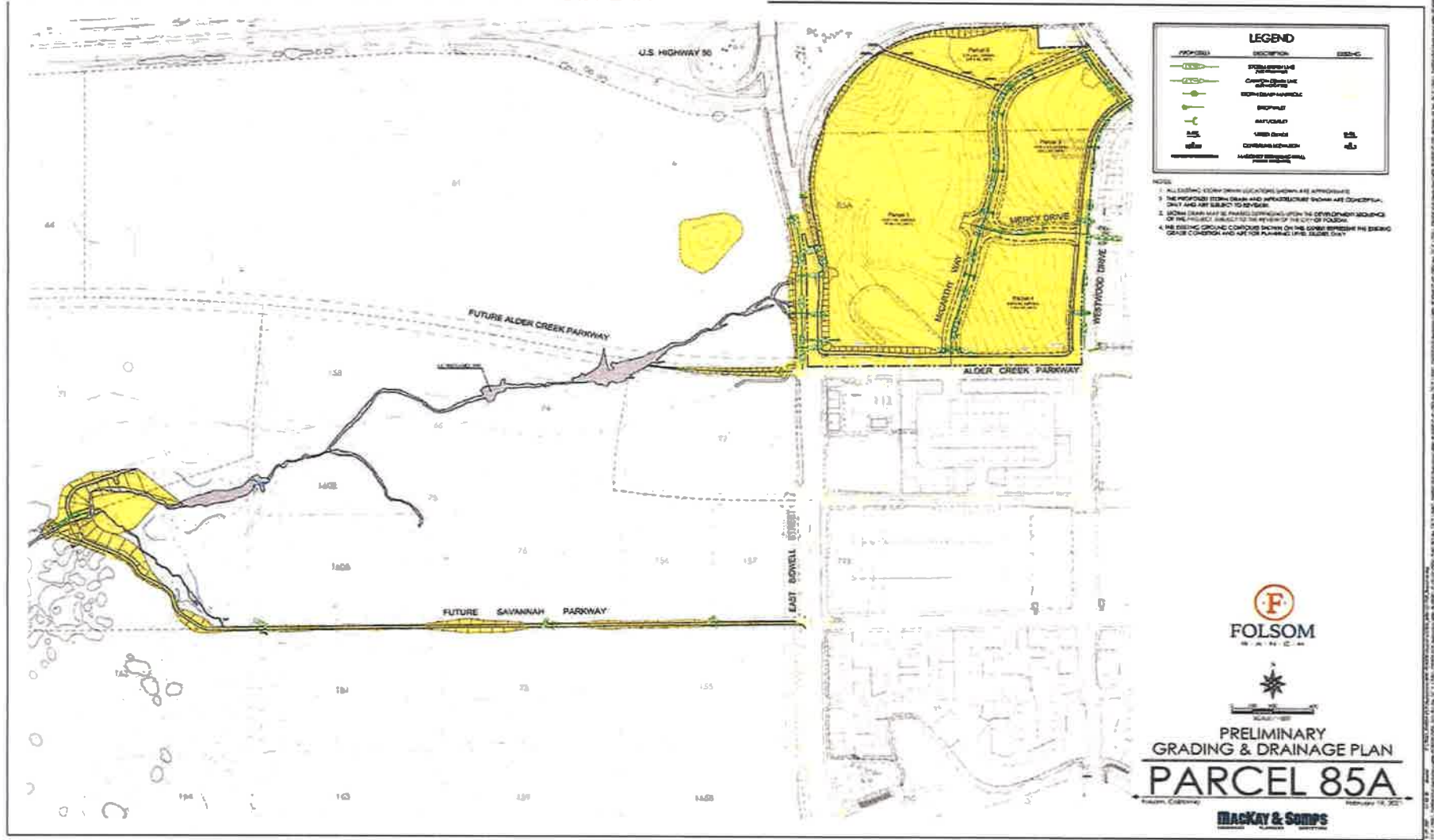


Planning Commission
Folsom Ranch Medical Center
May 19, 2021

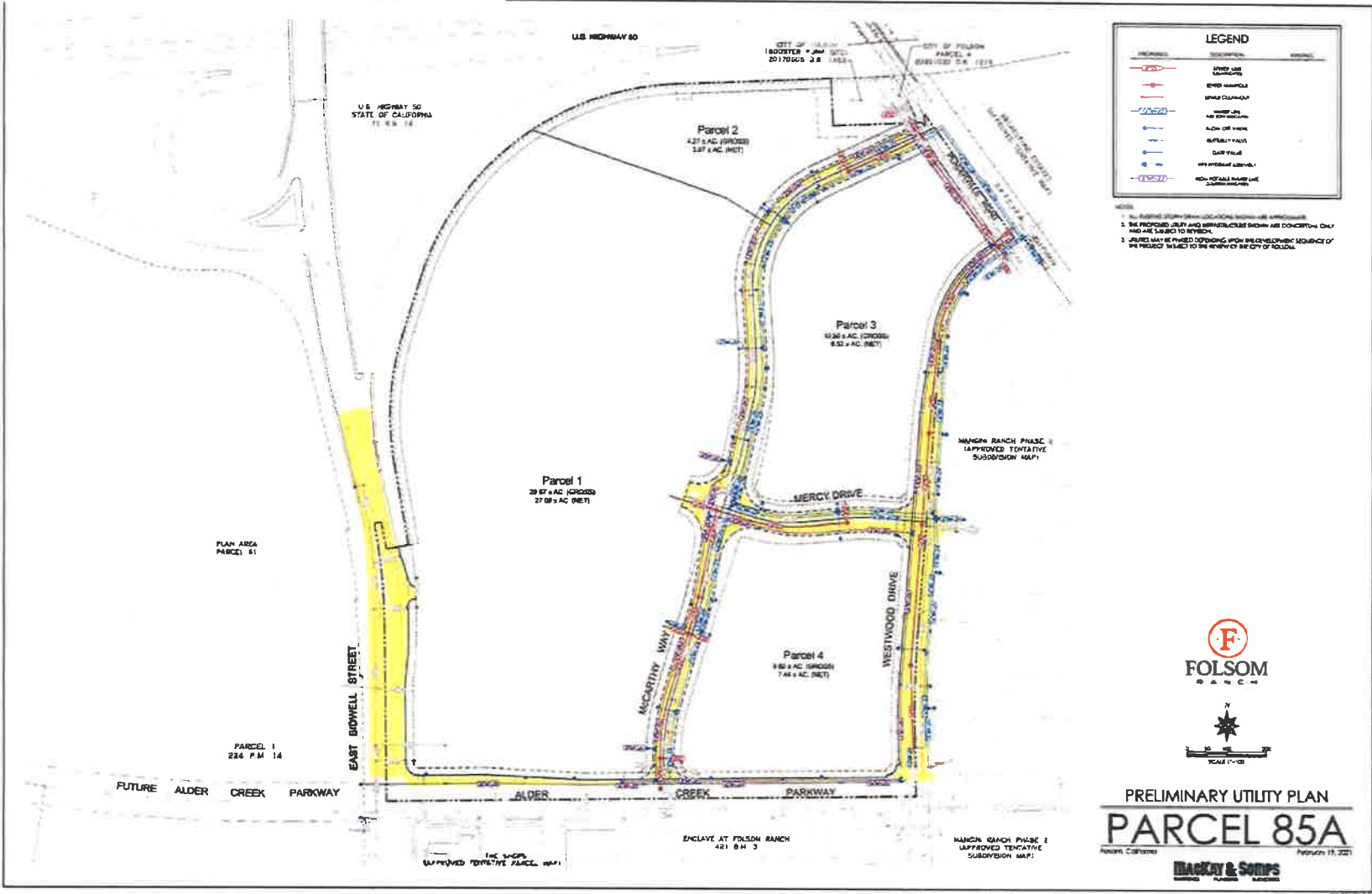
Attachment 12

Preliminary Off-Site Improvement Plans Dated May 6, 2021

3. PRELIMINARY OFF-SITE IMPROVEMENT PLANS PARCEL 85A - PRELIMINARY GRADING & DRAINAGE PLAN



3. PRELIMINARY OFF-SITE IMPROVEMENT PLANS
PARCEL 85A - PRELIMINARY UTILITY PLAN



Attachment 13

Conceptual Building Renderings
Dated May 6, 2021

4. CONCEPTUAL RENDERINGS
COMPLETE HOSPITAL AERIAL



8 | ARCHITECTURAL GUIDELINES



SOUTH ELEVATION

SCALE: 1" = 20'



EAST ELEVATION

SCALE: 1" = 20'

EXHIBIT I: EXTERIOR ELEVATIONS

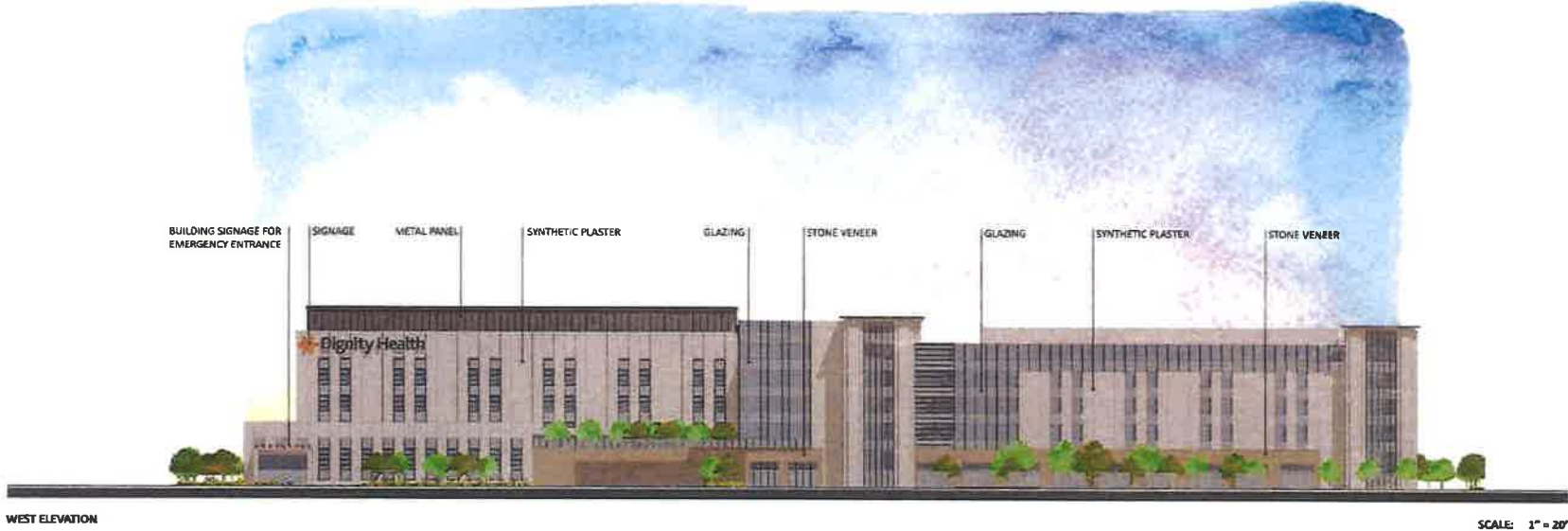
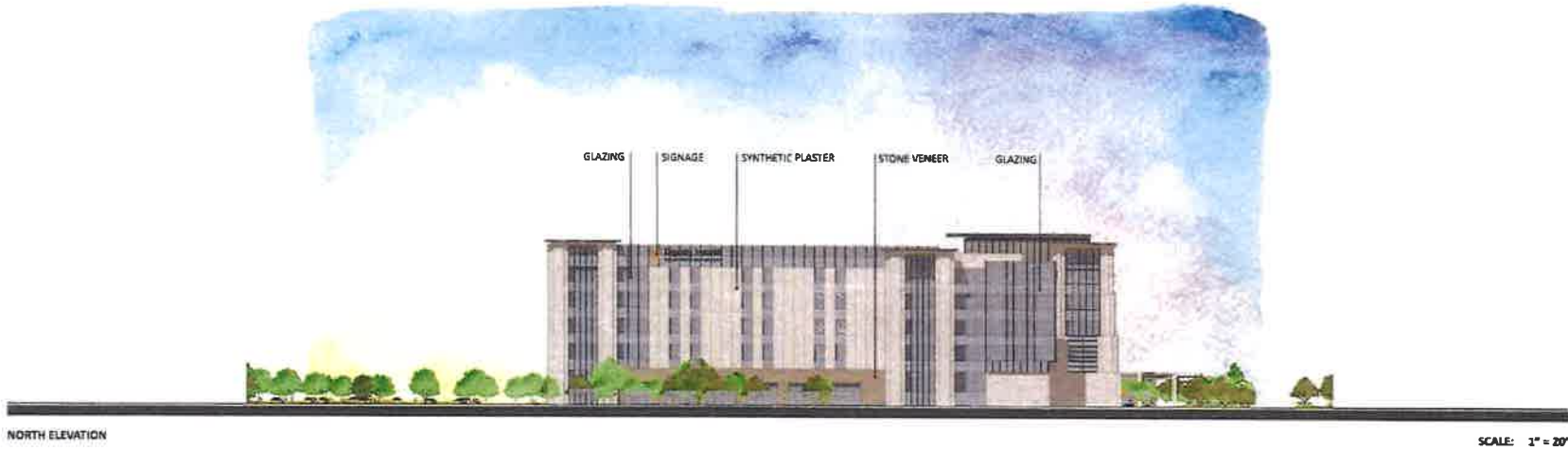


EXHIBIT J: EXTERIOR ELEVATIONS

Planning Commission
Folsom Ranch Medical Center
May 19, 2021

Attachment 14

Planned Development Guidelines
Dated May 12, 2021

Planned Development Guidelines



May 12, 2021

Planned Development Guidelines Folsom Ranch Medical Center

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1 | INTRODUCTION

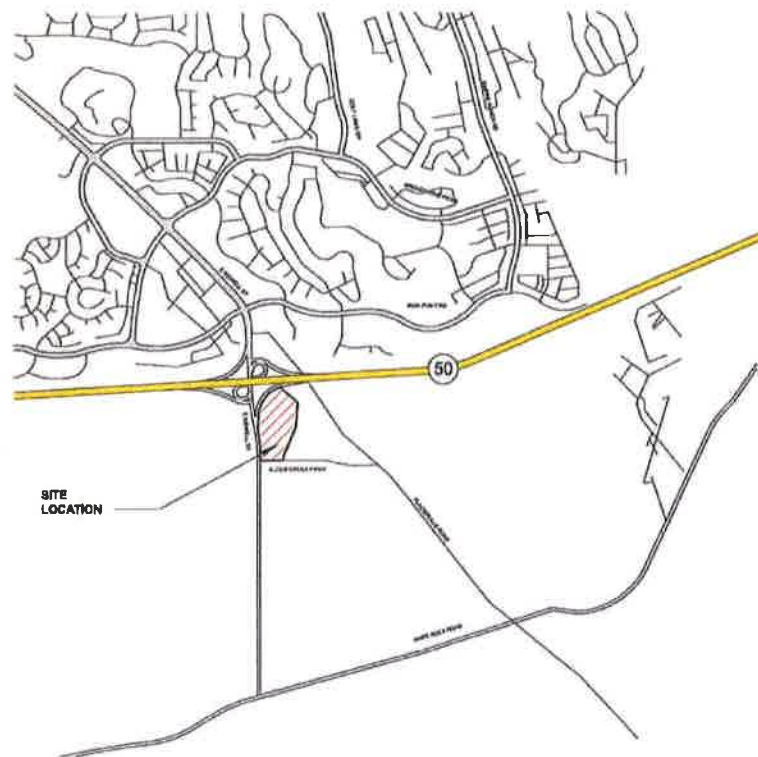
The Planned Development Guidelines (“Guidelines”) contained herein pertain to the Dignity Health Folsom Ranch Medical Center located within the Folsom Ranch development area. These Guidelines provide the framework for the Planned Development Permit, minor variances from the standards set forth in the Folsom Plan Area Specific Plan (FPASP) and Folsom Municipal Code, to accommodate an acute care hospital and medical office buildings, heliport, site design and planning, landscape, building exterior design, site lighting, and signage for the future development.

Dignity Health’s vision is to develop a dynamic new Folsom Ranch Medical Center providing a broad range of healthcare services to establish Folsom and Dignity Health as a destination and regional provider of health care services. The Medical Center includes the medical office buildings and hospital development. Folsom Ranch Medical Center will include primary and specialty care practices along with outpatient imaging, ambulatory surgery center, and other ancillary services. A new hospital with expanded acute services is envisioned in future phases of the Medical Center development

The project intent is to observe the responsible best practices of Folsom’s Community Development and also to engage flexibility for a new Medical Center campus. This development submission is the framework for a human centered healing environment that will set new foundations for quality and innovation.

2 | PROJECT LOCATION AND DESCRIPTION

The 27.44-acre site is located at the Northeast corner of East Bidwell Street and Alder Creek Parkway. The site is identified as "Parcel 1," and is located within the parcel identified by the FPASP as Parcel 85a, which extends to the intersection of Placerville Road and Westwood Drive. The Medical Center is planned for an ultimate build out of approximately 530,000 occupied square feet including a 300-bed 400,000 occupied square foot acute care hospital and two 65,000 occupied square foot medical office buildings. The development of this size is anticipated/strategized to serve the expanding population in the city of Folsom and its adjacent communities. Development of the site is intended to be accomplished over the course of approximately five individual phases. Although community needs and business conditions will affect both the order of phases and timing, it is generally anticipated that one medical office building will be constructed in the first phase as the most feasible and quickest pathway to providing high quality outpatient medical services to the growing community. Thereafter, the first phase of the main hospital will be constructed, offering critical acute-care services to include an emergency department, operating rooms with supporting services, imaging services, and patient beds. In order to serve the community and its surrounding region, a heliport for transfer of patients to or from the hospital is anticipated, in order to provide the highest level of care. The ultimate future buildout of the site as proposed is a coordinated response to the increased density of the surrounding residential community.



VICINITY MAP

3 | PARCEL 85A GRADING, INFRASTRUCTURE AND OFF SITE IMPROVEMENTS

The proposed development of the Folsom Ranch Medical Center necessitates the mass grading of the larger FPASP Parcel 85A (as shown and so identified in the Folsom Plan Area Specific Plan) and construction of onsite and offsite roadway and utility infrastructure.

Existing topography on the site ranges from approximately 470' to 400' in elevation and generally falls in the southwest direction. The site will be mass graded to provide developable areas and achieve earthwork balance. Onsite retaining walls (approximately 2' to 6' in height) are anticipated to maintain maximum developable areas and intended road grades. Excavation at a borrow site approximately 400' west of East Bidwell Street will provide fill material to widen the west side of the East Bidwell roadway. Fill material will be moved from the borrow site to the fill location by way of ground-disturbing equipment.

At buildout, surface runoff will generally flow to the southwest where it will be conveyed by offsite storm drain infrastructure to the offsite Hydromodification Basin #8 (HMB#8) located west of East Bidwell Street in the location as approved in the FPA Storm Drainage Master Plan (Exhibit M, page 47) and FPA Specific Plan (Figure 12.4 Stormwater Plan, page 248). The first phase of the basin will be constructed with the development of the Folsom Medical Center, including the basin outlet control structure and spillway, a paved access road along the Savannah Parkway alignment and perimeter of the basin, and a storm drain outfall swale from the western terminus of Alder Creek Parkway to the existing downstream waterway. Refer to Parcel 85A - Preliminary Grading and Drainage Plan Exhibit A.

Backbone Roadway improvements include the widening of East Bidwell Street and Alder Creek Parkway and the construction of Westwood Drive. Two additional roadways, Mercy Drive and McCarthy Way, are proposed within Parcel 85A to facilitate access and circulation for the Folsom Ranch Medical Center. Refer to Parcel 85A - Preliminary Utility Plan Exhibit B.

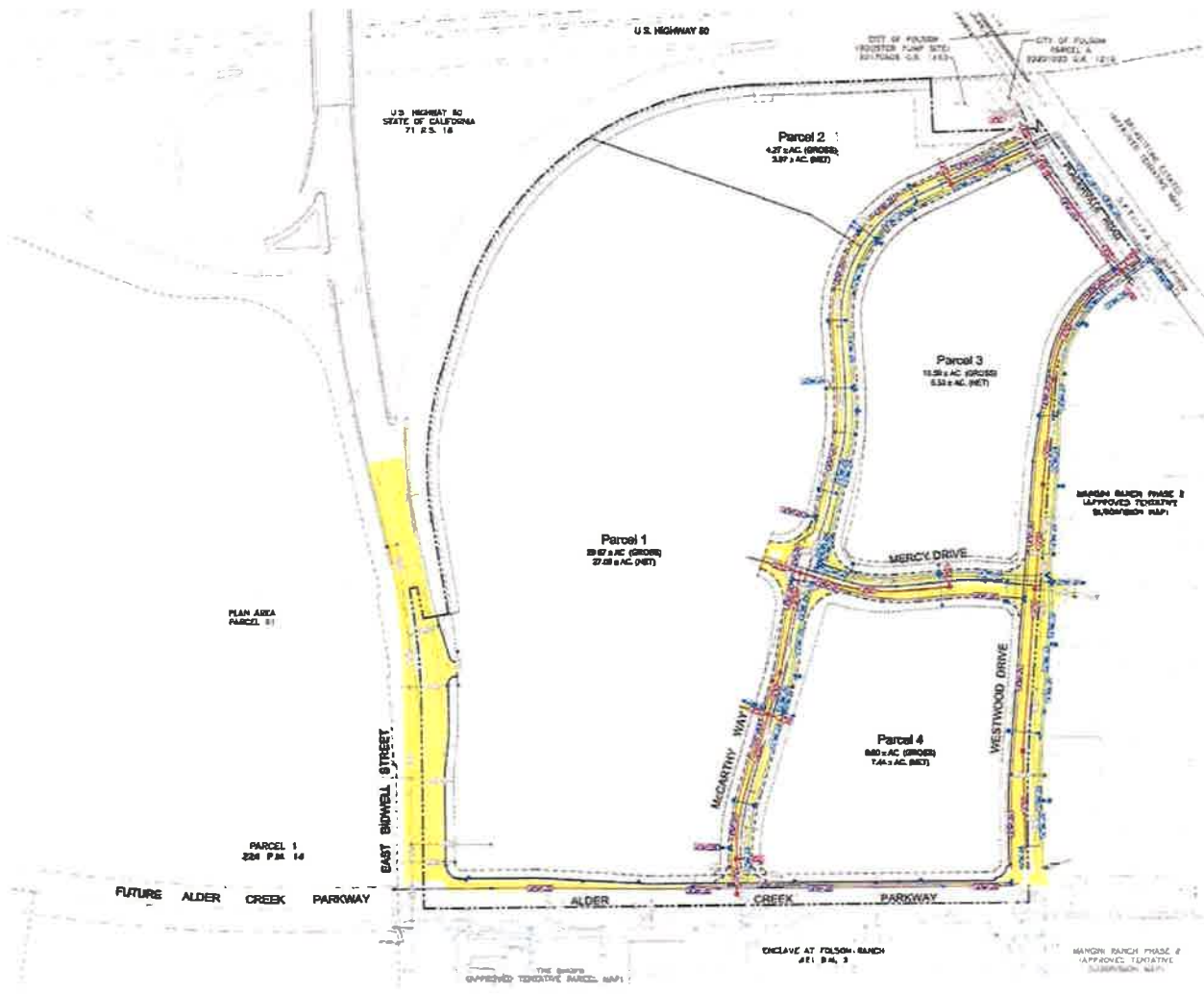
Existing backbone utility infrastructure within Alder Creek Parkway is adequately sized to serve the development of the Folsom Ranch Medical Center. Utilities, including storm drain, sanitary sewer, potable and non-potable water, and dry utility infrastructure will be extended within East Bidwell Street, Westwood Drive and Placerville Road as well as within onsite streets Mercy Drive and McCarthy Way to provide service to the proposed development. Refer to Preliminary Utility Plan Exhibit B. Further detail about water supply to Folsom Ranch Medical Center can be found in Section 7 of these Guidelines under "Water Supply and Infrastructure."

The following is a summary of grading and off site infrastructure improvements that support the Folsom

Ranch Medical Center:

- Mass grading of Parcel 85A and adjacent roadways resulting in contour graded developable parcels and subgrade along roadway alignments;
- Rough grading of a portion of Hydromodification Basin #8 (HMB#8) including the access road along the Savannah Parkway alignment and perimeter of the basin;
- Rough Grading of a storm drain outfall swale from the western terminus of Alder Creek Parkway to the existing downstream waterway;
- Excavation at a borrow site west of East Bidwell;
- Roadway and Utility improvements along East Bidwell, Alder Creek Parkway, Westwood Drive, Placerville Road, McCarthy Drive and Mercy Way including storm drain, sanitary sewer, potable and non-potable water, and dry utility infrastructure;
- HMB#8 improvements including a paved access road to East Bidwell and basin outlet control structure and spillway.

The infrastructure improvements will be completed in advance of any building construction on Parcel 1. While grading and other infrastructure development will occur on Parcels 2-4 and other off site locations, no entitlements on Parcel 2 thru 4 of Parcel 85A are included as part of this project.



LEGEND

SYMBOL	DESCRIPTION	NOTES
(Symbol)	WATER MAIN	
(Symbol)	SEWER MAIN	
(Symbol)	SEWER OVERHEAD	
(Symbol)	WATER OFF MAIN	
(Symbol)	SEWER OFF MAIN	
(Symbol)	GAS MAIN	
(Symbol)	TELEPHONE/COAXIAL	

- 1. ALL SHOWN UTILITIES ARE TO BE VERIFIED BY THE ENGINEER.
- 2. THE PROPOSED UTILITIES AND CONNECTIONS SHOWN ARE CONCEPTUAL ONLY AND ARE SUBJECT TO REVIEW.
- 3. UTILITIES MAY BE PLACED DEPENDENT UPON THE DEVELOPMENT SEQUENCE OF THE PROJECT SUBJECT TO THE REVIEW OF THE CITY OF FOLSOM.

FOLSOM
H A R C H

N
SCALE: 1" = 100'

PRELIMINARY UTILITY PLAN
PARCEL 85A
Folsom, California February 19, 2021
MACKAY & SHIMPS

EXHIBIT B: PARCEL 85A - PRELIMINARY UTILITY PLAN

4 | APPROVAL PROCEDURES

These Guidelines include the Dignity Health Folsom Ranch Medical Center Site Plan, Design Guidelines specific to the Medical Center, Landscape Plan, and Signage Criteria. Each are submitted for approval by the City of Folsom Planning Commission. The Site Plan Guidelines, and specific design and landscaping standards contained within the Guidelines shall govern the review of building permit submissions for this project. One master Planned Development Permit shall be issued for the entire Folsom Ranch Medical Center project upon approval by the Planning Commission. Construction of the individual buildings will occur in five or more phases. A Conditional Use Permit from the City of Folsom will be required for the development and use of the proposed heliport. While design standards are included in the Guidelines, specific design review and approval of the hospital and each medical office building will be considered by the Planning Commission in order to determine consistency with these Guidelines, the FPASP and the Community Design Guidelines.

The medical office buildings will be designed to comply with the California Building Code. The ground floor levels of both medical office buildings are proposed to be designed to California's Office of Statewide Health Planning and Development (OSHPD) 3 requirements.

Hospital design and construction is subject to California Building Code requirements along with the review and approval of California's Office of Statewide Health Planning and Development (OSHPD). As acute care buildings, the hospital will be designed to comply with OSHPD 1 requirements. While the Guidelines are intended to be consistent with OSHPD requirements, it is possible that OSHPD may require design modifications as part of the State review process. In addition, hospital and medical office design evolves along with technology and community-based health needs, which may also affect the overall design requirements and site plan. Consistent with the FPASP Section 13.3 Administrative Procedures, Administrative Modifications and Amendments, the City of Folsom Community Development Department will have the authority to consider, and approve administratively, minor changes to the massing, design, site plan and/or Guidelines that remain consistent with the overall intent and purpose of the FPASP, the Community Design Guidelines, and these Guidelines. If the Community Development Director determines that Planning Commission review is appropriate, changes will be submitted to the Planning Commission for review.

Grading and Off site Infrastructure Improvements Approvals

Parcel 85A is subject to existing regulatory permits including the project's USACE permit, 401 water quality certification, Biological Opinion and Master Lake and Streambed Alteration Agreement. The project is in various stages of compliance with these permits and authorizations and all outstanding conditions will be met prior to construction.

With respect to cultural resources, the proposed development is subject to compliance with provisions in the EIR/EIS for the Folsom Plan Area and the 2013 "First Amended Programmatic Agreement between the US Army Corps of Engineers and the California Office of Historic Preservation regarding the Folsom Plan Area Specific Plan, Sacramento County, California." All pre-construction surveys, evaluations of significance, recording, and mitigation required by the City and US Army Corps of Engineers have been completed for the area subject to this application. A pre-construction compliance verification for the on-site and a portion of the off-site development area was issued by the Corps on March 5, 2020 and the State Historic Preservation Officer concurred on April 16, 2020. A pre-construction compliance verification for the balance of the off-site infrastructure is currently in review by the Corps and will be issued prior to the commencement of construction-related activities. The project will be subject to construction-related requirements that are specified in Chapter 3 - Parcel 85A Grading, Infrastructure and Off Site Improvements.

5 | PERMITTED USES

The City approved a Minor Administrative Modification (MAM) that affected several parcels (APNs 072-3190-030 and 072-3190-046), and which includes Parcel 85A, on March 17, 2020. The MAM shifted residential and commercial gross square footage (GSF) within multiple parcels in order to meet the maximum development intent of the properties involved. The resulting Transfer of Development Rights (TDR) has resulted in the following uses for Parcel 1:

Land Use	acres	GSF	Floor Area Ratio (FAR)
GC-RC	5.0	106,000	0.49
GC-GC	18.3	314,620	0.39
GC-IND/OP	5.0	109,380	0.50

Generally, the following medical services are likely to be provided:

- Acute-Care Hospital
- Medical Office Buildings
- Outpatient Clinics, including Urgent Care
- Free-standing Emergency Departments
- Ambulatory Surgery Centers
- Wellness related Clinics and Retail facilities
- Imaging center and mobile imaging and treatment
- Mobile Trailer Pad(s) for mobile prefabricated modular structures or temporary emergency management services

Each of the zoning categories identified above allows for “Laboratory-Medical,” “Health Care Facility,” “Medical Services – Major,” and “Medical Services – Minor.” The category of “Medical Services – Major” is further defined in the FPASP, and covers “services requiring in-patient hospitalization or other services that require acute medical attention.” These services will be provided in the Hospital complex identified on the Site Plan and consisting of 400,000 occupied square feet. “Medical Services – Minor” also is further defined in the FPASP, and covers “outpatient services, including but not limited to Lasik surgery offices, dentistry offices, same day clinics.” Services within this category will be provided in the Medical Office Buildings.

Land use for office is allowed in both the Folsom General Plan and the FPASP. The medical buildings fall under an office land use designation as General Commercial (GC). The maximum FAR is 0.5 in the

General Plan, while the FPASP maximum FAR for GC is 0.25. While permitting medical services land use, the FPASP did not envision a hospital building. The Folsom General Plan has a land use designation of Public and Quasi-Public Facility (PQP) which corresponds with the hospital building. PQP use is defined as supporting the needs of the community by providing health uses, and the FAR ranges from 0.2 up to a maximum of 1.0. The Folsom Ranch Medical Center provides significant public amenities and community benefit not only to the City of Folsom, but also to the surrounding region. In order to provide a full service hospital, a minimum FAR of .55 is requested for the project site to accommodate the grossing and support space anticipated for a full service hospital. This FAR increase will translate into appropriate building footprints with efficient program layouts and onsite surface parking with open areas for a positive contribution to the visual environment.

6 | DEVELOPMENT PARAMETERS

The Site Plan included with the PD Permit Application and these Guidelines identifies two Medical Office Buildings of approximately 130,000 occupied square feet; a Hospital complex consisting of 400,000 occupied square feet and a Central Plant building consisting of approximately 20,000 square feet.

The entire project site is 27.44 acres, with each building comprising the approximate acreage in square feet:

Building Summary

Phase	Building	Central Plant*	Occupied Square Footage
Phase 1	Medical Office Building	N/A	65,000
Phase 2	Hospital (100 Beds)	15,000*	160,000
Phase 3	Medical Office Building	N/A	65,000
Phase 4	Hospital Expansion (100 Beds)	N/A	120,000
Phase 5	Hospital Expansion (100 Beds)	5,000*	120,000
Total Area		20,000*	530,000

**Central Utility Plant areas are estimated and are not included in total development area*

Occupied square footage defined:

Occupied square footage is the actual space occupied and defined as area from inside wall to wall dimensions. Healthcare space is a specialized use that has well defined and code dictated requirements that are unlike other building uses and types. This specialized use requires core elements to be typically much larger than other building types. A larger percentage of project square footage is specialized square footage that is not occupied or dedicated to specific program use. A few examples are as follows: Circulation corridors are a specialized use difference in which healthcare has descriptive requirements. California Building Code (CBC) Section 1224.4.7 requires a min of 8' minimum "Clear" width corridors throughout the hospital facility to allow gurney traffic, supply traffic, and staff movement without obstructions. Clear space is also required by CBC around beds to allow staff movement and patient access that ranges from 3' minimum clear to the edge of beds to 5'-0" minimum clear at stretchers. Storage requirements are another key driver per CBC requirements, with examples being a minimum of 20 net square feet of general storage per bed, exclusive of other required specialized storage spaces based on Acute Care Services provided. Specialized storage is required in each department with various requirements, but a rough estimate would require storage space at over 200 sq ft per bed. These

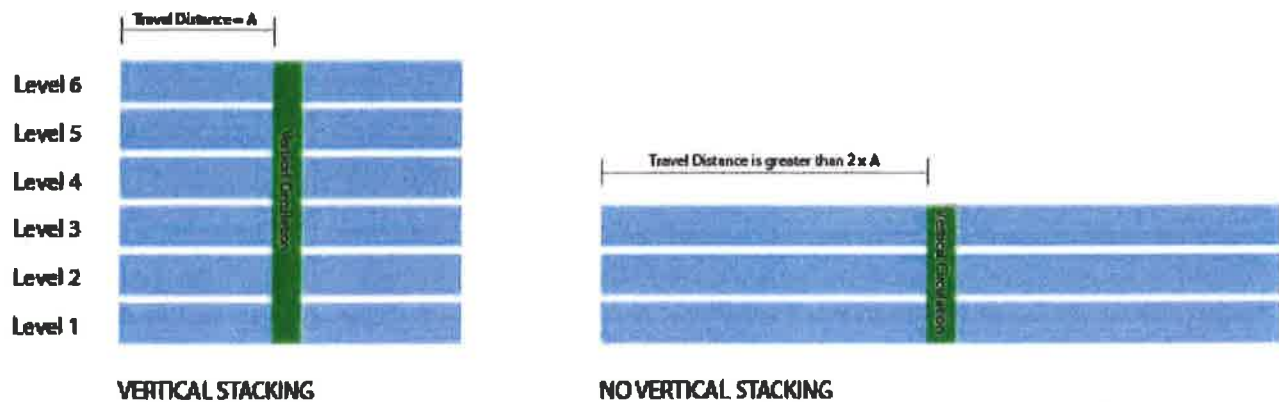
examples are a handful of key drivers of spaces that are not occupied uses. Occupied square footage does not include common areas of a healthcare building such as restrooms, stairwells, mechanical shafts, storage rooms, utility rooms, mechanical rooms, electrical rooms, and shared hallways or corridors.

Hospital Planning Drivers

Hospitals have a high level of complexity that begins with the design of a circulation system that connects a vast network of program spaces. The key types of space are inpatient care units, outpatient care, diagnostic/treatment, administrative and support services and public spaces. These spaces are organized based on key functional relationships and factors:

- Efficient travel distances for patients, visitors, and staff
- Stacking of inpatient units that have limited connections with each other but need convenient access to diagnostic and treatment services
- Emergency needs to be adjacent to Diagnostic/Imaging but can also be linked to Surgery and ICU, which should be adjacent to each other
- Access to natural light and views with connection to outdoors
- Utility services benefit from having the shortest and most direct distribution
- High volume outpatient services at medical office buildings separated from hospital inpatient services

One concept for building organization is the stacking diagram. The example below shows the program volume spread across 6 floors by Vertical Stacking versus that same volume spread over three floors by No Vertical Stacking. Consolidating program over multiple levels instead of spreading program spaces over available building footprint results in excessive travel distances. Short travel distances are preferred and critical for patient, visitor, and staff flow, not to mention material flow. In addition to extended travel distances in the No Vertical Stacking, a larger footprint generates inefficiencies in adjacencies and loss of views and access to natural light. A taller more compact building massing is more efficient than a sprawling facility. As a result, our proposed hospital will be a taller volume in a more compact footprint on the Medical Center site.



Another planning principle for the Medical Center is to preserve as much open space as possible in order to highlight a wayfinding hierarchy of buildings, with the tallest building the hospital and its inpatient services, flanked by the smaller medical office buildings providing outpatient care. The proposed site plan supports this approach and is designed for direct and intuitive wayfinding for patients, visitors and staff. The larger hospital is the focal point and centered in the site, between two smaller medical office buildings. The diagnostic/treatment services of the hospital are connected via sidewalks to both medical office buildings and the hospital is equidistant from each.

These planning principles will guide the development of the medical center. While the medical office buildings are proposed to be a maximum of three levels, future programming studies will ultimately determine the number of levels and corresponding building height of the hospital. Variables influencing the design are required and new service needs, new technologies, anticipated volume, operational and staffing assumptions, and building system decisions. Hospital planning and development is complex, as hospitals have longer lifespans than other building types and more intensity of use. An open system of planning which can fine tune to variables supports the goal of a high quality hospital of the future. Design flexibility is a request in these planning documents.

Building Heights

- Medical Office Buildings; 3 story, Sixty (60) feet maximum*
- Hospital Building; 3-6 story, One Hundred and Twenty (120) feet maximum*
- Central Utility Plant; 1 story, Thirty-five (35) feet maximum*

** Building Heights exclude roof top mechanical screen walls. Maximum height of mechanical screen walls is 15' above the finished roof.*

Building Setback Requirements (as indicated on the Exhibit C - Preliminary Site Plan)

- Front Setback (Alder Creek Parkway): 30 feet (18 foot landscape corridor and pedestrian access easement required)
- Rear Setback (US Highway 50 On Ramp): 25 feet (25 foot landscape easement required)
- Side Setback (East Bidwell): 20 feet (20 foot landscape and pedestrian easement required)
- Side Setback (McCarthy Way): 18 feet (18 foot landscape corridor and pedestrian access easement required)
- Interior Setbacks (building to building) - 60' minimum unless buildings are connected (10 feet per story unless buildings are connected; 0 feet at connected buildings; all building setbacks to comply with California Building Code requirements)

Surface Parking Requirements

- Medical Office Buildings; three spaces per 1,000 square feet of gross area
- Hospital Building; two spaces per hospital bed
- Accessible handicapped parking spaces to be provided per Folsom Municipal Code
- Loading/Unloading spaces will be provided at Hospital, designed to Dignity Health's just in time delivery program that will restrict loading traffic to off peak hours.
- Dimensions of parking spaces will meet Folsom Municipal Code requirements. Dimensions of ADA accessible car and van parking spaces shall comply with California Building Code requirements.
- Typical angled parking stalls measure 9'x19'. Typical drive isles measure 25' wide.
- A continuous 6" high raised concrete curb shall be provided along all landscape areas abutting parking or drive areas.
- Landscape island spacing and distribution of shade trees shall comply with Folsom Plan Area Specific Plan (FPASP) and Community Design Guidelines.
- Minimize cross-slopes across entire site and especially in areas of concentrated traffic in order to maximize accessibility and pedestrian safety.

Bicycle Parking

For Office land use, Folsom Municipal Code requires 5 bicycle parking spaces per 25 required vehicle parking spaces plus one additional bicycle parking space for every 10 additional vehicle parking spaces required or portion thereof. The maximum number of bicycle parking spaces required is 20. 20 bike parking spaces are provided at the entrance of each Medical Office Building. In addition to the Municipal Code required bicycle parking spaces, the Medical Center will provide 16 spaces at the Hospital building.

- Bicycle parking facilities shall be installed in a manner which allows adequate spacing for access to the bicycle and the locking device when the facilities are occupied. General space allowances shall include a 2-foot width and a 6-foot length per bicycle and a 5-foot maneuvering space behind the bicycle. The facilities shall be located on a hard, dust free surface, preferably asphalt or concrete slab.
- Bicycle parking shall consist of at least a stationary bicycle rack, typically a concrete slab or vertical metal bar, where the bicyclist supplies a padlock and chain or cable to secure the bicycle to a stationary object.

Sustainable Design

Energy efficiency shall be addressed in building design, in compliance with the FPASP and California Building Codes. In addition, Dignity Health has developed its own renewable energy goals for its facilities since 2010. This commitment is reflected in targeting below code required energy efficiency in the design and construction of new acute care buildings.

In addition to sustainable design for buildings, the site will have a solar panel array for covered employee parking. Electric Vehicle Parking Spaces and Electric Vehicle Charging Stations per the California Green Code will be provided in close proximity to both medical office buildings and the Hospital building. Cool Paving will be provided at the site in specific locations.

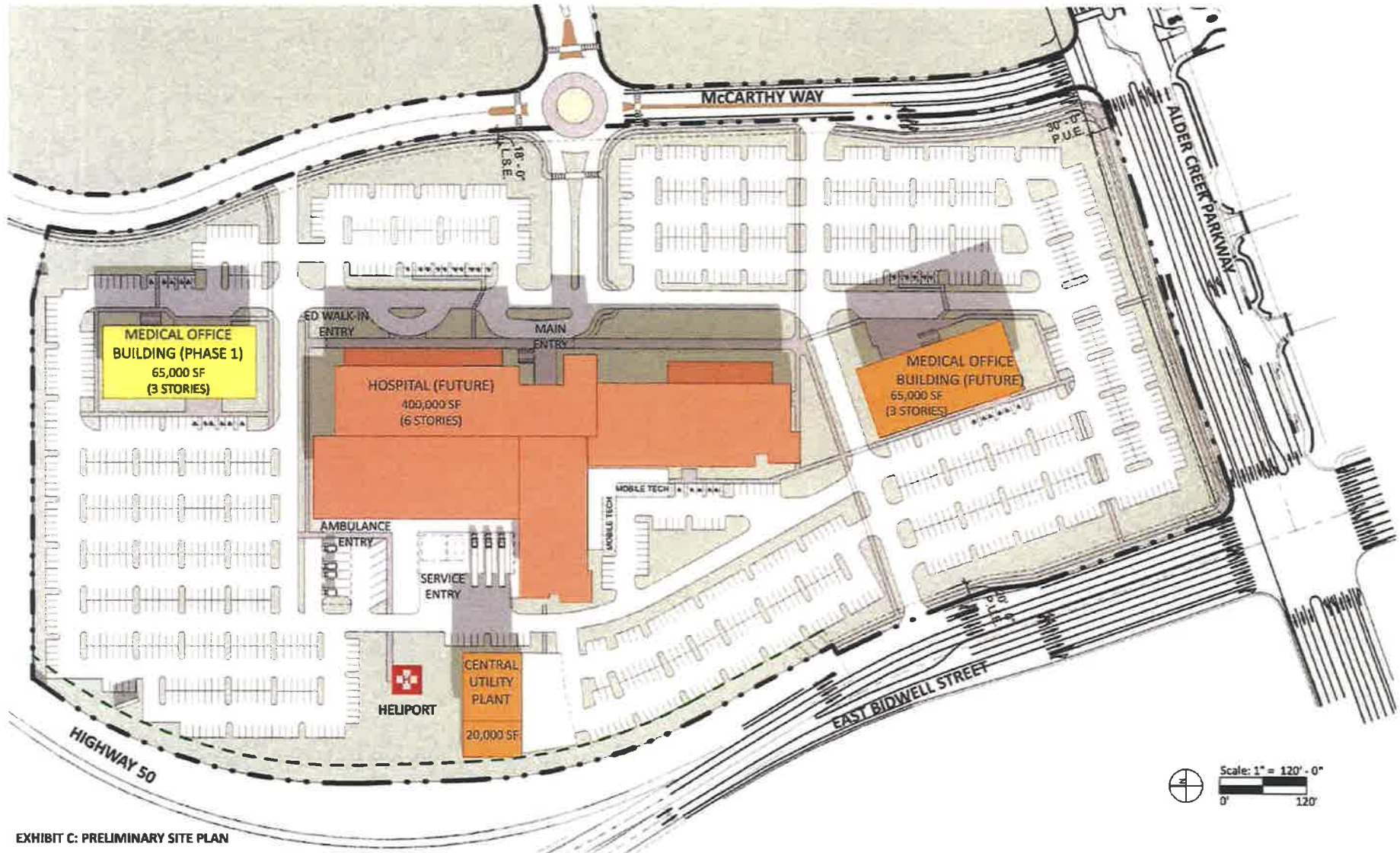


EXHIBIT C: PRELIMINARY SITE PLAN

7 | SITE DEVELOPMENT GUIDELINES

Site development guidelines will help to establish the type of experiences that visitors, employees, and patients will have within the Medical Center. Clarity in site planning can lay the groundwork for a medical facility that contributes to positive experiences and improved patient outcomes. This project aims to achieve the following site planning goals:

- This campus should define its place within the community. The outward sign to the community should clearly represent that this is a welcoming place for healing.
- Provide an environment that promotes patient safety, accessibility, and is easy to navigate for pedestrians, vehicles, and emergency vehicles.
- Create open spaces and safe, comfortable pedestrian pathways that provide outdoor places for family members and staff to have moments of respite throughout the day and evening.
- Clearly identify building entrances and primary pedestrian pathways.
- Integrate the project with the surrounding development to ensure that pathways and streets are coordinated.

Project Site Access

Three site access options were presented for consideration in the environmental analysis and project approval: (1) signalized intersection at Alder Creek Parkway and McCarthy Way; (2) roundabout at Alder Creek Parkway and McCarthy Way; and (3) unsignalized left turn at Alder Creek Parkway and McCarthy Way, with possible future main site access via a left turn from Alder Creek Parkway to Westwood Drive, followed by a left turn from Westwood Drive to Mercy Drive.

Dignity Health's preferred access to the project site is via a left turn at Alder Creek Parkway and McCarthy Way. Left turn access into the Folsom Ranch Medical Center at the intersection of Alder Creek Parkway and McCarthy Way is a priority for Dignity Health to facilitate non-emergency patient and visitor access as well as employee access. The proposed signal spacing (i.e., 700 feet) to facilitate this left turn movement would not meet the City's minimum spacing standards. Traffic analysis indicated that a signal is not needed until, at the earliest, construction of the last medical building. Although the Traffic Impact Analysis completed for the Project did not identify significant environmental impacts, due to the early stages of development of Folsom Ranch, the City will only allow future consideration of the traffic signal after completion of an updated traffic analysis, with details identified in the Conditions of Approval and the Development Agreement Amendment.

Through continued work on the site design, Dignity Health also believes (based on technical analysis)

that a roundabout at the intersection of McCarthy Way and Mercy Drive will facilitate traffic movements within the greater Parcel 85A. This type of intersection will increase pedestrian and vehicle safety and reduce vehicle emissions. The roundabout would serve as the main gateway entrance to the Folsom Ranch Medical Center campus. The site plan includes this roundabout.

Emergency vehicle access

Emergency vehicle access is provided to the site from all project site driveways in either direction of traffic. From southbound East Bidwell, emergency vehicle only access will be provided into the driveway. A half signal at this location will be installed for southbound left turn movement by emergency vehicles-only.

Wayfinding guidelines

Wayfinding is an essential component in every aspect of healthcare environmental design. Healthcare facilities are large scale, complex environments. Visitors to the Medical Center have varied physical abilities and cognitive/emotional states, and often feel rushed. Comprehensive wayfinding design influences positive patient and visitor outcomes by reducing stress and minimizing visitor disorientation. Dignity Health's signage program is a comprehensive design that begins with the patient or visitor leaving their home, and their arrival to their Medical Center destination.

Providing a safe passage to the Medical Center is a key element of providing a safe environment. During route navigation, visitors should be able to cognitively map out the Medical Center location and its coordinates, whether from Highway 50 or from streets of surrounding residential communities. Wayfinding via freestanding sign - pylon, then distant read skyline and skyline building signage will be repetitive in order to be effective for navigation. Key decision points occur at intersections and chokepoints. Building signage will occur frequently to provide reassurance to patients and visitors as they navigate, making reorientation seamless and easy. Continuous visual connection is desired for clear and intuitive wayfinding to the Medical Center. Further detail on the wayfinding signage program may be found in Section 11 Sign Criteria.

Site Design Guidelines

- Vehicular and pedestrian circulation and building placements, including the delineation of emergency, service, and public traffic flows, on the site shall generally conform with the approved Folsom Medical Center Preliminary Site Plan presented in this document as Exhibit A.
- Provisions for ADA access shall be designated consistent with Federal and State Americans with Disabilities Act (ADA) and California Building Code requirements. ADA accessible routes must be

provided from public sidewalks to building entrances.

Campus Edge Considerations

- Ensure patient safety by establishing simple wayfinding at the campus edge. Emergency, patient and visitor, and service traffic flows should be well planned and clearly identified with wayfinding signage.
- The campus edge design defines an institution's place in the community. Landscaping should be considerate of the local environment and increase connections to nature. Use native, contextually appropriate plants at campus edges. See required landscaping materials in the FPASP or the Community Design Guidelines.

Pedestrian Regulations

- Safe pedestrian pathways, traffic slowing measures at building entries and around crosswalks.
- Pedestrian crosswalks and drives shall be defined through use of enhanced paving.
- Shade elements including canopies, trellises, and trees should be thoughtfully placed along pathways, building entrances, and areas for respite.

Patient and Visitor Experience

- Provide adequate screening at back of house functions to maintain a calming visitor experience.

Loading, Storage and Utility Locations

- Loading areas shall be provided on site and shall be design to Dignity Health's loading and unloading operations requirements.
- Loading areas shall be thoughtfully planned and obscured from public view through the use of landscaping, earthwork, and/or screen walls.
- Material handling, storage of medical waste, above ground storage of water, fuel, bulk oxygen, medical gas bottles, and unsightly day to day processing of supplies should be thoughtfully planned and obscured from public view through the use of landscaping, earthwork, and/or screen walls.
- Freestanding screen walls, retaining walls and fences shall be designed to have consistent materials, styles and colors to complement the buildings on the campus.
- Per California Building Code, the hospital is required to have an onsite emergency generator for

standby power and onsite fuel storage to maintain generators for a minimum 72 hour duration. The generator location will be at the Central Utility Plant, distanced away from the hospital building.

Site Furniture

- Site furniture should be safe to use and ADA accessible.
- Shall have appropriate level of protection from vandalism (graffiti), theft, and weather resistance.
- Shall be easy to maintain.
- Shall be inviting to use and comfortable.
- Shall be complementary to the building designs.
- Shall be provided in adequate numbers to serve the expected use.
- Newspaper/Periodical Vending machines shall be limited in number. Shall be provided in obscured yet convenient locations.

Future Building Pads

- Rough graded building pads for later phases of the Medical Center should be treated with an un-irrigated hydro-seed application of annual grasses to minimize erosion.

Mobile Prefabricated Modular Structures

- Modular structures are proposed to the west of the Hospital, and will be screened from adjacent roadways with evergreen screening plantings.

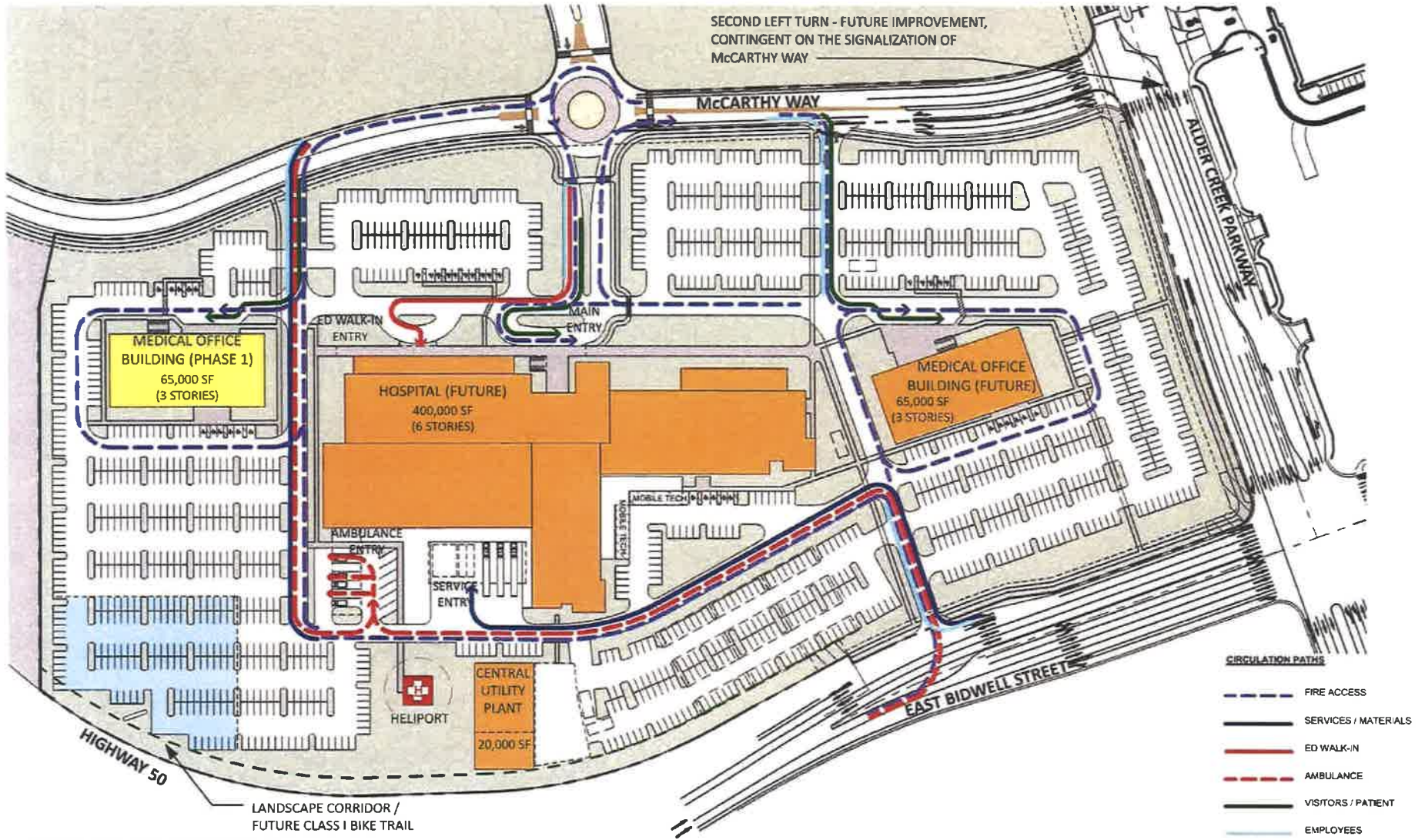


EXHIBIT D: SITE CIRCULATION FLOW PLAN

Heliport

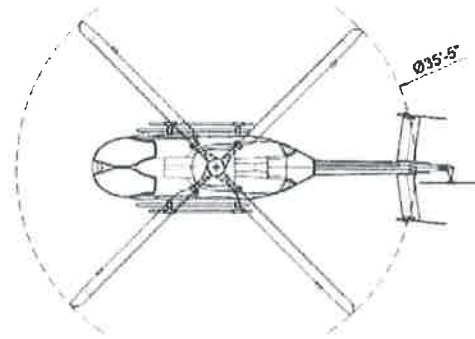
This facility is proposing heliport access for emergency patient transport to the hospital for emergency care, or to transport patients to other hospitals where a higher level of emergency care is available. The heliport will be designed and constructed in the future phases of the hospital. The hospital heliport will be located on the west side of the project site, near the Hospital's Emergency Department Ambulance Entrance. This location will be furthest away from surrounding residential development. The heliport will have a landing area of broom finish concrete and will have a connecting concrete pathway of minimum 8' width. Sufficient airspace will be provided for takeoff and landing. Regulations and permitting processes associated with the special use heliport must be in accordance with the California Department of Transportation Aviation Division (Caltrans/Aviation) and the Federal Aviation Administration (FAA) and the Sacramento County Department of Airports. Site planning will also need to accommodate the Final Approach and Takeoff Area (FATO) as established through a coordinated effort with the FAA and Caltrans. Final site plan design shall incorporate required lighting requirements for safe landing and departure of helicopters. Exhibit B depicts the conceptual standards of the heliport landing and takeoff design and placement on the site plan. Exhibit C depicts the proposed helicopter basis of design.



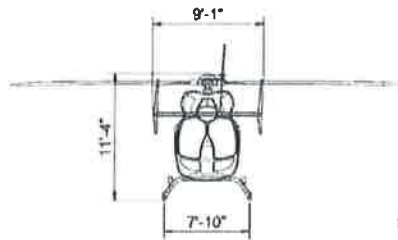
Exhibit B: Conceptual FATO Clearances diagram

DESIGN HELICOPTER: Airbus H145

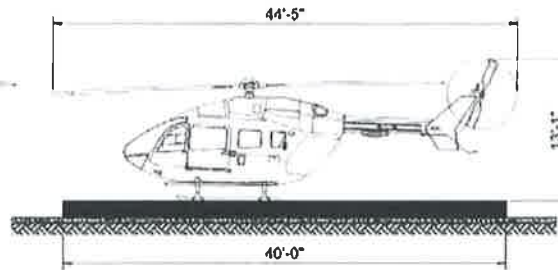
Maximum Operating Weight : 8,167 lbs
 Overall Length : 44'-5"
 Main Rotor Diameter : 35'-5"
 Undercarriage Width : 7'-10"
 Main Rotor Blades : 4



Airbus H145 top view
 Scale : 1/8" = 1'-0"



Airbus H145 front view
 Scale : 1/8" = 1'-0"



Airbus H145 and heliport section profile view
 Scale : 1/8" = 1'-0"

Exhibit C: Helicopter Design Parameters**Site Circulation**

Circulation is a key element in the planned development of the Medical Center, as it is located along US Highway 50 and at the northeast corner of two significant signature corridors in East Bidwell and Alder Creek Parkway.

A comprehensive public transit plan is one of the main FPASP planning principles. The transit corridor is along Alder Creek Parkway, with future transit bus stops at key locations in the Plan Area. Public transportation to the Medical Center in the form of a future on-site or site adjacent bus stop will be a lasting amenity for patients, visitors, and staff. The intent of the FPASP is to support a transit corridor connecting light rail stations and bus routes, and the expectation is that a bus network along Alder Creek Parkway will be provided as part of the FPASP proposed public transit plan. Future bus stops are shown eastbound and westbound on Alder Creek Parkway.

Water Supply and Infrastructure

The City entered a Water Supply Agreement with landowners to supply water to the Plan Area to meet a build-out demand estimated at 5,600-acre feet annually. Components of the water system

include extension of offsite water transmission mains, storage tanks, booster pump stations and distribution mains. A Hydraulic Analysis Technical Memorandum for both intermediate and buildout scenarios of impacts to the City of Folsom's water infrastructure has been completed with review by the Environmental and Water Resources Department.

Utility studies for Folsom Ranch Medical Center have validated a required minimum water pressure of 80 psi at the Hospital. The Folsom Ranch Medical Center design needs for higher pressure water is the basis for the design consultant team's recommendation and Dignity Health's proposal to connect to Zone 4 of the Folsom Plan Area (FPA) rather than Zone 3. Exhibit E shows the proposed Folsom Water Plan adding Folsom Ranch Medical Center to Zone 4. This Zone 4 connection is the buildout scenario.

The hydraulic analysis results of the Zone 4 buildout scenario complies with the system performance criteria, and the City has agreed to the Zone 4 water connection of the Folsom Ranch Medical Center, also known as Parcel 1 of Parcel 85A. The Medical Center site would have two points of connection to Zone 4 infrastructure.

Timing for construction of the Zone 4 infrastructure presently is under review between the City and other owners in Folsom Ranch. Pending completion of Zone 4 infrastructure and in light of the fact that the higher psi is not needed for early phases of the Folsom Ranch Medical Center (i.e., the two medical office buildings), the City proposed that initial water needs of the project site could be fulfilled by the Zone 4 infrastructure, which is currently supplied by the existing Zone 5 tank. Based on the proposed phasing provided by the consultant team, the first needs will be fulfilled by the Zone 5 tank as an interim supply. This Zone 5 tank interim connection is the intermediate scenario. By the time the Phase 1 (100 Beds) hospital is constructed, the Zone 4 Tank will be in service to meet the projected water needs of the hospital.

The analysis conclusion is that the Zone 5 tank has interim capacity to serve identified Folsom Specific Plan Area sites, including the Project site, until the Zone 4 tank is designed and constructed. Upon completion of the Zone 4 tank, Zone 4 infrastructure will cut over from the Zone 5 tank to the Zone 4 tank. The analysis established that the Zone 4 Tank should be constructed at the end of 2024, when demand in the FPA Zones 4, 5 and 6 exceeds 1 MG. The analysis results of the intermediate scenario complies with the system performance criteria. The City has advised that this technical memorandum will be used to update any references to water infrastructure in the Specific Plan and that no Specific Plan Amendment is required as part of the entitlement package related to this issue.

The 2014 Folsom Plan Area Water System Master Plan accommodated the water demand of the Medical Center. (See attachment to Environmental Information Sheet). The Medical Center's proposed square footage is designed to the available water supply, as well as the water supply allocated in the MAM.

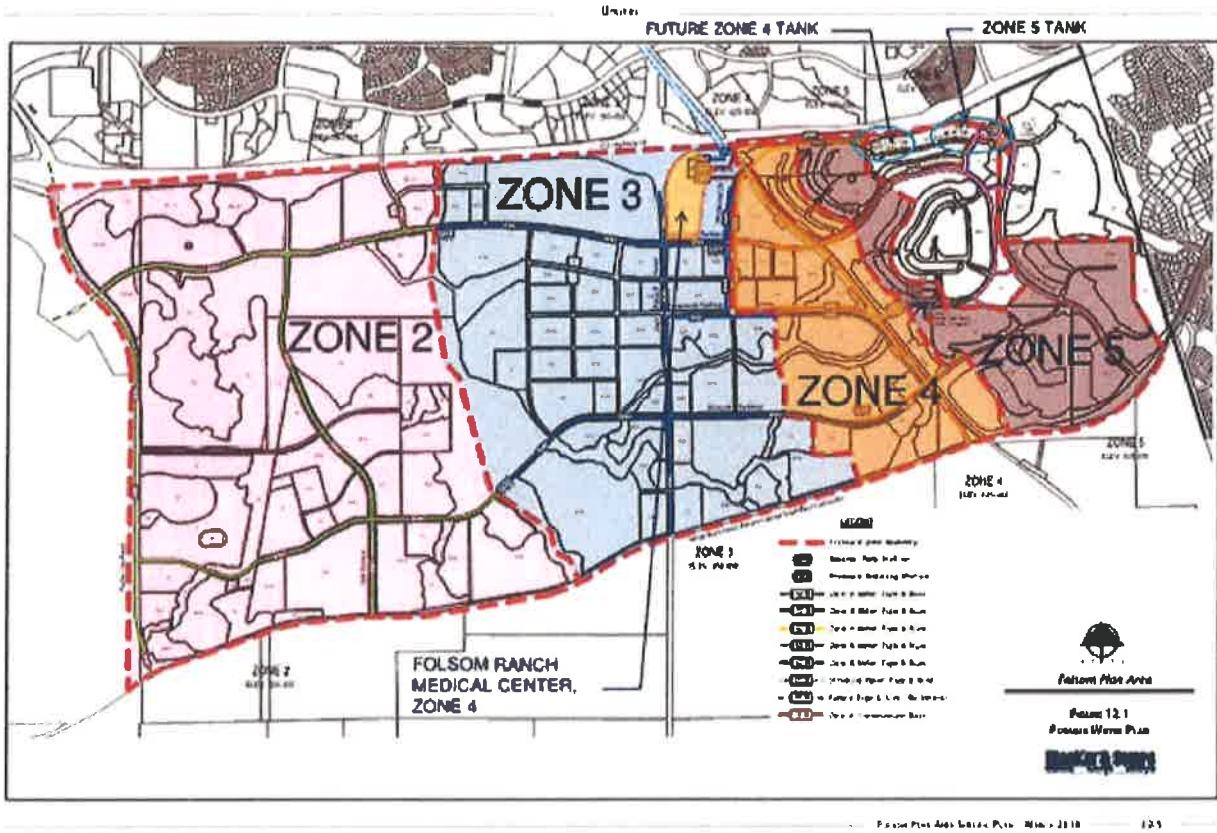


Exhibit E: Folsom Water Plan with Folsom Ranch Medical Center added to Zone 4

8 | LANDSCAPE GUIDELINES

Landscape Intent

Landscape design plays an essential role in supporting Folsom Ranch Medical Center's overall site planning goals of safe and easy navigation, comfortable pedestrian circulation, and a positive user experience for patients, staff, and visitors. Gardens provide a restorative environment from mental and emotional fatigue. A Healing Garden will be a featured site amenity. Fragrant plants, cool and calming colors, naturalistic massings and foliage with soft texture and movement will be the showcase of this Garden. Additionally, the landscape design of this project can support the Folsom Plan Area Specific Plan's principles of well-integrated sites, walkable neighborhoods, and sustainable design. Pollinator friendly plants that attract bees, hummingbirds, and butterflies will provide a habitat that will sustain healthy communities of beneficial insects and pollinators. The outdoor space design is an important component of providing a healing environment. With these goals in mind, the intent of landscape design for this project is to create user-friendly, functional, intuitive, human-scaled spaces that provide shade, biodiversity, seasonal interest, seating areas, healing spaces, and the overall calming and therapeutic effect that living landscapes can offer.

This chapter addresses guidelines for:

- Hardscape Materials
- Irrigation System
- Landscape Planting
- Plant Palettes

Landscape design for the Folsom Ranch Medical Center shall adhere to the guidelines in this chapter, and shall meet all applicable requirements in the FPASP and Community Design Guidelines.





EXHIBIT F: OVERALL CONCEPTUAL LANDSCAPE PLAN



EXHIBIT G: CONCEPTUAL PHASE 1 LANDSCAPE PLAN

TREE SCHEDULE










TREES	CODE	QTY	BOTANICAL / COMMON NAME	SIZE
	ACE BLO	6	<i>Acer palmatum</i> 'Bloodgood' / Bloodgood Japanese Maple	15 gal
	ACE RUB	44	<i>Acer rubrum</i> 'October Glory' / Red Maple	24"box
	ARB TRE	8	<i>Arbutus unedo</i> / Strawberry Tree Multi-Trunk	15 gal
	CAR FRA	18	<i>Carpinus betulus</i> 'Franz Fontaine' / Hornbeam	15 gal
	CER WES	18	<i>Cercis occidentalis</i> / Western Redbud Multi-trunk	15 gal
	CIN CAM	28	<i>Cinnamomum camphora</i> / Camphor Tree	15 gal
	KOE PAN	41	<i>Koelreuteria paniculata</i> / Golden Rain Tree	15 gal
	LAG TUS	50	<i>Lagerstroemia</i> x 'Tuscarora' / Pink Crape Myrtle	15 gal
	LAU SAR	9	<i>Laurus</i> x 'Saratoga' / Saratoga Laurel	15 gal
	OLE SWA	22	<i>Olea europaea</i> 'Swan Hill' / Fruitless Olive	24"box
	PIN CAL	43	<i>Pinus brutia</i> / Calabrian Pine	15 gal
	PIS KEI	55	<i>Pistacia chinensis</i> 'Keith Davey' / Chinese Pistache	24"box
	POD MAC	12	<i>Podocarpus macrophyllus</i> / Yew Pine	24"box
	FRU VES	8	<i>Prunus</i> cer. 'Krauter Vesuvius' / Purple Leaf Plum	15 gal
	QUE DOU	12	<i>Quercus douglasii</i> / Blue Oak	24"box
	QUE LOB	19	<i>Quercus lobata</i> / Valley Oak	36" Box
	QUE WIS	72	<i>Quercus wislizenii</i> / Interior Live Oak	24"box
	THU EME	18	<i>Thuja occidentalis</i> 'Emerald' / Dwarf Arborvitae	15 gal
	ULM ALL	66	<i>Ulmus parvifolia</i> 'Allee' / Allee Lacebark Elm	15 gal
	ZEL VIL	30	<i>Zelkova serrata</i> 'Village Green' / Sawleaf Zelkova	15 gal

EXHIBIT H: PROPOSED TREE SCHEDULE

Hardscape Materials Guidelines

The use of durable and simple materials helps to create a contemporary and cohesive user experience across the site. Pedestrian sidewalks and paths will be simple gray concrete flat-work. Enhanced areas of cast concrete pavers and/or integrally colored concrete shall be considered for building entries, pathway nodes, and therapeutic garden spaces to enhance the visitor experience and help self-navigation. A limited use of stabilized decomposed granite pavement in low-traffic areas should be considered to create secondary walking paths or patient relaxation areas. The overall intent of the materials selection should be towards creating calming, unfussy, easy-to-navigate pedestrian spaces.

Irrigation System Guidelines

The irrigation system shall be fully automatic and shall be designed for the most efficient use of water in the landscape while maintaining plant health. To that end, design and management of the irrigation system shall be in compliance with California's Model Water Efficient Landscape Ordinance (MWELO) and the City of Folsom's current irrigation standards. Specific measures include:

- Internet-connected 'Smart' irrigation controller(s) with weather sensors that automatically adjust watering times based on seasonal evapotranspiration data.
- Flow sensor and master valve assembly to monitor water use and shut off system in case of leaks.
- In-line subsurface drip emitter lines to irrigate all shrub/ground cover plantings.
- At-grade bubblers (2 bubblers per tree) at all trees. Trees grouped on their own valves, separate from understory plantings.
- Irrigation hydrozones organized based on similar plant water requirements and solar exposure.

The entire system shall utilize 'purple pipe' components to support future conversion to a reclaimed water source that the City of Folsom may bring to Folsom Ranch.

Landscape Planting Guidelines

Landscape planting shall be designed with sustainability and ease of maintenance in mind, and should complement the overall architectural style of Folsom Ranch Medical Center, specifically:

- All plant material shall be California-adapted, long-lived, non-toxic, and non-invasive. California-native plant species should be incorporated where appropriate.
- All plant material shall have a Very Low Water Use, Low Water Use, or Medium Water Use rating according to the WUCOLS rating system.

- Plants shall be spaced with adequate room to grow to their full size without requiring shearing.
- Perennial plants may be used sparingly, in accent plantings at entries and therapeutic garden spaces. There shall be no annual color plantings.
- Mowed lawn shall be limited to small areas for patient/visitor use for therapeutic purposes, and shall not exceed 5% of the total landscaped area.
- Street trees on the frontages along East Bidwell Street and Alder Creek Parkway shall be consistent with the streetscape conditions described in the Folsom Plan Area Specific Plan (March 2018; Figures 7.3 and 7.11)
- Evergreen screening trees shall be planted along the northwestern portion of the parcel that borders US Highway 50 right-of-way.
- Trees shall be interspersed throughout the parking lot areas such that in 15 years, 40% of the parking lots will be shaded at midday.
- Planting that will attract humming birds and butterflies is encouraged and can create unexpected positive distractions for patients and visitors.

Additionally, per Folsom Municipal Code Section 17.57.070, the following shall be applied to landscaping at all parking lots:

- Planted and irrigated landscape areas equal to at least 5 percent of the total parking area shall be provided to create the visual and physical separation necessary to reduce the traffic hazards between pedestrians and vehicles. All landscaping areas shall be designed so that plant materials are protected from vehicle damage or encroachment.
- Trees shall be interspersed throughout the parking area so that in 15 years, 40 percent of the parking lot will be in shade at high noon, assuming the sun directly overhead. The percentage of area required to be shaded shall be based on aboveground, uncovered parking area. Trees shall be minimum 15-gallon size at planting.

Plant Palettes

On the following pages are lists of suggested tree and understory plant species. Low water use perennials should be added to the planting design in high-impact areas such as building entries and therapeutic garden spaces. Other species beyond this list may be incorporated into the design if they support Folsom Ranch Medical Center's overall design principles and adhere to the guidelines herein.

Tree Palette

Common Name / Botanical Name

- Red Maple / *Acer rubrum* `October Glory`
- Strawberry Tree Multi-Trunk / *Arbutus unedo*
- Hornbeam / *Carpinus betulus* `Franz Fontaine`
- Western Redbud Multi-trunk / *Cercis occidentalis*
- Camphor Tree / *Cinnamomum camphora*
- Golden Rain Tree / *Koelreuteria paniculata*
- Pink Crape Myrtle / *Lagerstroemia* x `Tuscarora`
- Saratoga Laurel / *Laurus* x `Saratoga`
- Fruitless Olive / *Olea europaea* `Swan Hill`
- Calabrian Pine / *Pinus brutia*
- Chinese Pistache / *Pistacia chinensis* `Keith Davey`
- Yew Pine / *Podocarpus macrophyllus*
- Purple Leaf Plum / *Prunus cer.* `Krauter Vesuvius`
- Blue Oak / *Quercus douglasii*
- Valley Oak / *Quercus lobata*
- Interior Live Oak / *Quercus wislizenii*
- Allee Lacebark Elm / *Ulmus parvifolia* `Allee`
- Sawleaf Zelkova / *Zelkova serrata* `Village Green`

Understory Palette

Common Name / Botanical Name

Shrubs

- Century Plant Agave / *Agave americana*
- Dwarf Bottle Brush / *Callistemon citrinus* `Little John`
- Camellia / *Camellia* spp.
- Bush Anemone / *Carpenteria californica* `Elizabeth`
- California Wild Lilac / *Ceanothus* spp.
- Rockrose / *Cistus* spp.
- Pineapple Guava / *Feijoa sellowiana*
- Flannel Bush / *Fremontodendron* x `California Glory`
- Veitch Jasmine / *Gardenia jasminoides* `Veitchii`
- Noel Grevillea / *Grevillea* x `Noellii`

- Chinese Fringeflower / *Loropetalum spp.*
- Red-Tip Photinia / *Photinia fraserii*
- California Coffeeberry / *Rhamnus c. 'Mound San Bruno'*
- Yedda Hawthorn / *Rhaphiolepis umb. 'Southern Moon'*
- Rosemary / *Rosmarinus spp.*
- Australian Bluebell / *Sollya heterophylla*
- Coast Rosemary / *Westringia fruticosa 'Wynabbie Gem'*

Ornamental Grasses and Grasslike Plants

- Feather Reed Grass / *Calamagrostis x a. 'Karl Foerster'*
- Berkeley Sedge / *Carex tumulicola*
- Prairie Fire Sedge / *Carex testacea 'Prairie Fire'*
- Flax Lily / *Dianella revoluta 'Clarity Blue'*
- Fortnight Lily / *Dietes vegeta*
- Blue Lyme Grass / *Leymus condensatus 'Canyon Prince'*
- Pink Muhly / *Muhlenbergia capillaris 'Regal Mist'*
- Deer Grass / *Muhlenbergia rigens*
- Red Bunny Tails Fountain Grass / *Pennisetum m. 'Red Bunny Tails'*
- New Zealand Flax / *Phormium spp.*

Groundcovers and Vines

- Groundcover Manzanita / *Arctostaphylos 'Emerald Carpet'*
- Bank Catclaw / *Acacia redolens 'Lowboy'*
- Rockrose / *Cistus salviifolius 'Prostratus'*
- Violet Trumpet Vine / *Clytostoma callistegioides*
- Cotoneaster / *Cotoneaster dammeri 'Lowfast'*
- Creeping Fig / *Ficus pumila*
- Creeping Mahonia / *Mahonia repens*
- Dwarf Heavenly Bamboo / *Nandina 'Firepower'*
- Creeping Rosemary / *Rosmarinus 'Huntington Carpet'*
- Creeping Snowberry / *Symphoricarpos mollis*
- Star Jasmine / *Trachelospermum jasminoides*

Healing Garden Trees and Plants

- Alpine Strawberry / *Fragaria vesca*
- Prairie Dropseed / *Sporobolus heterolepsi*
- Gardenia / *Gardenia jasminoides*
- Lavender / *Lavandula*
- Rosemary / *Salvia rosmarinus*
- Lilac / *Syringa*
- Heliotrope / *Heliotropium*

9 | ARCHITECTURAL GUIDELINES

Design Intent

The architecture that makes up the Folsom Ranch Medical Center will be a key contributor to positive patient experiences and improved outcomes. The aesthetic of the campus buildings should instill confidence in the exceptional level of care and dedication to professionalism that medical center patrons will experience during their stay at the hospital or medical office buildings. Building designs shall be clean, comfortable, and approachable. Buildings shall be well-proportioned, constructed of high-quality materials, and should demonstrate an attention to care and craftsmanship. Primary public entrances shall be easily identified and highly visible to assist in wayfinding. Building form shall distinguish main public entries versus emergency and staff entrances. Building design and landscaping should be well coordinated in order to leverage the healing aspects of natural environments. As good stewards of our environment and our communities, green building principles will be an emphasis of the architectural design.

Massing, Form, and Entrances

The combination of massing and form is a critical component of how buildings are perceived and experienced. Buildings on this campus should exhibit a sound understanding of massing including a base, middle, and top massing, the human scale, and the macro scale. Hierarchy and rhythm should be used to create building forms that are cohesive, balanced, and approachable.

Articulation and naturally cast shadow are important aspects of building facade design that can animate and bring visual interest to built structures. Articulation is expressed through the push and pull of building surfaces, combination of complementary materials, and thoughtful placement of reveals. Overhangs, awnings, architectural integrated shading devices, and recesses will create shadows and protect the building and occupants from heat gain.

Main entrances to buildings or key departments within the hospital are opportunities to create additional visual interest in the building forms. They should be clearly delineated such that visitors are not confused with wayfinding. In terms of hierarchy, the main entrance of the hospital should be distinct and most clearly defined.

Building Materials, Colors, and Textures

Appropriate selection of materials, colors, and textures will ensure that the medical campus fits within and strengthens the fabric of the larger Folsom community. Exterior finish materials and colors shall be selected according to the following criteria:

- The exterior building materials shall form a complimentary palette of textures and colors. Warm-tone, approachable materials and colors shall establish the primary palette. Natural stone of hewn, rough cut, flame honed or polished texture; integrally colored synthetic plaster, textured finish; pre-finished metal panels of composite or plate aluminum construction shall constitute the primary exterior wall finishes and accent materials.
- High quality, emphasis on natural materials that demonstrate professional craftsmanship.
- Vandal resistance.
- Contextual response to surrounding architecture.
- Long term durability and ease of maintenance.
- Primary roof material shall be of either single ply sheet roofing or built-up roofing. Standing-seam or glass panel roofing may be used at the building entry canopies and other features.
- Window frames shall be pre-finished (coated) aluminum window systems.
- Paving materials may include exposed aggregate concrete, integrally colored concrete, decorative colored and textured asphalt or cast concrete pavers. Pedestrian pathways within gardens may be stabilized decomposed granite.
- Highly reflective materials, such as mirror finish glazing will not be permitted. See additional design considerations section for bird friendly design guidelines.

Use of materials will further emphasize building entrances. Texture and durability are especially important at the human scale where visitors intimately experience the quality of finishes.

Roof equipment

All roof-mounted equipment, including satellite dish antennas, shall be completely screened from public view with parapets or screen walls of architecturally integrated colors and forms. Roof mounted mechanical equipment may not exceed the height of the screen walls.

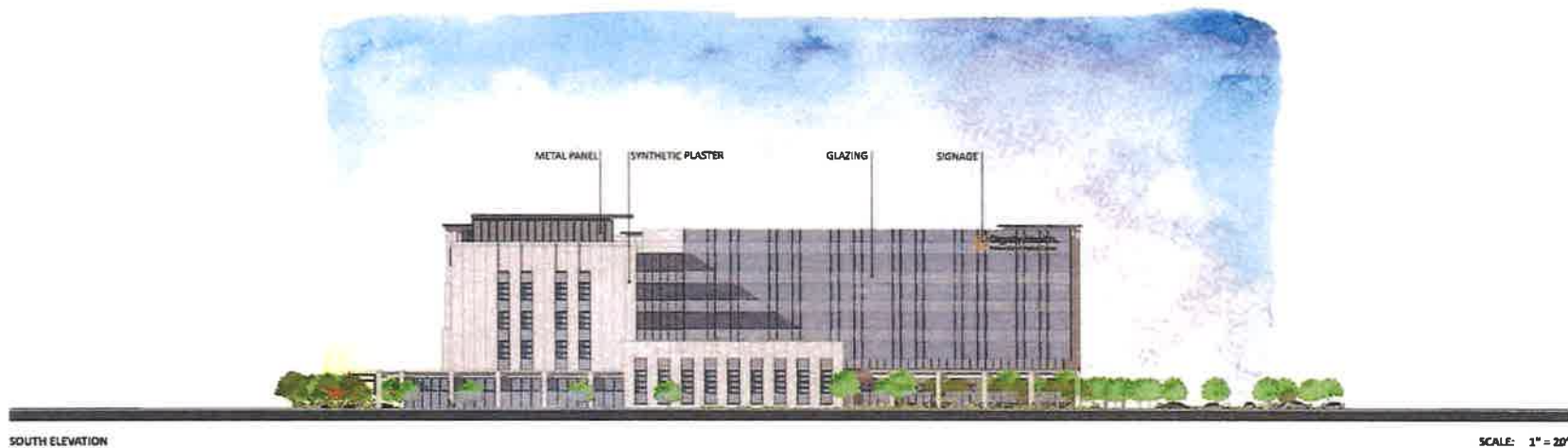
Additional Design Considerations

Canopy elements providing shelter for drop-off and pick-up of patients should be architecturally integrated within the total building aesthetic. Canopies can be used to bring emphasis to building entrances and contribute to clarity in wayfinding.

All roof-mounted equipment, including satellite dishes, antennas, and mechanical equipment shall be completely screened from public view with parapets or screen walls of architecturally integrated colors and forms.

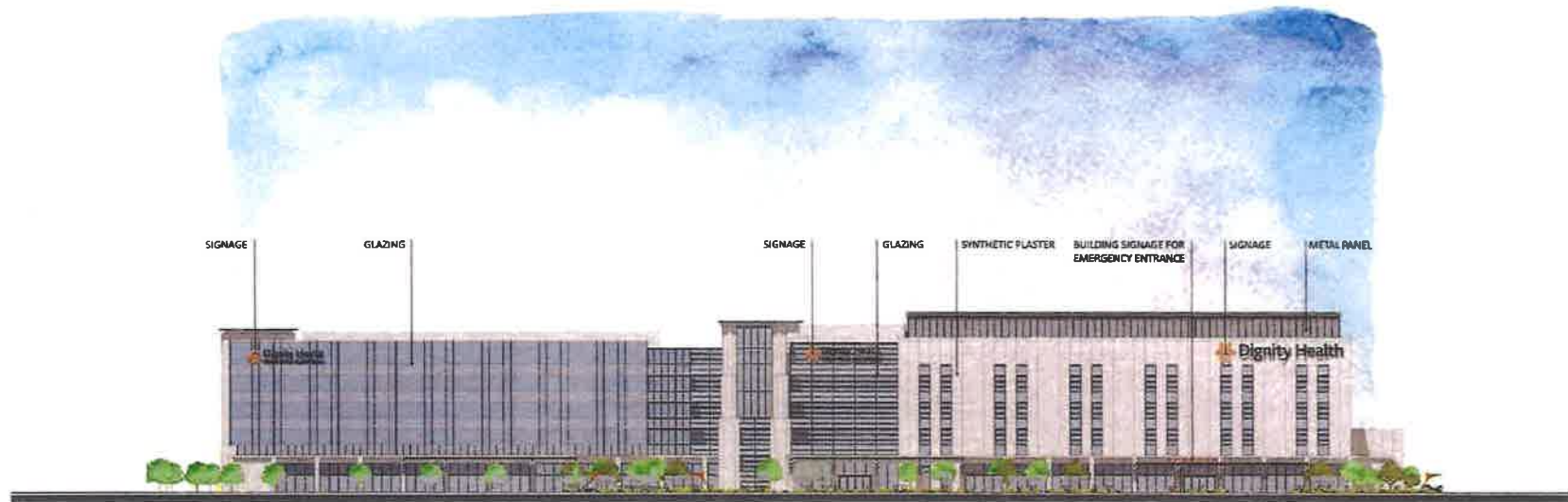
The south of Highway 50 area has grassland and oak savannah, and is likely used as a bird corridor. Bird friendly design strategies will be addressed in the exterior building and lighting design of the Hospital, where large expanses of curtainwall occur. In order to deter bird collisions, highly reflective glass, along with mirrored glass will not be permitted. Fly through conditions where glass provides a clear line of sight to birds will not be permitted. Planning for a bird friendly building may include using UV patterned glass, fritted glass, and low reflectance, opaque glass such as spandrel glass, window films, or solutions applied to interior glass, such as interior window shades, or a combination thereof. Landscaping adjacent to the curtainwall façade shall be low level. Another bird strike mitigation measure will be to specify exterior light fixtures that will shield the light source to minimize glass and light trespass and to facilitate better vision at night for birds.

8 | ARCHITECTURAL GUIDELINES



SOUTH ELEVATION

SCALE: 1" = 20'



EAST ELEVATION

SCALE: 1" = 20'

EXHIBIT I: EXTERIOR ELEVATIONS

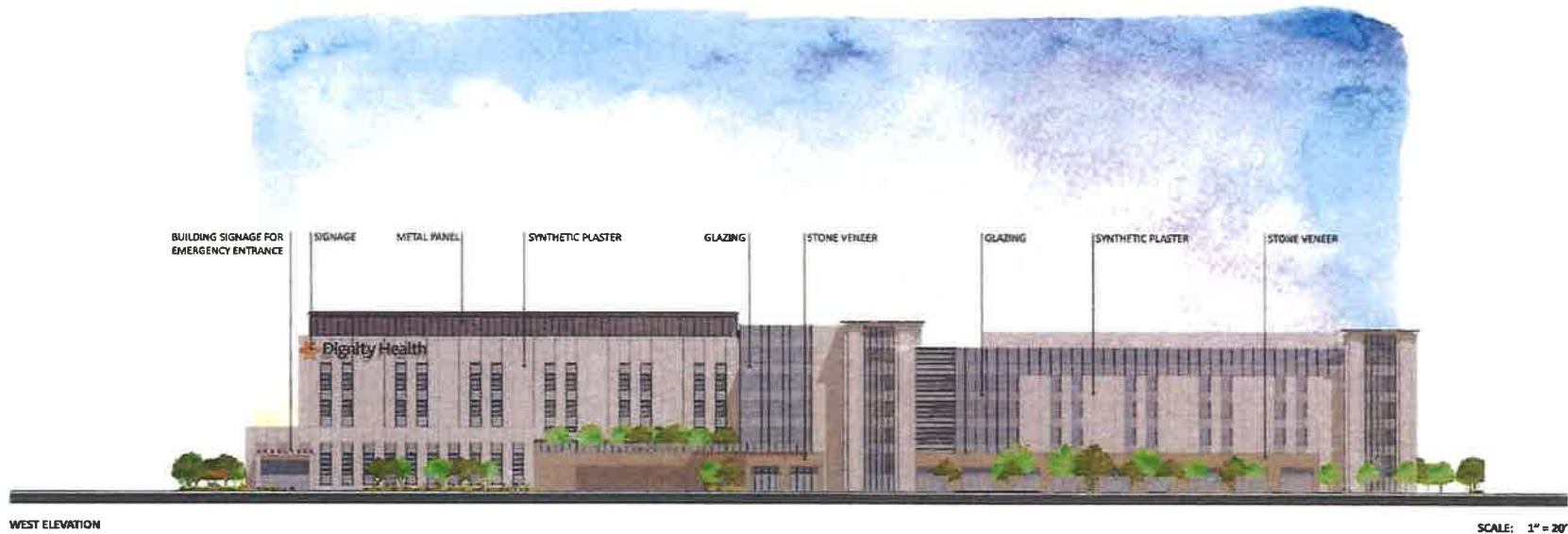
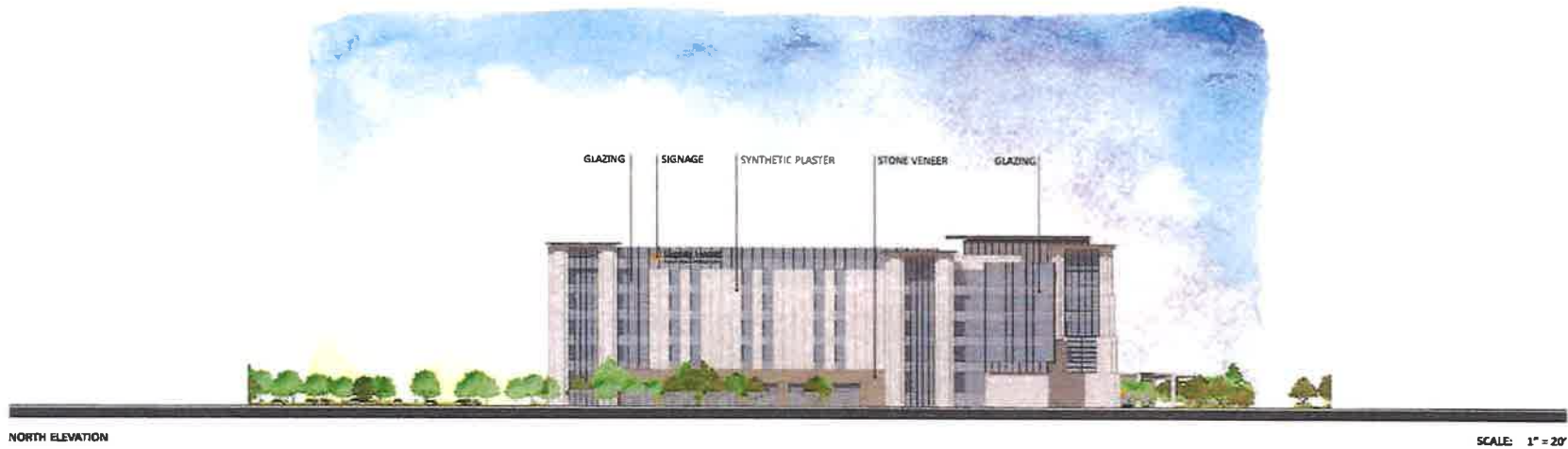


EXHIBIT J: EXTERIOR ELEVATIONS

10 | SITE & BUILDING LIGHTING

Appropriate site and building lighting is critical for establishing a welcoming and safe campus. As with building massing, lighting should address the visitors experience to the campus at a both a macro and human scale. Building accent lighting should be used to compliment the building forms and materials, and add further emphasis to significant elements such as building entrances. Site lighting should not be overly bright, but should illuminate parking areas, pedestrian pathways, areas of respite, and open plazas, so that these spaces are inviting and safe into the evening hours.

- Free standing parking lot lighting shall be the same throughout the parking areas. Height to be thirty (30) feet maximum.
- Luminance within parking and pedestrian areas shall comply with Folsom Municipal Code requirements.
- Lighting should be appropriately scaled to the building.
- Buildings can be lit with a combination of ground mounted up lighting, architecturally integrated down lighting, and building mounted sconce lighting.
- Color of the lighting should be white and warm, and at building entrances, lighting should flatter skin tones.

11 | SIGN CRITERIA

Appropriately designed signage and environmental graphics provide a clear, comforting, and welcoming experience to visitors. The application of these signage guidelines is a key component in creating an initial impression which conveys the quality of care, professionalism, and promise of excellence provided at the Folsom Ranch Medical Center.

Sign Types

Exhibit J shows conceptual diagrams of the various primary exterior sign types that may occur on the Medical Center campus. As future brand components evolve, these sign types and graphics may also change. Dignity Health reserves the right to modify onsite signage based on corporate branding efforts, which may change the visual identity system in the future. Below are examples of brand types that may change:



EXAMPLE OF CORPORATE BRAND TYPE



EXAMPLE OF CORPORATE BRAND TYPE

Selection of sign types and placement of signs are dependent on the viewer distance from the sign, rate of motion of the viewer, and critical wayfinding decision points. Distant Read Pylon and Distant Read Skyline sign types are intended to be viewed from Highway 50. Skyline sign types are to reinforce the wayfinding sequence at connector streets. Monument signs are best placed at entry points to the campus. Monument Directional signs are viewed from a vehicle traveling at lower speed within the campus boundaries. Onsite digital informational signage is permitted within these established sign types.



Pylon



Distant Read Skyline



Skyline



Monument



Monument Directional



Two-Post Directional



Post Mounted to Roof Canopy



Porte Cochere



Two-Sided Blade Sign

EXHIBIT K: SIGN TYPES

Sign Type	Sign Function	Sign Location	Sign Height
Monument	Marks campus entry points. Provides branding and wayfinding information at the campus edge and corners.	At campus edge entry points and corners of significant intersections	6' tall
Porte Cochere	Identifies primary entrances to the facility. Used for branding and wayfinding purposes within the campus boundaries.	At building canopies over primary building entrances.	3' tall; building mounted
Freestanding Sign -Pylon	Large scale branding and way-finding targeted toward vehicular traffic on adjacent busy streets and highways.	Adjacent to highways and busy roads.	80' max
Post Mounted to Roof Canopy	Directional signage marking building entry points for Emergency, Ambulance and specialized services	At building canopies over building entrances	3' tall: building canopy roof mounted
Skyline	Large scale branding and wayfinding targeted toward vehicular traffic on adjacent busy streets and highways.	At skyline and/or parapet level of select buildings on campus	up to 70" tall letter height, building mounted
Distant Read Skyline	Building mounted, monument scale branding signage specifically oriented for long approach viewing such as from a busy street or highway.	At skyline, high parapet walls, roof screen walls of select buildings on campus	letter height up to 120" tall
Monument Directional	Provides branding and wayfinding information at the campus edge and within the campus boundaries. Provides specific wayfinding information for individual departments on campus.	At primary vehicular decision points within the campus	12' max
Two-Post Directional	Provides branding and wayfinding information within the campus boundaries. Provides specific wayfinding information for individual departments on campus.	At secondary vehicular decision points within the campus	8' max
Two-Sided Blade Sign	Provides branding and wayfinding information within the campus boundaries for a singular campus department.	Emergency Department wayfinding	8' max



Appropriate scale of signs is dependent on the intended viewing distance.

Site Pageantry

Site pageantry may be incorporated with campus signage in order to call attention to events, holidays, and promotions. Pageantry consists of fabric or metal banners fixed to light poles and building facades throughout the campus. Pageantry should be of a consistent branded theme and should be concentrated along primary vehicular and pedestrian pathways.

Religious Symbols

Religious symbols in the form of crosses, crucifix, statues, etc. may be integrated within the landscaping and architecture.

Summary

As the project site is located adjacent to Highway 50, the site will have one freestanding freeway pylon sign. The sign area will not exceed 750 square feet. The height is proposed as 80 feet maximum from the nearest highway crown. It may include internal illumination behind the letters and or logos only. Freeway signage will provide visualization of the Medical Center from both east and westbound traffic along Highway 50. Maximum visualization is needed to provide patients and families with a safe wayfinding experience when in a heightened state of emergency or concern traveling at a high rate of speed.

Two Distant Read Skyline freeway signs are proposed in this development and will be located adjacent to and intended to be viewed either westbound or eastbound direction of Highway 50. As the project develops and evolves over phases, the design team will review key lines of sight to determine sign placement on the appropriate buildings per phase. Only one Distant Read Skyline sign will be installed to be visible from each direction of Highway 50 at one building at one time. The intent is to provide a hierarchy of signage, as the buildings are constructed over phases. Therefore the Distant Read Skyline signage will be relocated to the most prominent building as the project evolves.

Distant Read Skyline and Skyline building signs and one Freestanding Pylon sign are proposed for this development, as shown in Exhibit K. The sign locations as shown in Exhibit L are potential locations, with the exact location to be evaluated as project phases progress. As noted in the section below under "Proposed Variances," the design and precise location of Distant Read Skyline and Skyline building signs will be submitted for review and approval by the Planning Commission along with each building design submittal.



DISTANT READ PYLON/ FREESTANDING FREEWAY SIGN AND DISTANT READ BUILDING SIGNS VIEWED FROM WESTBOUND HIGHWAY 50

- A** Distant Read Skyline at Hospital Building
- B** Distant Read Skyline at Medical Office Building

Distant Read Skyline signage will be provided at either location A or B for each view. As the project develops and evolves over phases, the design team will review key lines of sight to determine actual distant read signage placement.



DISTANT READ PYLON/ FREESTANDING FREEWAY SIGN AND DISTANT READ BUILDING SIGNS VIEWED FROM EASTBOUND HIGHWAY 50

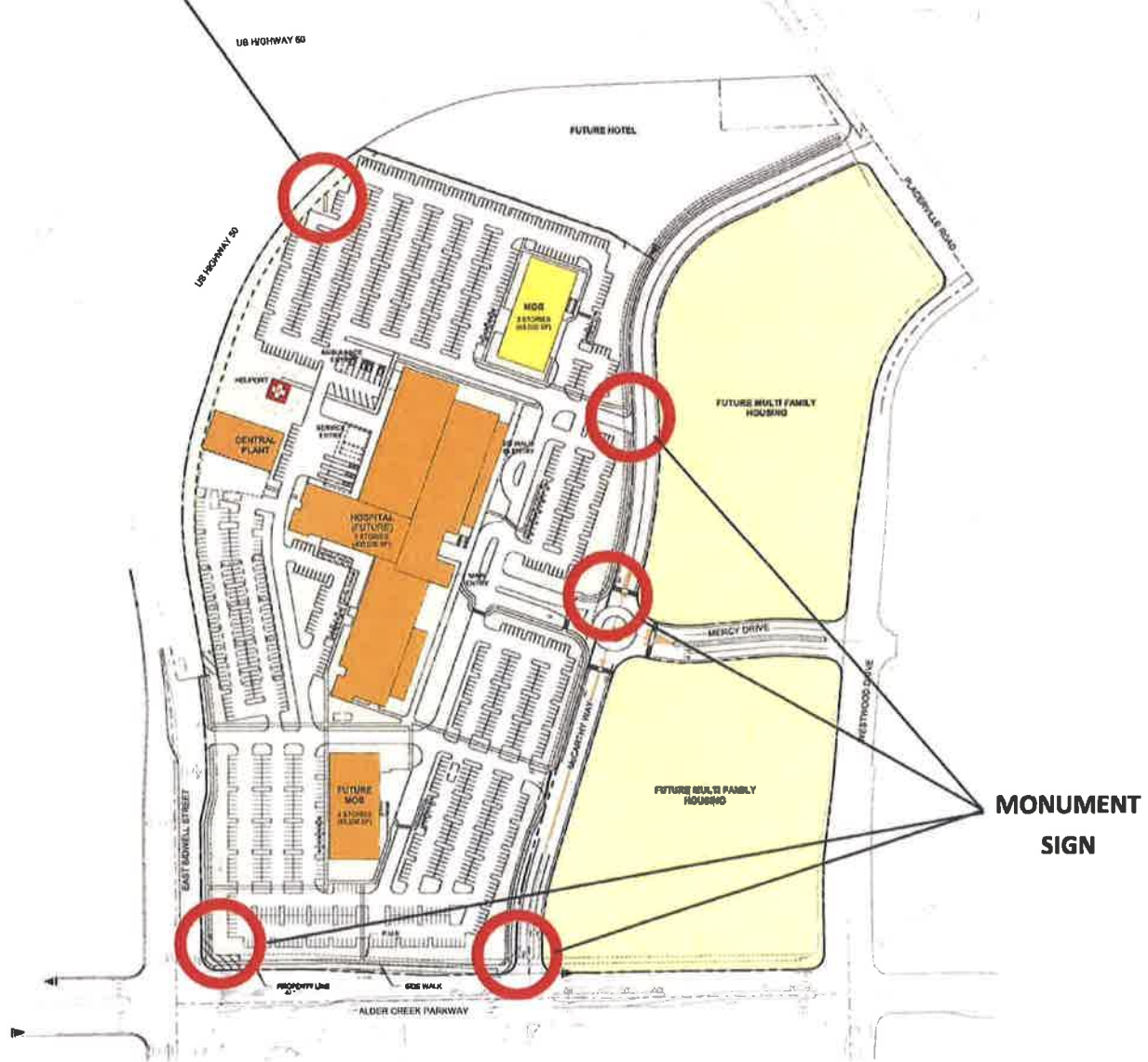
EXHIBIT L: DISTANT READ PYLON AND BUILDING SIGNAGE

12 | PUBLIC REALM DESIGN ELEMENTS

In addition to on-site design considerations, the design of the public realm which borders this property must be understood and integrated into the project planning. Guidelines specific to the design of the public realm are included in the Folsom Plan Area Specific Plan Community Design Guidelines. In particular, this site abuts a “Primary Gateway” and an “Enhanced Intersection.” These particular design elements announce arrival and help to establish the character of the larger plan area and are not calculated in overall sign area square footage for this project. Refer to the guideline document for specific design criteria pertaining to “Primary Gateways” and “Enhanced Intersections.”

Exhibit M indicates the proposed locations for major site signage for the project that is not affixed to a building wall, and therefore does not include Distant Read Skyline and Skyline Sign types.

**DISTANT READ PYLON/
FREESTANDING
SIGN**



**MONUMENT
SIGN**

WALL SIGNAGE NOT SHOWN



EXHIBIT M: CONCEPTUAL MAJOR SITE SIGNAGE PLAN

13| PROPOSED VARIANCES

The proposed development has the following variances from the FPASP as outlined below. The proposed conditions are noted in bold text, with the corresponding Folsom Municipal Code or FPASP in italics:

- **Building height is proposed as 120' maximum, exclusive of mechanical roof screens.** *Maximum building height in the FPASP General Commercial Development Standards Table A.11 is 50' for SP-GC land use.* This variance is requested in order to maximize the site and building efficiency of the project use for a Hospital. Hospitals typically have higher floor to floor dimensions due to interstitial space requirements for mechanical, plumbing, electrical, low voltage and medical gas systems.
- **FAR is proposed as .55 maximum for the overall project site.** *FAR for SP-GC is 0.25 in the FPASP General Commercial Development Standards Table A.11.* This variance is requested in order to maximize the site and building efficiency of the project use for a Hospital. This FAR request supports the development of a Medical Center appropriately sized for the surrounding community needs. The hospital is sized to provide comprehensive services for the area.
- **Signage variances are proposed in this development for Freestanding Pylon, Distant Read Skyline, and Skyline signage.** Distant Read Skyline signage is intended to be viewed from vehicles on Highway 50 and will provide visualization of the Medical Center from both east and westbound freeway traffic. Maximum visualization is required in order to provide patients and families with a safe wayfinding experience as they negotiate traffic at high speeds on the freeway to their destination. Skyline signage at the building exterior wall is requested to orient drivers at Alder Creek Parkway and East Bidwell, as opposed to regional patients and visitors using Highway 50. **The Medical Center Distant Read Skyline signage shall be limited to two (2) signs for the Project with a total sign area estimated at 2,000 square feet for the entire site. Placement of signage will be shown on building designs when presented to the Planning Commission for final design approval. For Folsom Ranch Medical Center, multiple wall signs are proposed to include Skyline, Porte Cochere, and Monument sign types. The proposed Skyline, Porte Cochere, and Monument signage is estimated at 1,750 square feet total for the entire site, including both medical office buildings and the hospital. Folsom Municipal Code Section 17.59.040 Sign regulations state that the maximum sign area is one and a half square feet of signage for each 1 lineal foot of primary building frontage up to a maximum of 150 square feet. Freestanding Pylon, Distant Read Skyline, Skyline, Porte Cochere and Monument signage is proposed to have LED illumination. Folsom Municipal Code states that each hospital use is permitted 1 non illuminated wall sign with a maximum sign area of .5 square feet for each 1 lineal foot of primary building frontage up to a maximum sign area**

of 50 square feet. Two Distant Read Skyline freeway signs are proposed in this development and will be located adjacent to and intended to be viewed either westbound or eastbound direction of Highway 50. As the project develops and evolves over phases, the design team will review key lines of sight to determine sign placement on the appropriate buildings per phase. Only one Distant Read Skyline sign will be installed to be visible from each direction of Highway 50 at one building at one time. The intent is to provide a hierarchy of signage, as the buildings are constructed over phases. Therefore the Distant Read Skyline signage will be relocated to the most prominent building as the project evolves. Distant Read Skyline signage proposed on the building frontage facing the freeway is proposed to have a letter height of 120 inches. Folsom Municipal Code states that only one additional wall sign is permitted on the building frontage facing the freeway, and that the letter height maximum is 60 inches from a distance of more than 500 feet from the nearest freeway lane. A proposed Freestanding Pylon sign near the freeway will not exceed 750 square feet sign area. This Freestanding Pylon sign is proposed as eighty feet maximum from the crown of the adjacent highway. The additional height is offset by the fact that the site is 6 feet below grade and visibility of other signage may be obscured because the project site is behind the berm of the overpass. Folsom Municipal Code Section 17.59.040 Sign regulations indicate a maximum height of eighteen feet measured from the crown of the adjacent highway. Further detail regarding signage is provided in Sign Criteria, Section 10, of this guideline. These variances are requested in order to highlight the Dignity Health Medical Center as a proud member of the Folsom Ranch community and to accentuate wayfinding to the project site. Size estimates are provided for the Distant Read Skyline and Skyline signage due to the possible addition of future "centers of excellence" that would be highlighted for patient awareness. Both the final placement of signage and the precise size of signage (except for the Freestanding Pylon sign, the size of which is fixed) will be shown on building designs when presented to the Planning Commission for final design approval.

- **Variance proposed to apply Folsom Municipal Code for bicycle parking count calculations.** *For Office land use, FPASP 2 spaces per 20 required vehicle parking spaces plus one additional bicycle parking space for every 10 additional vehicle parking spaces is required. Variance is requested due to most patients arriving with a family member in a vehicle. 20 bicycle parking spaces are provided at each medical office building. An additional 16 bicycle parking spaces will be provided at the Hospital.*
- **Variance proposed to provide a maximum of 6 loading/unloading spaces to coordinate with Dignity Health's just in time delivery program.** *FPASP Table A.14 requires 3 each for office and 3 for hospital for a total of 9 loading/unloading spaces. Variance is requested due to minimized*

anticipated volumes and frequencies of truck traffic based on Dignity Health's just in time delivery program. High volumes of deliveries and constant truck traffic are discouraged, as they stress the Hospital system with the need for excessive storage areas. Truck traffic will be limited to off peak hours. 6 loading/unloading spaces, 10' wide x 35' long by 14' high per Folsom Municipal Code are provided at the hospital.

Attachment 15

Folsom Ranch Medical Center Booklet (Separate Bound Document)

Attachment 16

Amendment No. 2 to First Amended and Restated Development Agreement Relative to Folsom South Specific Plan (Dignity Health)

FOR THE BENEFIT OF THE CITY OF FOLSOM
PURSUANT TO GOVERNMENT CODE §6103

RECORDING REQUESTED BY CITY CLERK

WHEN RECORDED MAIL TO:

City Clerk
City of Folsom
50 Natoma Street
Folsom, CA 95630

(SPACE ABOVE THIS LINE RESERVED FOR RECORDER'S USE)

**AMENDMENT NO. 2 TO
FIRST AMENDED AND RESTATED TIER 1 DEVELOPMENT
AGREEMENT
RELATIVE TO FOLSOM SOUTH SPECIFIC PLAN
(DIGNITY HEALTH)**

**AMENDMENT NO. 2 TO
FIRST AMENDED AND RESTATED TIER 1 DEVELOPMENT AGREEMENT
RELATIVE TO FOLSOM SOUTH SPECIFIC PLAN
(DIGNITY HEALTH)**

This Amendment No. 2 to First Amended and Restated Development Agreement ("Amendment No. 2") is entered into this ___ day of _____, 2021, by and between the City of Folsom ("City") and Dignity Health, a California nonprofit public benefit corporation ("Landowner") pursuant to the authority of Sections 65864 through 65869.5 of the Government Code of California. All capitalized terms used herein and not otherwise defined herein shall mean and refer to those terms as defined in Section 1.3 of the Restated Development Agreement and Amendment No. 1 to the Restated Development Agreement, described below between the Predecessor in Interest to Landowner and the City.

RECITALS

A. Restated Development Agreement and Amendments Thereto. The City and Landowner's predecessor in interest Eagle Commercial Partners, LLC (referred to herein as the "Predecessor in Interest") previously entered into that certain First Amended and Restated Tier 1 Development Agreement By and Between the City of Folsom and Landowner Relative to the Folsom South Specific Plan, recorded on July 15, 2014, in the Official Records of the County Recorder of Sacramento County in Book 20140715, Page 0517 (the "Restated Development Agreement"). Section 1.5 of the Restated Development Agreement allows the Restated Development Agreement to be amended from time to time by mutual written consent of the parties. On November 12, 2015, Eagle Commercial Partners, LLC and the City entered into Amendment No. 1 to First Amended and Restated Tier 1 Development Agreement Relative to the Folsom South Specific Plan, recorded on January 29, 2016, in the Official Records of the County Recorder of Sacramento County in Book 0160129, Page No. 0385 ("Amendment No. 1"). The Restated Development Agreement and Amendment No. 1 are collectively referred to herein as the "Development Agreement."

B. Conveyance of Property to Landowner and Assignment of Development Agreement. Predecessor in Interest conveyed the Property to its affiliate, Enclave at Folsom Ranch, LLC ("Predecessor Affiliate") which in turn conveyed the Property identified by legal description on **Exhibit A-1** and depicted on **Exhibit A-2** to Landowner on June 5, 2020. Concurrently with such conveyance, Predecessor in Interest entered into an Assignment and Assumption Agreement Relative to The Folsom South Specific Plan Amended and Restated Tier 1 Development Agreement, pursuant to a form approved and required by the City, recorded on June 5, 2020, in the Official Records of the County Recorder of Sacramento County as Document Number 202006050658 ("Assignment and Assumption Agreement"). The Assignment and Assumption Agreement transferred all rights, title, interest, burdens and obligations of the

Predecessor in Interest under the Development Agreement with respect to the Property to Landowner.

C. Prior City Determinations and Approvals Relative to the Property. The Property is identified as "Parcel 1" on a Parcel Map approved by the City Planning Commission on December 4, 2019 (PN 19-389). Parcel 1 is one of four parcels created by the subdivision of the property identified as Parcel 85a in the Specific Plan. The Final Parcel Map including Parcel 1 was approved by the City Council and thereafter filed for record on May 22, 2020 in Book 240, Page 13 of Parcel Maps, Sacramento County. Prior to approval of the subdivision of Parcel 85a, on March 17, 2020, the Community Development Director for the City issued approval of a Minor Administrative Modification ("MAM") associated with Parcels 61, 77, 78, and 85A (PN 20-003). The MAM provided for the transfer of certain residential units and gross square footage within the four parcels and remains in effect. The Community Development Director thereafter issued a second letter on April 1, 2020, confirming that Table A-7 of the Specific Plan contained a clerical error regarding allowed land uses for General Commercial (GC) and Regional Commercial (RC) for several parcels, including Parcel 85A (hereafter the "Table A-7 Correction"). The allowed land uses in the Table A-7 Correction remain in effect.

D. Subsequent Entitlements. On [REDACTED], the City Planning Commission, in a duly noticed and conducted public hearing, approved the Subsequent Entitlements for the Development of the Property as follows:

1. Planned Development Permit (including the Site Improvements and all Amendments to the Planned Development Permit submitted and considered as of the date of the hearing).
2. Planned Development Guidelines.
3. Conditional Use Permit.
4. Site Plan, as depicted on Exhibit B (Except parcels marked "Future Multifamily Housing" and "Future Hotel." Other than the Site Improvements, development on Parcels marked "Future Multifamily Housing" and "Future Hotel" (Parcels 2-4 on Exhibit A-2) are not part of the Subsequent Entitlements and shall not be vested).

The Planning Commission further recommended for approval by the City Council of this Amendment No. 2 to the Development Agreement.

E. Purpose of Amendment; Findings Related to Medical Center. Landowner is processing Subsequent Entitlements for the Development of the Property for comprehensive medical uses, specifically two medical office buildings, an acute care hospital and related structures (as further defined in Section 1.3 of this Amendment No.

2 and hereafter the "Medical Center"). Landowner has requested that the Subsequent Entitlements, including the Conditions of Approval related thereto, as approved by the City, be included in the definition of Entitlements as that term is used throughout the Development Agreement, pursuant to Section 1.5.3 of the Development Agreement. The City and Landowner also intend to amend certain provisions of the Development Agreement to allow for flexibility in the approval of and term for the Subsequent Entitlements, specifically in relation to the development of a Medical Center. The City Council has determined that flexibility is required for the Subsequent Entitlements because development of the Medical Center presents significant benefits to the City and the region and unique characteristics in the buildout of the Medical Center buildings and related structures, as follows:

1. Development of the Medical Center will occur over a long period of time, with a phased timeline for construction and potential adjustments to physical structures as medical delivery systems change over time;
2. The California Office of Statewide Health Planning and Development ("OSHPD") is required to approve the hospital building design which may result in required changes to the design of the Medical Center buildings and related structures;
3. The Medical Center will generate significant employment and other economic benefits to the City;
4. The Medical Center will provide needed expansion of access to health care services for the City and other jurisdictions in the region;
5. A significant capital investment is required for the Medical Center buildings and related structures; and
6. The status of Landowner as a nonprofit public benefit corporation.

F. Property. The subject of this Amendment No. 2 is the Development of the Property, as defined in Section 1.3. Landowner owns the Property and represents that all persons holding legal or equitable interests in the Property shall be bound by this Amendment No. 2 and the Development Agreement.

G. Hearings. On _____, 2021, the City Planning Commission, designated as the planning agency for purposes of development agreement review pursuant to Government Code section 65867, in a duly noticed and conducted public hearing, considered this Amendment No. 2 and recommended that the City Council approve this Amendment No. 2 to the Development Agreement. On _____, 2021, the City Council, in a duly noticed and conducted public hearing, conducted the first reading of Ordinance No. _____ and approved this Amendment No. 2, and thereafter conducted the second reading of Ordinance No. _____ at a duly

noticed regular meeting of the City Council on _____, 2021 and adopted the Ordinance approving this Amendment No. 2.

H. Environmental Review. On _____, 2021, the Planning Commission considered the Environmental Checklist and Addendum Dignity Health Folsom Ranch Medical Center (the "Addendum") to the Specific Plan EIR/EIS for Development of the Property consistent with the Specific Plan. An Initial Study prepared in support of the Addendum identified mitigation measures to reduce environmental impacts to less than significant, and those mitigation measures have been incorporated into the Project and the Subsequent Entitlements, as reflected by the findings adopted by the Planning Commission in connection with the approval of the Subsequent Entitlements and the City Council's consideration, adoption of findings, and approval of this Amendment No. 2.

I. No New Impacts Associated with Approval of Amendment. The City Council has determined that the adoption of this Amendment No. 2 involves no new impacts not considered in the Specific Plan EIR, the Previous Environmental Analyses listed in Section 1.2 of the Addendum, and the Addendum; therefore, no further environmental documents relating to the adoption of this Amendment No. 2 are required.

J. Consistency with General Plan and Specific Plan. Having duly examined and considered this Amendment No. 2, the City finds and declares that this Amendment No. 2 is consistent with the General Plan and the Specific Plan.

NOW, THEREFORE, the parties hereto, in consideration of the mutual covenants, promises, and agreements herein contained, and for other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged and agreed, the parties agree to hereby amend the Development Agreement as follows:

AMENDMENTS

1. Incorporation of Recitals. The Recitals above are true and correct and constitute enforceable provisions of this Amendment No. 2.

2. Definition and Section 1.5.3 – Subsequent Entitlements. The term "Subsequent Entitlements" in the Definitions Section of the Restated Development Agreement and also referenced in Section 1.5.3 of the Restated Development Agreement and later updated by Amendment No. 1 is amended to include the following:

- a. The MAM approved by the Community Development Director on March 17, 2020.
- b. The Table A-7 Correction issued by the Community Development Director on April 1, 2020.

- c. The Planned Development Permit (including the Site Plan for Parcel 1 and all amendments to the Planned Development Permit and Planned Development Guidelines); the Conditional Use Permit; the Planned Development Guidelines and this Amendment No. 2 approved by the Planning Commission and the City Council on [insert date].
- d. Landowner acknowledges and agrees that, in addition to design features described in the Planned Development Guidelines and approved by the Planning Commission as described in Recital D, supplementary design detail for individual buildings, related structures and the heliport will be presented to the City as required by the Specific Plan and the Folsom Municipal Code by Landowner for review and approval by the City. As this additional design detail is presented to the City, Landowner shall prepare Design Guidelines for the Medical Center for review and approval by the City, which may be updated as phases of Development of the Project continue. Upon approval of the Design Guidelines by the City (or sections thereof being amended and approved by the City from time to time), such Design Guidelines shall be a Subsequent Entitlement without the necessity of further amendment to this Amendment No. 2 or the Development Agreement.

3. **Section 1.3 – Definitions.** The following Definitions are added:

“Adopting Ordinance” means Ordinance No. _____, dated _____, approving this Amendment No.2.

“Addendum” means the Environmental Checklist and Addendum, Dignity Health Folsom Ranch Medical Center, dated [insert date of document].

“Conditional Use Permit” means the Conditional Use Permit approved by the City pertaining to the Project.

“Design Guidelines” means the design guidelines for the Medical Center approved by the City, and thereafter to be applied by the City to guide and evaluate the design of the Medical Center.

“Effective Date” means the date which is thirty (30) calendar days after the date of the Ordinance approving this Amendment No. 2.

“Environmental Analysis” means the Addendum and the Previous Environmental Analyses listed in Section 1.2 of the Addendum.

“Landowner” means Dignity Health, a California nonprofit public benefit corporation.

“Planned Development Permit” and “Planned Development Guidelines” include the Planned Development Permit and Planned Development Guidelines approved by the Planning Commission pertaining to the Project.

“Medical Center” means the buildings and related structures, including but not limited to the hospital and two medical office buildings, in which health care, research, staff support and ancillary services are provided (including the heliport) both temporary or permanent buildings or structures, and whether considered supportive or ancillary that are identified on the Planned Development Permit and described in the Planned Development Guidelines and the Conditional Use Permit.

“Predecessor in Interest” shall mean the prior owner of the Property and party to the Restated Development Agreement and Amendment No. 1, i.e., Eagle Commercial Partners, LLC.

“Predecessor Affiliate” shall mean the affiliate of Predecessor in Interest, Enclave at Folsom Ranch, LLC.

“Project” means development of the Medical Center and Site Improvements included in the Planned Development Permit consistent with the Development Agreement and the Subsequent Entitlements.

“Property” means the land identified by legal description on Exhibit A-1 and depicted on Exhibit A-2.

“Site Improvements” means the grading, infrastructure and off-site improvements identified in the Planned Development Permit Guidelines.

“Square Footage” for purposes of the Medical Center shall mean “Occupied Square Footage,” as that term is defined and described in the Planned Development Permit and the Planned Development Guidelines, as distinct from building gross square footage.

“Subsequent Entitlements” shall have the amended definition set forth in paragraph 2, subsections (a)-(d), inclusive, of this Amendment No. 2.

“Technical Memorandum” shall mean the PA Parcel 85A Zone Supplemental Analysis dated April 28, 2021 prepared in conjunction with the Environmental Analysis that determined the appropriate water infrastructure connection for water services to the Property.

“Traffic Study” means the traffic analysis prepared and included in the Addendum titled Final Local Transportation Analysis & CEQA Impact Study, dated April 23, 2021 and the Local Transportation Analysis & CEQA Impact Study Errata dated April 29, 2021.

“Vested Rights” means the rights to Develop the Property consistent with the terms and provisions of the Restated Development Agreement, Amendment No. 1, this Amendment No. 2 and the provisions of the Subsequent Entitlements.

4. **Section 1.4.1 – Commencement, Extension, Expiration.** In light of the unique circumstances identified in Recital E of this Amendment No. 2, the Term of the Development Agreement and this Amendment No. 2 are extended to June 30, 2056, or until 530,000 occupied square feet has been built out, whichever is later. All other provisions of Section 1.4.1 of the Restated Development Agreement remain in full force and effect. Landowner may request an additional extension pursuant to Section 1.4.1 of the Restated Development Agreement.

5. **Section 1.6 –Changes to the City’s Inclusionary Housing Ordinance** in Amendment No. 1 is hereby revised to read as follows: “The City has amended the Inclusionary Housing Ordinance (i.e., Folsom Municipal Code Chapter 17.104) by Ordinance No. 1243, to eliminate Second Dwelling Units (also referred to as “granny flats”) as an alternative means of meeting the City’s inclusionary housing requirements. Both Parties acknowledge that the Project is for medical uses and not residential housing. However, in light of the allowed uses under the Specific Plan, City has requested that Landowner acknowledge, and Landowner hereby acknowledges, that there is no vested right to use Second Dwelling Units as an alternative means for meeting the City’s inclusionary housing requirements and that this alternative shall not be available to Landowner from and after the date of Ordinance No. 1243. Landowner further acknowledges that the State adopted amendments to Section 65850 of the California Government Code (specifically Section 65850(g)), effective January 1, 2018, to allow for the implementation of inclusionary housing requirements in residential rental units, upon adoption of an ordinance by the City. The Landowner is not currently contemplating any residential rental projects within the Property; however, in the event the City amends its Inclusionary Housing Ordinance with respect to rental housing pursuant to Section 65850(g), Landowner (or a successor in interest) agrees that the Property shall be subject to said City Ordinance, as amended, should any residential rental project be proposed within the Property. Other than the elimination of the “granny flat” option and the possible future application of an inclusionary housing requirement on residential rental properties (upon the conditions stated herein), the Parties agree that all other alternatives for meeting the City’s inclusionary housing requirements remain vested to the full extent provided for in the Restated Agreement.”

6. **Section 2.1 – Permitted Uses.** The permitted uses of the Property, the density and intensity of use, the maximum height and size of proposed buildings and related structures, set backs, Square Footage (as defined in Section 1.3 and calculated based on Occupied Square Footage and not building gross square footage), heliport location and approach, all signage approved in the Subsequent Entitlements (inclusive of spiritual symbolism), provisions for reservation or dedication of land for public purposes and location of public utilities and public improvements shall be those set forth

in the Entitlements, the Subsequent Entitlements, the Development Agreement and this Amendment No. 2. The permitted uses of the Property shall also include the types of buildings and related structures within the definition of Medical Center and as identified in the Subsequent Entitlements that are ancillary to the development of the Medical Center or the provision of medical services, including but not limited to concrete pads, trailers and structures or facilities that may be necessary for emergency (e.g., pandemic) services (including, but not limited to, tents), as long as they are identified in the Site Plan attached hereto as **Exhibit B**. Any temporary structures that are not identified in the Site Plan shall be processed through City administrative processes, with recognition given to the expedited review that may be necessary for temporary structures to address emergency purposes (including but not limited to pandemics).

7. **Section 2.2 - Vested Rights.** The City agrees that, except as otherwise provided in and as may be amended in accordance with the Exceptions to Vested Rights set forth in Section 2.2.3 of the Restated Development Agreement, Amendment No. 1, and this Amendment No. 2, the City is granting, and grants herewith, Vested Rights to Development for the Term of this Amendment No. 2 in accordance with the terms and conditions set forth herein. The City acknowledges that the rights vested by the Restated Development Agreement, Amendment No. 1 and this Amendment No. 2 include the land uses, utility connections and water (subject to the provisions of Sections 4.6 of the Restated Agreement and Section 4.6.1 of this Amendment No. 2), approximate acreages and Site Plan for the Property as shown and described in **Exhibits A-2 and B** attached hereto. Nothing in this Amendment No. 2 shall impair or affect the rights of Landowner under a vesting tentative map or the City's rights to condition such maps. (Govt. Code Sec. 66498.1, et seq.)

Such uses shall be developed in accordance with the Subsequent Entitlements, as the Subsequent Entitlements are described in Section 1.5.3 of this Amendment No. 2 and as approved by the City on the Effective Date.

Section 2.2.1 – Vested Provisions of the PFFP. No changes.

Section 2.2.2 - Vested Provisions of the Specific Plan. In addition to the provisions of Section 2.2.2(A)-(C) of the Restated Development Agreement, as modified by Amendment No. 1, the following shall apply to the Property:

- D. Notwithstanding the provisions of Section 2.2.2(C) of Amendment No. 1, the street width and roadway sections for McCarthy Way and Mercy Way as approved in the Planned Development Permit and the Planned Development Guidelines, as well as all internal road widths for the Medical Center shall be vested for the Term of this Amendment No. 2, except that the City reserves all rights to add turn lanes, deceleration tapers and other necessary traffic improvements to accommodate safe vehicular access to the Medical Center in the future that have been analyzed and approved

in the Traffic Study or in a technical traffic analysis approved by the Planning Commission or City Council in connection with another project (either public or private).

- E. The Specific Plan identifies a route for Bus Rapid Transit (“BRT”), which as of the Effective Date would be constructed and operated by Sacramento Regional Transit (“Sac RT”). The route identified for BRT has been considered in the Traffic Study, and the Subsequent Entitlements, including the Conditions of Approval, take into account the location for BRT identified in the Specific Plan. If Sac RT proposes any modifications to the location of BRT or location of a bus stop on the Property, the City agrees to (a) notify Landowner, (b) work in good faith with Landowner on any modifications to the location of BRT or a bus stop that may impact the Medical Center, and (c) notify Sac RT that the Subsequent Entitlements are vested pursuant to the terms of this Amendment No. 2.

Sections 2.2.3 – 2.2.8 Exceptions to Vested Rights. No changes, except for the addition of Section 2.2.9 as an Exception to Vested Rights, as follows:

Section 2.2.9 – Intersection Controls at Alder Creek Parkway and McCarthy Way. The Planned Development Permit proposes a non-standard traffic signal (i.e., 700-foot signal spacing) at the left turn access to the Medical Center at the intersection of Alder Creek Parkway and McCarthy Way as depicted on Exhibit C to this Amendment No. 2. The Environmental Analysis concluded, based on the Traffic Study, that no significant environmental impacts would result from 700-foot signal spacing proposed by Landowner for a traffic signal at this intersection, although this spacing does not meet City design standards and is not reflected in the Specific Plan. City acknowledges that Landowner proposes to install the proposed signal concurrent with development of Phase 4 (second addition of 100 beds) of Project development and no sooner (unless traffic conditions require earlier installation), and City requires an updated analysis prior to final approval of installation of the traffic signal to identify any impacts to the Specific Plan transportation system. Subject to the specific requirements of Condition of Approval No. [], Landowner will prepare a supplemental traffic study no later than one year before the proposed installation of the traffic signal for City review and evaluation. The supplemental traffic study shall evaluate the traffic impacts associated with the installation of a traffic signal at Alder Creek Parkway and McCarthy Way. If the supplemental traffic study concludes that there is no significant traffic operational impact, the traffic signal may be installed by Landowner pursuant to the Conditions of Approval and consistent with technical specifications as approved by the City Engineer. For purposes of this section, “traffic operational impact” shall mean and include an environmental impact under CEQA, a safety impact, an impact to BRT, or an impact that results in unacceptable delays to an adjacent street. If the Traffic Study identifies any significant traffic operational impacts that can be mitigated and the

Landowner desires to install the traffic signal, the City will identify the required mitigation and the Landowner shall install the traffic signal along with the required mitigation. In the event the supplemental traffic study identifies any significant traffic operational impact that cannot be mitigated, the City will advise the Landowner and the traffic signal shall not be installed, and the intersection configuration approved as part of the Subsequent Entitlements shall remain in place. Any other modifications proposed by Landowner shall be subject to appropriate environmental review, City approval, and compliance with applicable City standards. If City determines that the left turn access to the Medical Center at the intersection of Alder Creek Parkway and McCarthy Way must be discontinued for any reason, City shall (a) advise Landowner of the City's intention to discontinue access, (b) provide Landowner with a reasonable opportunity to comment on the City's determination to discontinue access, including an appeal to the City Council, and (c) consider the impact on safe and efficient public access to the hospital and emergency services, including any evidence Landowner may provide to City with respect to increased driving times.

Section 2.2.10 – Class 1 Bicycle Path. Landowner acknowledges that it is responsible for rough grading and installation of the necessary retaining wall at its sole cost and expense to accommodate the Class 1 Bicycle Path on the northern portion of the Property as shown in Figure 7.32 of the Specific Plan, and that said work shall be completed prior to the issuance of a building permit on the second expansion of the hospital, identified as Phase 4 and is anticipated to occur in approximately 2034. The City agrees that the design of the Class 1 Bicycle Path shall impact no more than five (5) parking spaces, and further that Landowner shall be relieved of the aforementioned obligation should the City amend the Specific Plan to relocate the Class 1 Bicycle Path, or if the City shall not have identified funding for construction of said the Class 1 Bicycle Path across the Property and the connection to East Bidwell Street at the time of issuance of a building permit on the second expansion of the hospital for Phase 4. For purpose of this section, "identified funding" shall mean either: (1) the Class 1 Bicycle Path is incorporated into a subsequent project to widen the East Bidwell Overcrossing structure of US Highway 50 or (2) the submission or application for federal, state or other grants which, together with the City's available matching funds, would be sufficient to construct the Class 1 Bicycle Path across the Property and the connection to East Bidwell Street.

8. **Section 3.5 – EIR Mitigation Measures.** Notwithstanding any other provision in the Restated Development Agreement or Amendment No. 1, as amended hereby, as and when Landowner elects to Develop the Property, or any portion of the Property, Landowner shall be bound by, and shall perform, or cause to be performed, all mitigation measures contained in the Specific Plan EIR/EIS, the Addendum, the Backbone Infrastructure IS/MND, and any environmental mitigation measures referenced therein applicable to the Development of the Property. The City acknowledges that Landowner has entered into a contractual agreement with

Predecessor's Affiliate, which requires that the Site Improvements and related mitigation measures identified in **Exhibit D** attached hereto shall be completed by the Predecessor's Affiliate. Landowner acknowledges that the Site Improvements are Conditions of Approval and, as such, are incorporated into the Subsequent Entitlements and are required for completion pursuant to the timing identified in the Conditions of Approval.

9. **Additions to Development Agreement.** Landowner acknowledges and confirms the Additions to Restated Development Agreement provided for in Section 2 of Amendment No. 1. The following Sections are also added to the Development Agreement as follows:

a. **Section 3.9.2.1 - Phasing of In-Tract Improvements.** The City acknowledges that the Medical Center will be constructed in numerous phases, with the timing and order of phases to be at the discretion of Landowner. The required in-tract improvements for development of the Medical Center shall be phased along with the specific building phases that trigger the need for in-tract improvements, as provided for in the Conditions of Approval for the Subsequent Entitlements.

b. **Section 3.9.3 – Design Review.** Landowner has provided some information related to design of the Medical Center in the Planned Development Guidelines, but acknowledges that additional design review for the two medical office buildings and the hospital is required, and that Landowner shall submit to the City for review and approval by the Planning Commission the design of the buildings and related structures that comprise the medical office buildings and/or hospital prior to construction of any permanent building. The approved building design(s) may be phased, but each phase shall be incorporated into the Design Guidelines that govern the design of the Medical Center.

c. **Section 4.1.1 – City Cooperation in Connection with State OSHPD Approval of Hospital Building Design and Review by Other Agencies.** In addition to the requirements of good faith cooperation and other provisions of Section 4.1 of the Development Agreement, the City acknowledges that Landowner will be required to obtain approval of the design of the hospital building by State OSHPD and will also be required to obtain approvals for components of the Medical Center (including but not limited to the heliport) from Caltrans, Sacramento County and other federal, state or regional agencies. This approval may result in required changes to, among other things, building structure, fenestration, awnings, set backs, and other physical features of the hospital building and/or layout of the Project on the Property. The City shall evaluate and process any such modifications pursuant to the MAM procedure in the Specific Plan if such modification qualifies to be processed through the MAM procedure, but reserves the right to process any such changes required by such agencies through the Planning Commission and/or the City Council, with appropriate notification to the approval body of the mandatory requirements imposed by such agencies.

d. **Section 4.6.1 – Water Supply.** The City, through approval of the Subsequent Entitlements, has made a finding pursuant to Folsom Municipal Code section 17.38.100(D) of the “availability of necessary public facilities including, but not limited to, water, sewage and drainage and the adequacy of the provision which the development makes for the furnishing of such facilities.” While the City retains the ability pursuant to Section 4.6 of the Restated Development Agreement to “address water shortages on a citywide basis,” the City acknowledges Landowner’s reliance upon the finding of the availability of necessary water to serve the Medical Center, as confirmed in the Addendum for the Project. The City based this determination upon the technical analysis supporting the Addendum regarding water demand for the Project and the conclusion from this technical analysis that the increased water demand estimated at 126 acre feet per year for the Project would remain within the 5,600 acre-feet per year available for the Specific Plan. A graphic included in the Addendum depicting the water supply for the Project is attached hereto as **Exhibit E**. City acknowledges that the estimate of water supply for the Project is now included in the baseline analysis of water usage for the Specific Plan. The City further acknowledges that Landowner will provide essential medical services throughout the Medical Center and that, in the event of a water shortage, the City will take into account the water requirements for essential medical services in any future action that may be necessary to address water shortages. Any disruption in water supply imposed by the State or the City that prevents Landowner from constructing any portion of the Medical Center shall provide a basis for an extension to the Term of this Amendment No. 2 for the same period that such disruption in water supply exists, subject to approval by the City Council.

e. **Section 4.6.2 – Water Infrastructure.** The City, through approval of the Subsequent Entitlements and consideration of a Technical Memorandum titled “PA Parcel 85A Zone Supplemental Analysis” dated April 28, 2021, provided in support of the Addendum, has determined that the water infrastructure for the Medical Center will be provided through “Zone 4,” as that Zone is identified in Figure 12.1 of the Specific Plan, and the landowner shall pay all costs and expenses for piping, and tank size expansion from 2 million gallons to 2.6 million gallons, and construction to connect water from Zone 4 to the Medical Center. On a temporary basis, until the completion of the Zone 4 infrastructure, the water infrastructure for the Medical Center will be provided through Zone 5, as that Zone is identified in Figure 12.1 of the Specific Plan. Landowner bears all costs and expenses to connect water from Zone 5 to the Medical Center. Unless improvements or connections to Zone 4 are solely attributable to the hospital uses included in the Project, Landowner shall only be required to provide a fair-share contribution towards the construction of the Zone 4 infrastructure and may elect to participate in any special assessment/special tax funding mechanisms that are established by the City. Landowner shall coordinate with developers of other projects that also use Zone 4 infrastructure to determine the amounts of their respective fair share contribution toward Zone 4 infrastructure.

f. **Section 4.10 – Commencement of Construction and Term of Planned Development Permit and Conditional Use Permit.** In light of the extended Term of this Amendment No. 2, the City agrees that the provisions of Folsom Municipal Code sections 17.38.110 and 17.60.060 related to expiration, revocation or abandonment of a Planned Development Permit or a Conditional Use Permit shall have no effect, and that the term of the Planned Development Permit and Conditional Use Permit are equal to the Term of this Amendment No. 2.

g. **Section 4.11 – Maintenance of Landscaping Frontage, Primary Gateway and Street Lights on Public Streets.** Subject to receipt of funding through a mutually agreeable financing mechanism, the City agrees to maintain the frontage landscaping improvements identified on **Exhibit F** and the streetlights on McCarthy Way and East Bidwell Street along frontage of the Property as identified on **Exhibit F**. Landowner acknowledges the identification of a "Primary Gateway" on Figure 2.2 of the Folsom Plan Area Community Design Guidelines and as further described in Section 2.2.1 of that document. City acknowledges that the installation of the Primary Gateway and maintenance of the Primary Gateway is included as an amenity in the Folsom Plan Area Community Design Guidelines and, as such, is not an individual obligation assigned to Landowner. City shall not require Landowner to bear more than a fair share of the cost of such design, installation and maintenance, and Landowner agrees to coordinate with developers of other projects in the Specific Plan to determine the amounts of their respective fair share contribution toward the design, installation and maintenance of the Primary Gateway. Notwithstanding the foregoing, City shall not be responsible for any costs to design, install, or maintain said Primary Gateway.

10. **Effect of Amendment.** This Amendment No. 2 amends, but does not replace or supersede, the Restated Development Agreement and Amendment No. 1. In the event of any conflict, the language of this Amendment No. 2 shall be controlling in all events or circumstances. Except as modified hereby, all other terms and provisions of the Restated Development Agreement and Amendment No. 1 shall remain in full force and effect.

11. **Section 7.8 – Notices.** All notices required by the Development Agreement or this Amendment No. 2 as such requirements relate to the Property or the Subsequent Entitlements, or the enabling legislation or the procedure adopted pursuant to Government Code section 65865 shall be as provided for in Section 7.5 of the Development Agreement, with the substitution for Landowner as follows:

CommonSpirit Health
3200 N. Central Avenue, 23rd Floor
Phoenix, AZ 85012
Attention: System Senior Vice President, National Real Estate Services

With copies to:

CommonSpirit Health
3400 Data Drive
Rancho Cordova, CA 95670
Attention: National Real Estate Services

And

CommonSpirit Health
3200 N. Central Avenue, 23rd Floor
Phoenix, AZ 85012
Attention: Legal Team

12. **Form of Amendment – Execution in Counterparts.** This Amendment No. 2 is executed in duplicate originals, each of which is deemed to be an original, and may be executed in counterparts.

IN WITNESS WHEREOF, the City of Folsom has authorized the execution of this Amendment No. 2 in duplicate by its Mayor and attested to by the City Clerk under the authority of Ordinance No. [] adopted by the City Council on the ___ day of _____, 2021.

CITY:

CITY OF FOLSOM
a municipal corporation

Michael Kozlowski, Mayor

APPROVED AS TO CONTENT:

Elaine Andersen, City Manager

APPROVED AS TO FORM:

Steven Wang, City Attorney

ATTEST:

Christa Freemantle, City Clerk
[Notary Pages to be Added]

LANDOWNER:

DIGNITY HEALTH
a California nonprofit public
benefit corporation

By: _____
Its: _____

APPROVED AS TO FORM:

Martha Clark Lofgren, Brewer
Lofgren LLP

EXHIBIT LIST

- A-1** Legal Description of the Property
- A-2** Depiction of Parcel 1 on Map
- B** Approved Site Plan
- C** Map Depicting Potential Future Signalized Access to Medical Center
- D** Improvements and Mitigation Measures to be Completed by Predecessor's Affiliate
- E** Project Water Supply
- F** Map Depicting McCarthy Way Lighting, Landscaping Frontage and Tentative Location of Gateway and Monument Signs

EXHIBIT A-1

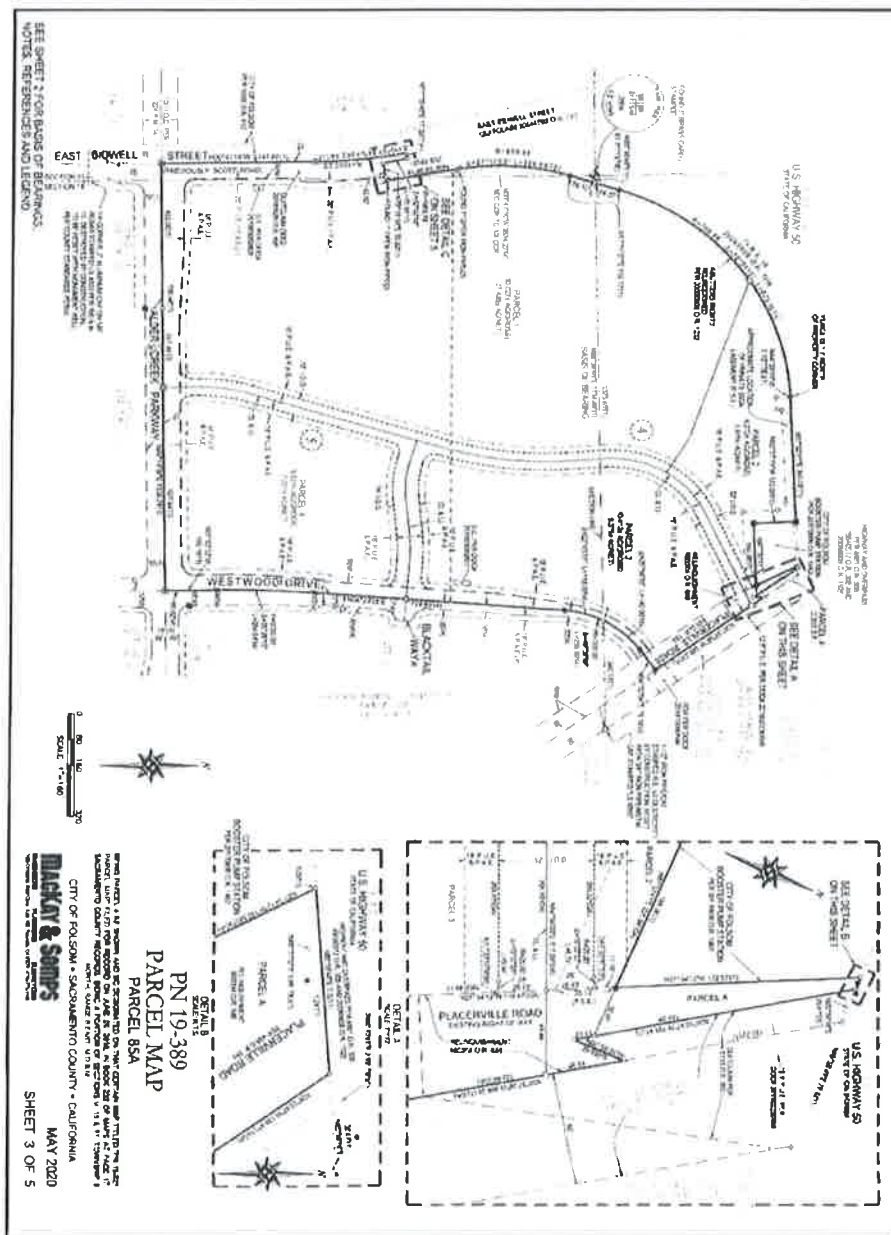
LEGAL DESCRIPTION OF PROPERTY

Situated in the City of Folsom, County of Sacramento, State of California and more particularly described as follows:

Parcel 1 of "PN 19-389 Parcel Map Parcel 85A", filed for record May 22, 2020, in Book 240 Page 13 of Parcel Maps, Sacramento County Records.

APN: 072-3190-046 (portion)

EXHIBIT A-2
DEPICTION OF PARCEL 1 ON RECORDED PARCEL MAP



240-13-3

Exhibit A-2

[SAC-214573]

60.765-3375345.4

EXHIBIT B SITE PLAN

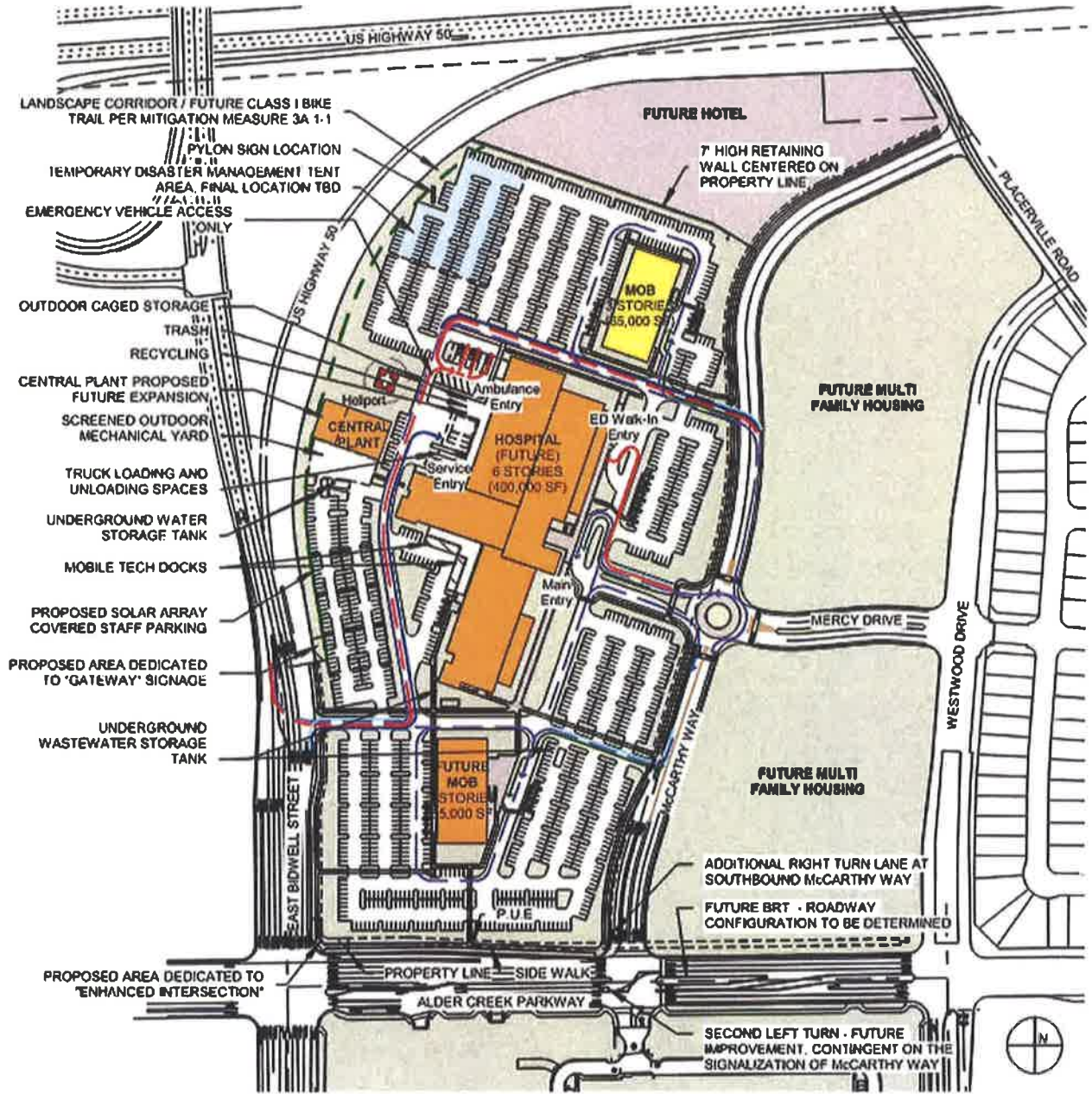


EXHIBIT B - SITE PLAN

Legend Follows on Next Page



Exhibit B

[SAC-214573]

60.765-3375345.4

**EXHIBIT B
SITE PLAN, CONTINUED**

PHASE LEGEND

-  PHASE 1
-  FUTURE PHASE

CIRCULATION PATHS

-  FIRE ACCESS
-  SERVICES / MATERIALS
-  ED WALK-IN
-  AMBULANCE
-  VISITORS / PATIENT
-  EMPLOYEES

PROJECT INFORMATION

FUTURE: Acute Care Hospital = 400,000 SQFT
Medical Office Building = 130,000 SQFT

FUTURE TOTAL SQFT: 530,000 SQFT (OCCUPIED)

TOTAL DEVELOPMENT ACREAGE: 27.44 acres

OVERALL SITE ACREAGE: 31 acres

TOTAL INPATIENT BEDS: 300+ beds

PROPOSED BUILDING HEIGHT = 120' (6 LEVELS)

PROPOSED MAX FAR: .55

EXHIBIT B – SITE PLAN LEGEND

EXHIBIT C
FUTURE POSSIBLE SIGNALIZED ACCESS

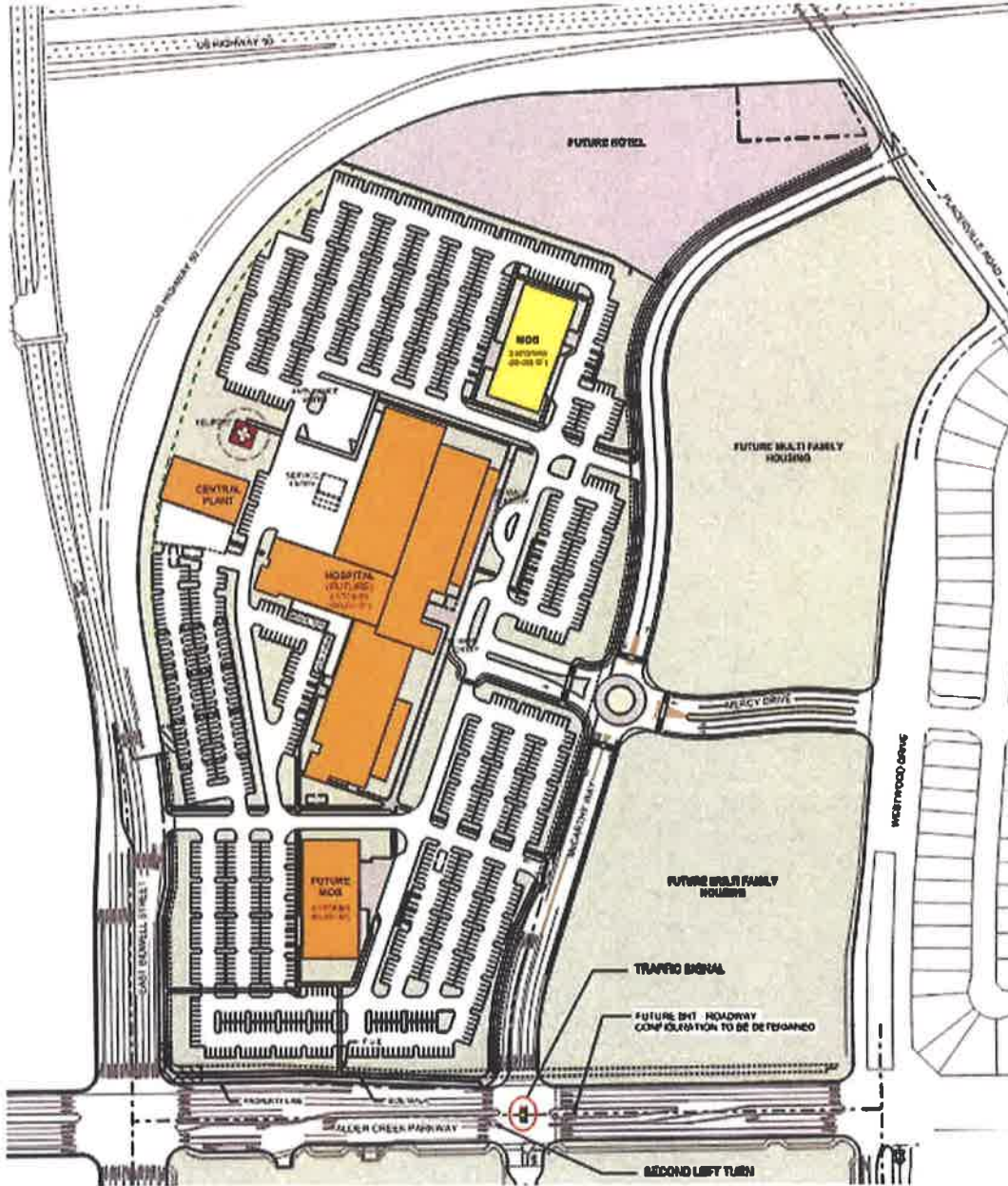


EXHIBIT C - FUTURE POSSIBLE SIGNALIZED ACCESS

**EXHIBIT D
SITE IMPROVEMENTS AND MITIGATION
MEASURES TO BE COMPLETED BY
PREDECESSOR'S AFFILIATE**

1. Mass grading of Parcel 85A and adjacent roadways resulting in contour graded developable parcels and subgrade along roadway alignments.
2. Rough grading of a portion of Hydromodification Basin #8 (HMB#8) including the access road along the Savannah Parkway alignment and perimeter of the basin.
3. Rough Grading of a storm drain outfall swale from the western terminus of Alder Creek Parkway to the existing downstream waterway.
4. Excavation at a borrow site west of East Bidwell.
5. Roadway and Utility improvements along East Bidwell, Alder Creek Parkway, Westwood Drive, Placerville Road, McCarthy Drive and Mercy Way, including storm drain, sanitary sewer, potable and non-potable water, and dry utility infrastructure.
6. HMB#8 improvements including a paved access road to East Bidwell and basin outlet control structure and spillway.

[Reference to Mitigation Measures Pending]

EXHIBIT E PROJECT WATER SUPPLY, INCLUDING PARCEL 1 OF PARCEL 85A

Folsom Plan Area
Folsom Ranch Medical Center
Potable Water Demand Chart

Date: 02/20/21

Water Supply Agreement - 6,000 AFY

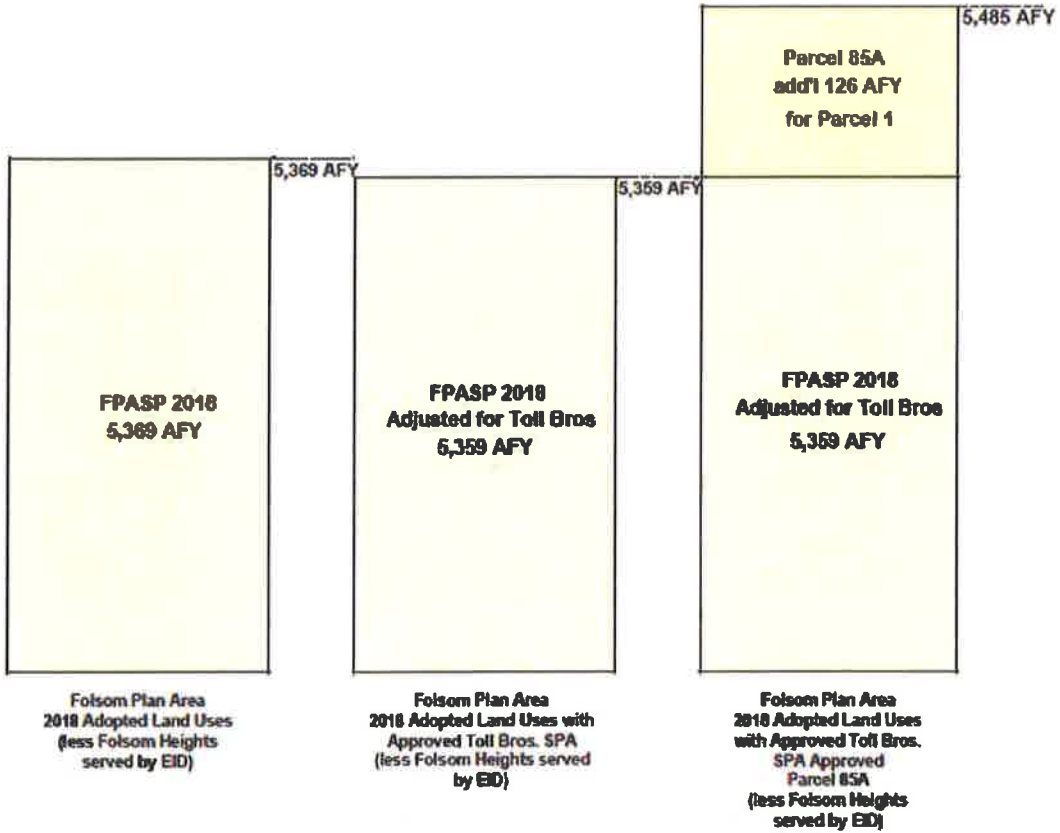
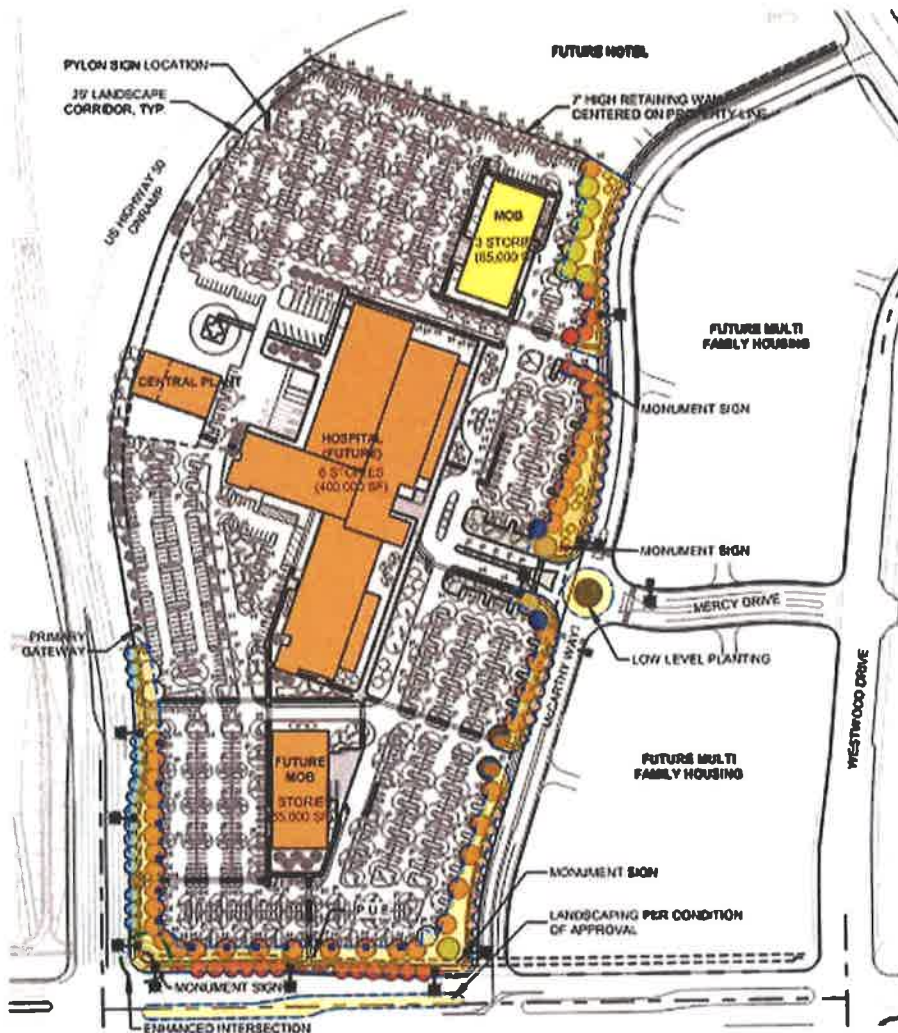


EXHIBIT F STREET LIGHTING AND LANDSCAPING FRONTAGE



- ▲ 1 PRIMARY GATEWAY BY OTHERS (MAINTENANCE TO BE ADDRESSED BY OWNER'S GROUP)
- 4— MONUMENT SIGN (LANDOWNER RESPONSIBILITY)
- 12 STREET LIGHTING (MAINTENANCE TO BE ADDRESSED BY SEPARATE AGREEMENT)
- 12 FLOOD LIGHTING FOR SITE SIGNAGE (LANDOWNER RESPONSIBILITY)
- 32 LIGHT POLE / AREA LIGHT (LANDOWNER RESPONSIBILITY)
- LANDSCAPE AREA - TBD (MAINTENANCE TO BE ADDRESSED BY SEPARATE AGREEMENT)
- () ENHANCED INTERSECTION (MAINTENANCE TO BE ADDRESSED BY SEPARATE AGREEMENT)

NOTE: PLANT TYPES AND QUANTITIES ARE SHOWN IN THE PD GUIDELINES

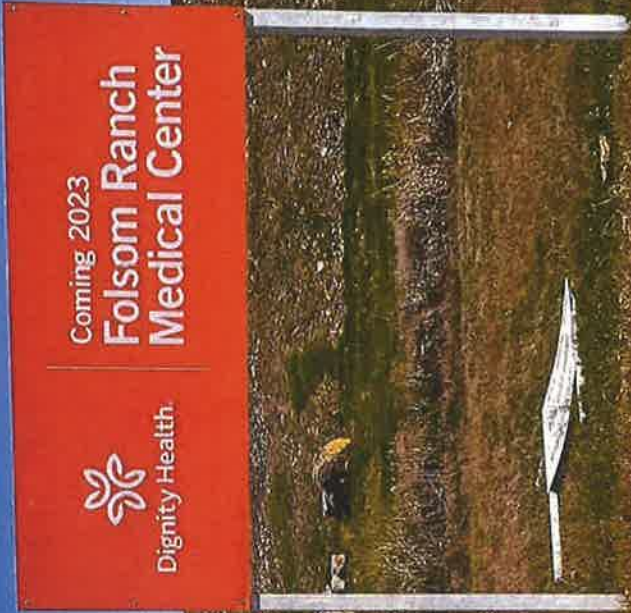


EXHIBIT F - STREET LIGHTING AND LANDSCAPING FRONTAGE

Planning Commission
Folsom Ranch Medical Center
May 19, 2021

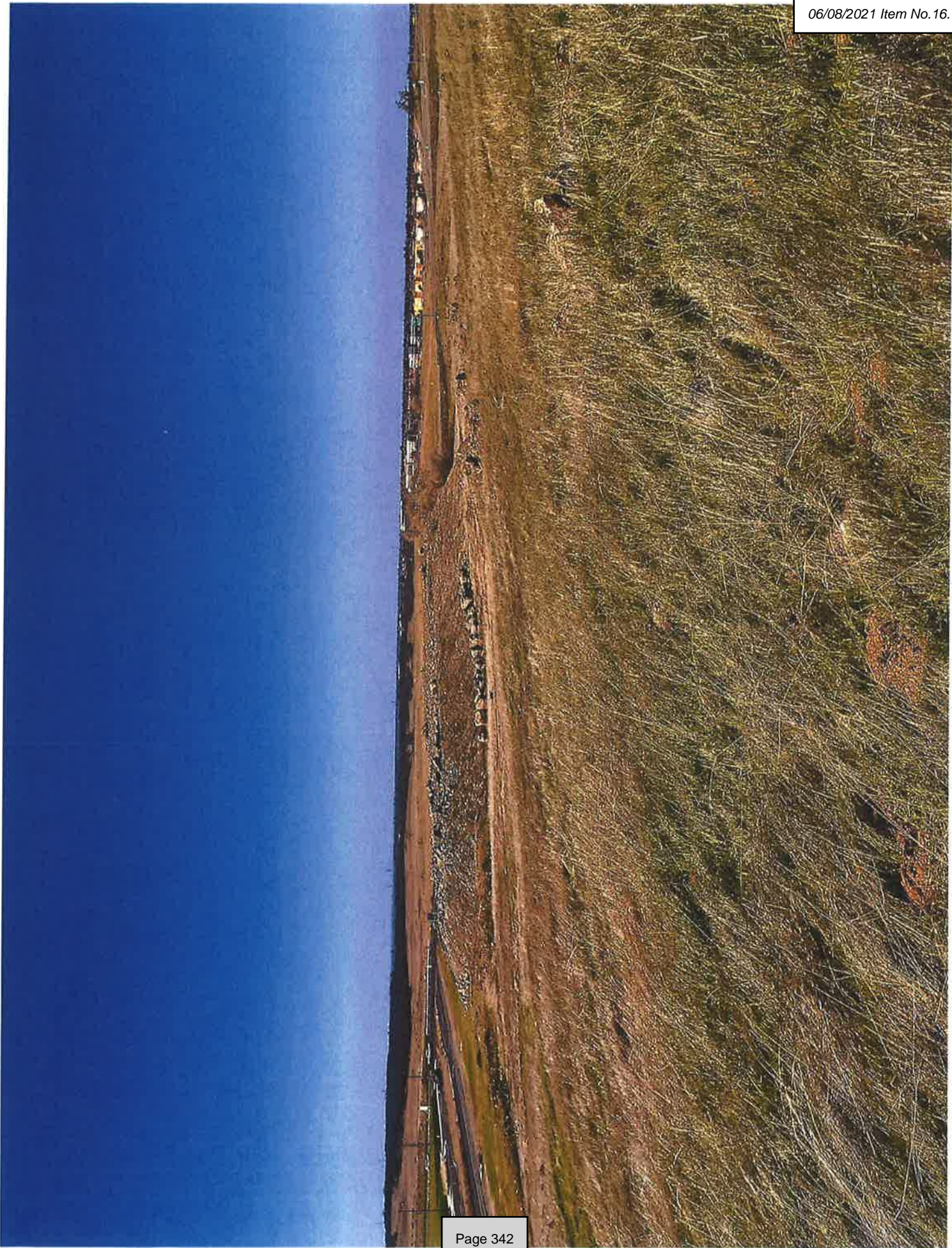
Attachment 17

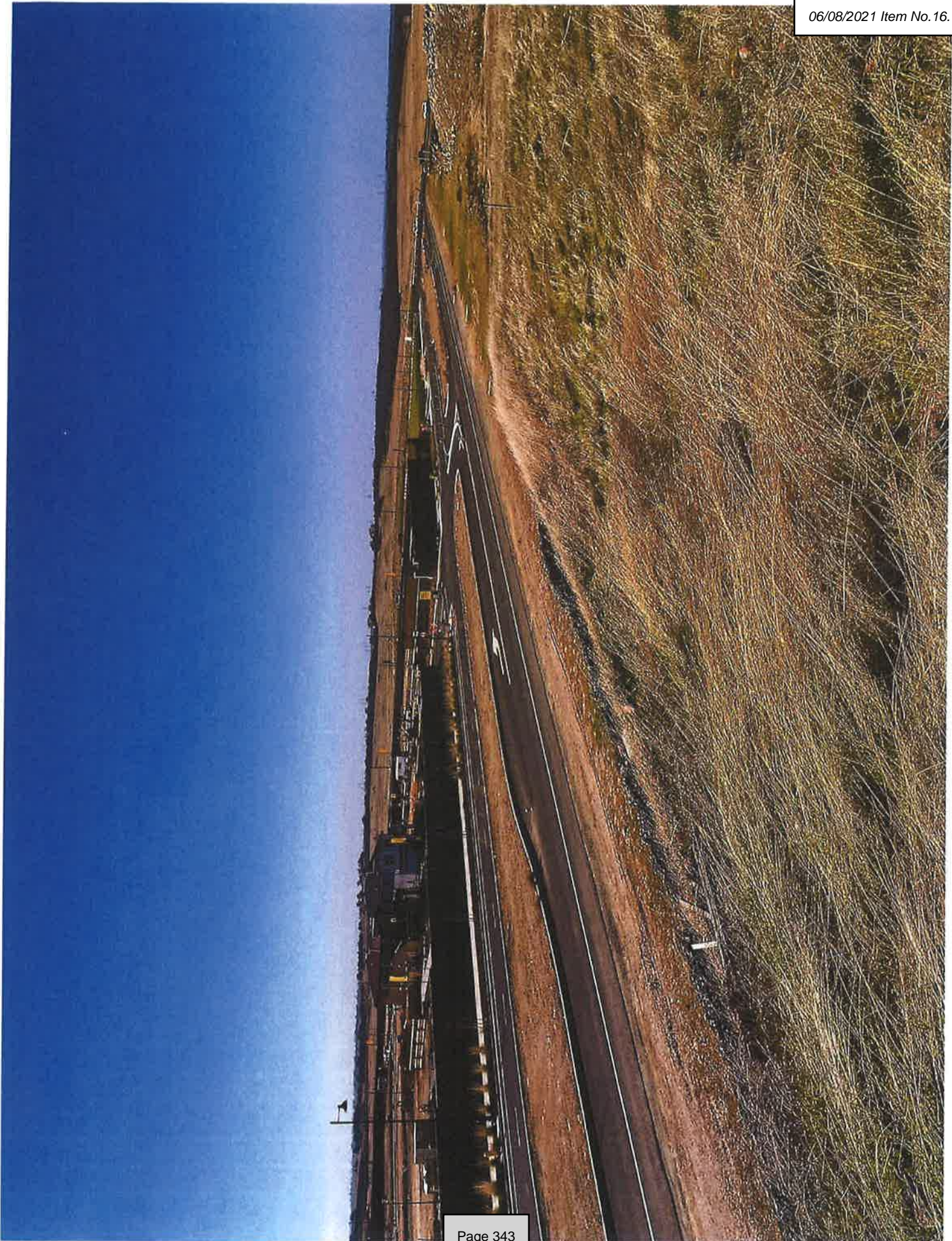
Site Photographs



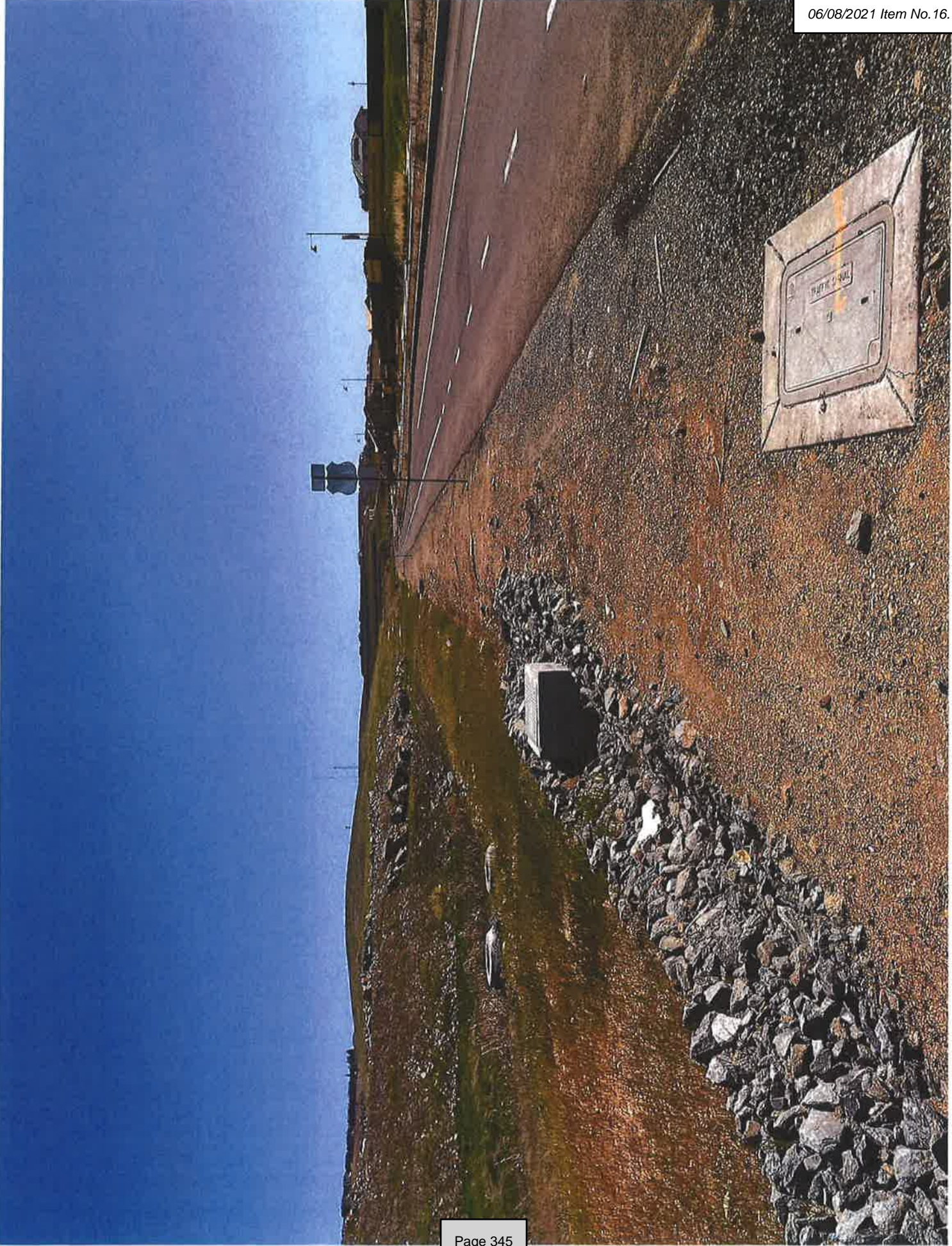












Planning Commission
Folsom Ranch Medical Center
May 19, 2021

Attachment 18

City Memorandum Regarding Folsom Ranch Medical Center Site Access Dated September 30, 2020

Memo



CITY OF
FOLSOM

To: Pam Johns, Community Development Director

From: Dave Nugen, Public Works Director

Date: September 30, 2020

Subject: Dignity Health Project site access (Alder Creek/McCarthy signal)

Over the past few months our staff has had the opportunity to work with the Dignity team to evaluate their request for a traffic signal at the intersection of Alder Creek Parkway and the future McCarthy Way. While we appreciate the efforts that have been put forth to evaluate the signal, based on the limited information available at this time we remain extremely concerned with the addition of a signal at this location. Given the current level of development in the Folsom Plan Area Specific Plan (FPASP) a traffic signal at this location seems premature and could significantly limit our opportunities to operate our signal system in an efficient manner. While we are not completely foreclosed on the concept of a signal at this location, we would advise that either the signal could be conditionally approved based on a longer-term buildout of the FPASP, or that other forms of traffic control be considered and evaluated at this location, such as a roundabout.

Background

Our concerns are based on several factors, generally having to do with the timing of the signal with respect to development in the FPASP, deviating from normal traffic signal design standards, and the concessions that would have to be made in order to accommodate a signal at the requested location. The following bullet points are intended to summarize our concerns.

1. **Timing of this development with respect to land use growth and backbone transportation infrastructure in FPASP**
 - East Bidwell Street will carry the majority traffic originating in, or passing through the FPASP until the Empire Ranch interchange, Oak Avenue interchange, and Rowberry overcrossing are constructed; this condition will

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likely exist for several years due to a lack of funding to complete those major capital projects

- The Capital Southeast Connector will be constructed as a four-lane expressway from Prairie City to East Bidwell by 2022; the remaining portion from East Bidwell to the El Dorado County line will not be a high funding priority for the foreseeable future, therefore East Bidwell Street will function as the de facto eastern end of the Connector for several years, adding more traffic to the corridor
- It is too early in the FPASP development to make such significant changes to the planned traffic signal system without a full understanding of how traffic will flow through this area until the FPASP approaches buildout conditions
- To date the traffic modeling prepared by the applicant's traffic consultant has focused on the immediate area surrounding Alder Creek/McCarthy; staff has not yet seen any detailed analysis of the project impacts on the surrounding transportation network, including the traffic signals along the East Bidwell corridor (including the freeway ramps and north of 50), and the Alder Creek corridor (from Empire Ranch Rd to Prairie City Rd)

2. Design Standards and Signal Operations

- Standard traffic signal spacing is one signal per every quarter mile (about 1,300 feet) to minimize the chances of one signal's operations (queueing, delay) negatively impacting an adjacent signals and intersections
- The Dignity proposal would result in signal spacing of only 600 feet on Alder Creek between Westwood and East Bidwell
- According to Community Development Department staff, they have been approached by the developers on the other side of East Bidwell who are also asking the City to consider sub-standard signal spacing, which would result in sub-standard signal spacing on both sides of the East Bidwell corridor
- Sub-standard signal spacing can be allowed in some cases, such as in the Central Business District or Historic District, but those areas will not be carrying the same volume of traffic that we can expect within a quarter mile of the freeway.
- The Shops project, located in the southeast corner of East Bidwell/Alder Creek has been approved with two, right-turn only driveways on Alder Creek Parkway, just east of East Bidwell Street. Staff is concerned that, if a signal is approved at Alder Creek/McCarthy, that traffic exiting those driveways (including gas

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trucks from the approved gas station in the Shops), would attempt to cross several lanes of Alder Creek Parkway to make a u-turn at McCarthy in order to travel towards East Bidwell Street. Due to the inadequate signal spacing, this movement would be a potential safety concern. Staff would prefer that traffic performing this movement should do it at the Westwood/Alder Creek signal in order to have adequate room to safely cross the eastbound lanes in order to perform a u-turn movement.

- In order to accommodate the turn pocket length for the eastbound left at McCarthy, the applicant proposes to limit the westbound left at the East Bidwell intersection to 150 feet of storage, which is less than our preferred standard (250 to 300 feet); this increases the potential to have traffic queues extending out of that turn pocket and blocking through lane access. There is also the potential safety concern of traffic traveling at the posted speed limit on Alder Creek not having sufficient stopping distance entering the turn pocket depending on the length of the left turn queue (which hasn't been noted in any memos provided by the consultant)
- The consultant has suggested that the Alder Creek/McCarthy signal could work if it is synchronized/coordinated with the East Bidwell/Alder Creek signal. If this coordination is necessary in order to make the McCarthy signal work, then this may create an unacceptable concession for us to make as it may limit our abilities to coordinate or time other signals along the East Bidwell corridor.
- We are currently in the process of upgrading/modernizing our traffic signal controllers to create "Smart" corridors, and this equipment will be the standard for all new traffic signals in the FPASP; however we do not believe that "smart" signal technology alone will be able to manage the traffic congestion and delay that we are anticipating along the East Bidwell corridor.

3. Impacts to the future Bus Rapid Transit (BRT) system in the FPASP

- The master plan currently shows the BRT route along Alder Creek Parkway between Westwood and East Bidwell and it is assumed that there would be a bus stop in that segment
- The cross sections in the adopted FPASP for both Alder Creek Parkway and Westwood Drive include dedicated BRT lanes in the median, and therefore the bus stops would also be located in the median

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- In order to accommodate the required left turn storage for a signal at Alder Creek/McCarthy, the eastbound BRT would have to be moved to the shoulder and the station out of the median; this would also impact the BRT operation to the west (where the BRT would have to transition out of the median) and to the east (where BRT would have to transition back into the median)
 - Sacramento Regional Transit is now the transit provider for the City of Folsom and at this time does not have this BRT system in their long-range transit plan, nor do they currently operate any BRT lines in their system so it is unclear what their concerns/applicable standards might be
- 4. Public emergency access is already planned along East Bidwell Street and provides direct access to the proposed Emergency Department**
- The applicant has stated that a signal at Alder Creek/McCarthy is necessary to facilitate emergency access but has not demonstrated why this is important when the proposed project already includes a driveway on East Bidwell between US50 and Alder Creek which enters the site in closer proximity to the Emergency Department. This driveway can be accessed by motorists approaching from any direction (pending design that has yet to be provided by the applicant)
 - If the site access was not allowed at Alder Creek/McCarthy, but instead at Alder Creek/Westwood (as is currently designed), the additional travel time into the hospital site would seem reasonable given that there are other emergency access options.

Conclusion:

Based on the limited analysis conducted to date, the Public Works Department is not prepared to support a traffic signal at the Alder Creek/McCarthy intersection at this time. It is our recommendation that the project should consider and evaluate the following access scenarios.

Scenario 1 (Preferred): Alder Creek/McCarthy intersection should be designed to operate without traffic controls and would be limited to north/south right turns in/out only at this time, but could potentially also include east/west uncontrolled left turn lanes until such time that traffic conditions become unsafe or result in excessive delay

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- a. The Alder Creek/McCarthy intersection can be re-evaluated for future traffic control only after the FPASP is at least 75% built out and if traffic conditions at that time warrant further consideration
- b. The City will work with the applicant on an extensive way-finding program to guide hospital traffic from US 50 to Alder Creek Parkway and on to Dignity Way using a combination of street banners, pavement markings and signage; this could also include re-naming streets such as Westwood Drive north of Alder Creek to aid in the wayfinding strategy
- c. The intersections of Westwood/Dignity and Dignity/McCarthy should be evaluated at roundabouts to facilitate access to the hospital while minimizing delay.

Scenario 2 (Alternative): Alder Creek/McCarthy intersection evaluated as a 2x1 roundabout.

- a. A conceptual design should be required to demonstrate that a roundabout would not negatively impact geometry at adjacent signals and any potential right of way impacts
- b. Some analysis/design should be included to demonstrate how BRT line and stations could be accommodated within or adjacent to the roundabout
- c. As with scenario 1, the intersection of Dignity/McCarthy should be evaluated as a roundabout.

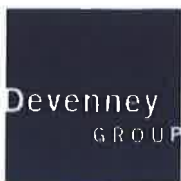
My staff and I remain committed to assisting your staff and the applicant in finding a mutually acceptable solution to this issue. If you have any questions, please let us know.

cc: S. Krahn, CDD
S. Johnson, CDD
S. Banks, CDD
M. Rackovan, PW
Z. Bosch, PW

Planning Commission
Folsom Ranch Medical Center
May 19, 2021

Attachment 19

Devenny Group Traffic Scenarios Memorandum Dated October 26, 2020



HEALTHCARE PLANNING AND ARCHITECTURE SINCE 1962

October 26, 2020

Robert O'Hare
 Manager, Planning | Design | Construction
 National Real Estate Services

CommonSpirit Health[®]
 10901 Gold Center Drive, Suite 300
 Rancho Cordova CA 95670

Traffic Scenarios Memorandum

Project: Folsom Ranch Medical Center
 DGL No.: 18300.00

Dear Robert:

As part of the Dignity Health Folsom Ranch Medical Center master planning effort, three traffic scenarios for campus access were developed and analyzed for Alder Creek Parkway between East Bidwell Street and Westwood Drive.¹ The following scenarios were modeled:

- Scenario 1 - Signalized left at Alder Creek Parkway and McCarthy Way
- Scenario 2 - Roundabout at Alder Creek Parkway and McCarthy Way
- Scenario 3 - Left turn at Westwood, with main entrance to campus at Mercy Dr; eastbound left turns from Alder Creek at McCarthy prohibited; right in/ right out only for McCarthy Way at Alder Creek

The consultant team has collaborated on this memorandum to summarize the analysis for each scenario so that the technical information can be utilized to brief leadership both at Dignity Health and the City of Folsom. The documentation for each scenario includes the following information:

- Summary of the traffic control components along with phasing considerations,

¹ DKS Associates provided pre-application traffic analysis, with the concurrence of City staff. All model inputs (including baseline and future year AM/PM peak hour volume sets, intersection geometrics and signal timings, access characteristics, and operational outputs) are documented in the following technical memoranda: *Cumulative Volumes and Trip Generation Memo* (DKS, July 30, 2020); *VISSIM Model Assumptions and Queue Results* (DKS, August 26, 2020); *Scenario Review and Key Discussion Points* (DKS, September 11, 2020). All analysis inputs and outputs were reviewed and accepted by the City of Folsom and will be incorporated into the environmental analysis for the Planned Development Permit.



- Discussion of any potential operational concerns, including any specific points mentioned by City staff, and
- Review of turning analysis for trucks that will need to navigate a path from the adjacent fueling station.

Given that the City has also expressed concerns regarding current operations at closely spaced signals at other locations within Folsom, this memorandum also provides a short discussion contrasting the identified locations with the study segment of Alder Creek Parkway. Finally, the memorandum identifies observable thresholds of operation for each scenario that would indicate that a proposed traffic control is not operating as predicted. The analysis then identifies solutions for consideration that may ultimately be incorporated into terms of an Amendment to the Development Agreement.²

Summary of Conclusions

Left turn access into the Folsom Ranch Medical Center at the intersection of Alder Creek Parkway and McCarthy Way is a priority for Dignity Health to facilitate non-emergency patient and visitor access as well as employee access. The proposed signal spacing (*i.e.*, 700 feet) to facilitate this left turn movement does not meet the City's minimum spacing standards. However, as required by the City, Dignity Health commissioned a Progression Study for pre-application site planning. This traffic analysis demonstrates that if the signals at East Bidwell/Alder Creek Parkway and Alder Creek Parkway/McCarthy Way are coordinated, queuing will not occur beyond acceptable City thresholds. Bus Rapid Transit (BRT) may also be accommodated successfully in the median lane, and also functions to an acceptable standard if located on right lanes.

In addition, and by way of comparison, the proposed 700-foot signal spacing is a farther distance than other traffic signal spacing noted in the City. For example, signals are 500 feet apart at the intersections of Broadstone Parkway/ Marketplace/Cavitt Drive, two similar intersections located near another section of East Bidwell, with high-traffic commercial on one side and a high density multi-family project across the street.

By contrast, the City's preferred option (*i.e.*, signalized left turn at Alder Creek Parkway/Westwood Drive) will result in increased traffic delays during certain hours that barely meet City's maximum delay limits. Traffic modeling indicates that afternoon and evening departures from the medical campus will be adversely affected, as traffic queues develop along McCarthy Way and extending up to Mercy Drive, blocking vehicles from entering or exiting the medical campus and adjacent multi-family and commercial parcels.

Roundabouts offer a possible alternative to facilitate and control left turn movements from Alder Creek Parkway into the medical campus. Early coordination with adjacent property owners and facilitated by

²Proposed language for a development agreement is deferred pending approval by Dignity Health leadership of the preferred option.



the City will be required to obtain right of way at no cost. Moreover, given the expense of installation of this traffic control measure, it should be considered permanent, with no financial exposure to Dignity Health if results become unsatisfactory in the future.

For each scenario discussed below, large truck movements from a nearby gas station at the intersection of East Bidwell and Alder Creek Parkway also have been considered in traffic modeling analysis. The analysis establishes that truck traffic can be accommodated, in part by prohibiting u-turns at the Alder Creek Parkway/McCarthy Way intersection. Any inconvenience to commercial vehicles is outweighed by the importance of direct access to the medical campus.

Background – Phased Construction and Anticipated Traffic Control Installation Timeline

The following analysis is provided in the context of the phased development for the Folsom Ranch Medical Campus. The anticipated phasing for the medical center is seven phases, as follows:

Phase 1 – Medical Office Building 2023

Phase 2 – Hospital (100 Beds) 2028

Phase 3 – Medical Office Building 2030

Phase 4 – Hospital Support Services (Radiology, Surgery, Pharmacy, Emergency, or Lab) 2031

Phase 5 – Hospital Expansion (100 Beds) 2034

Phase 6 – Hospital Support Services (Radiology, Surgery, Pharmacy, Emergency, or Lab) 2040

Phase 7 – Hospital Expansion (100 Beds) 2045

If approved by the City, the traffic signal installation is not likely to occur any sooner than Phase 5, in 2034, which is the second expansion of the hospital. Alternatively, a roundabout would be constructed within the first phase.

The existing site condition includes a median, which allows for an unprotected left turn. Under the City's preferred scenario (i.e., Scenario 3) this median presumably is removed, with no additional traffic controls added at Alder Creek Parkway/Westwood Drive. The analysis below identifies the operational flaws with this scenario.

This table summarizes the timing for improvements under the three scenarios:



PHASE	SCENARIO 1: SIGNAL	SCENARIO 2: ROUNDBABOUT	SCENARIO 3: NO LEFT TURN
PHASE 1 (2023)	Interconnect Installed	Roundabout installed	Interconnect installed and existing left-turn concrete median removed to be replaced with full median
PHASE 2 (2028)	No change <i>(Ped signal installed if BRT implemented)</i>	No change	No change <i>(Ped signal installed if BRT implemented)</i>
PHASE 3 (2030)	No change <i>(Ped signal installed if BRT implemented)</i>	No change	No change <i>(Ped signal installed if BRT implemented)</i>
PHASE 4 (2031)	No change <i>(Ped signal installed if BRT implemented)</i>	No change	No change <i>(Ped signal installed if BRT implemented)</i>
PHASE 5 (2034)	Signal Warrant met and signal with double eastbound left-turn pockets installed	No change	No change <i>(Ped signal installed if BRT implemented)</i>
PHASE 6 (2040)	No change	No change	No change <i>(Ped signal installed if BRT implemented)</i>
PHASE 7 (2045)	No change	No change	No change <i>(Ped signal installed if BRT implemented)</i>



Scenario 1 - Signalized left at Alder Creek Parkway and McCarthy Way

This scenario includes the following components:

- McCarthy signal coordination with the signal at Bidwell Street and Alder Creek Parkway
- Double left turn lanes at Alder Creek for the eastbound approach
- No left turns allowed to exit from the Enclave development to the south of McCarthy
- BRT, when implemented, will be left running along the median heading westbound and in the right lane with mixed traffic running eastbound



Potential Operating Concerns

- No operational or queuing concerns shown in full buildout model
- Spacing between signals (700 ft) is less than the City of Folsom standard (see Exhibit A-Standard Construction Specifications Design Standards)
- There are other locations in the City with similarly spaced signals; these locations have circulation issues that are unique to the land use and operations

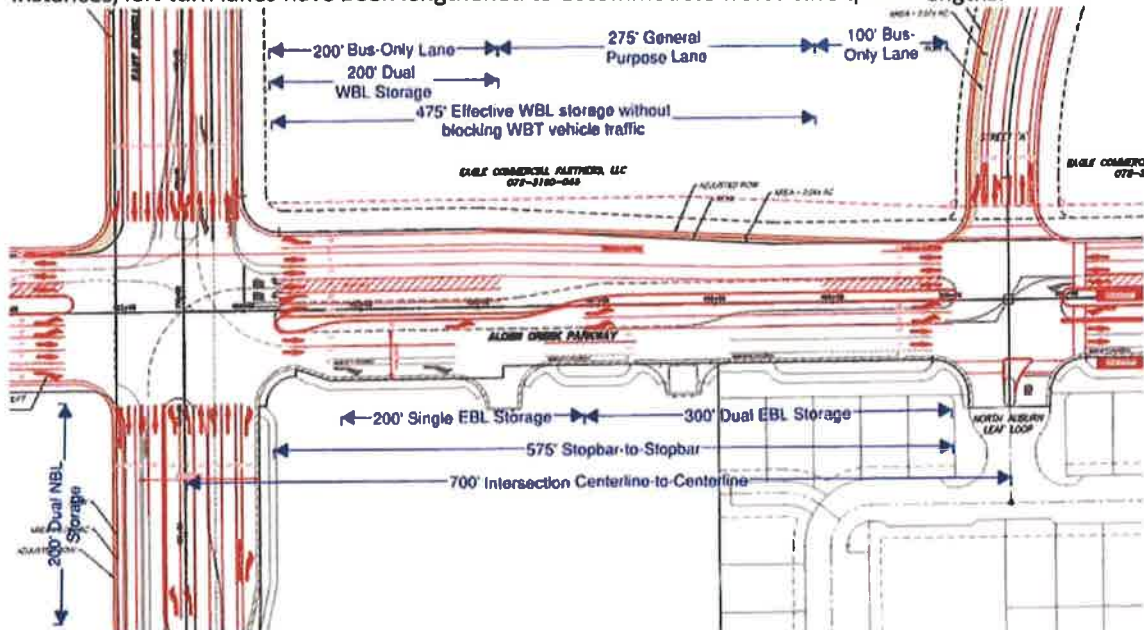


City Identified Concerns and Responses

- Shortened left-turn lanes could result in queue spill-back and/or lane-starvation conditions

Response:

The proposed design does not include any shortened or competing left turn lanes. In some instances, left turn lanes have been lengthened to accommodate worst-case queue lengths.



- Signal spacing is closer than minimum spacing required by City in Standard Construction Specifications Design Standards (i.e., spaced closer than $\frac{1}{4}$ of mile, or 1,320 feet apart); see Exhibits A, B and C. (Relevant excerpts of the Design Standards are attached as Exhibits A, B and C at the end of this memorandum.)

Response:

- Pursuant to City guidelines, proposed signal spacing of less than 1,320 feet requires the development of a Progression Study to verify that satisfactory operating conditions can be achieved. A Progression Study was developed based on City-approved volume sets and state-of-the-art micro-simulation analysis tools. Progression Study findings determined that no queuing or operational issues would result from implementation of Scenario 1.
- Precedents of similarly spaced signals that operate without queuing issues are found throughout the City. Counterexamples cited by the City – namely the In & Out Burger adjacent to Placerville Road should not be considered an appropriate analogy given the operational and design differences along Bidwell Street. Traffic consultants believe, based upon knowledge of City records, that none of the counterexamples cited by the City – including the In & Out Burger – had a Progression Study performed to determine the operational performance of the proposed signal spacing prior to approval/construction. Additional details and examples are provided in a following section of this document.



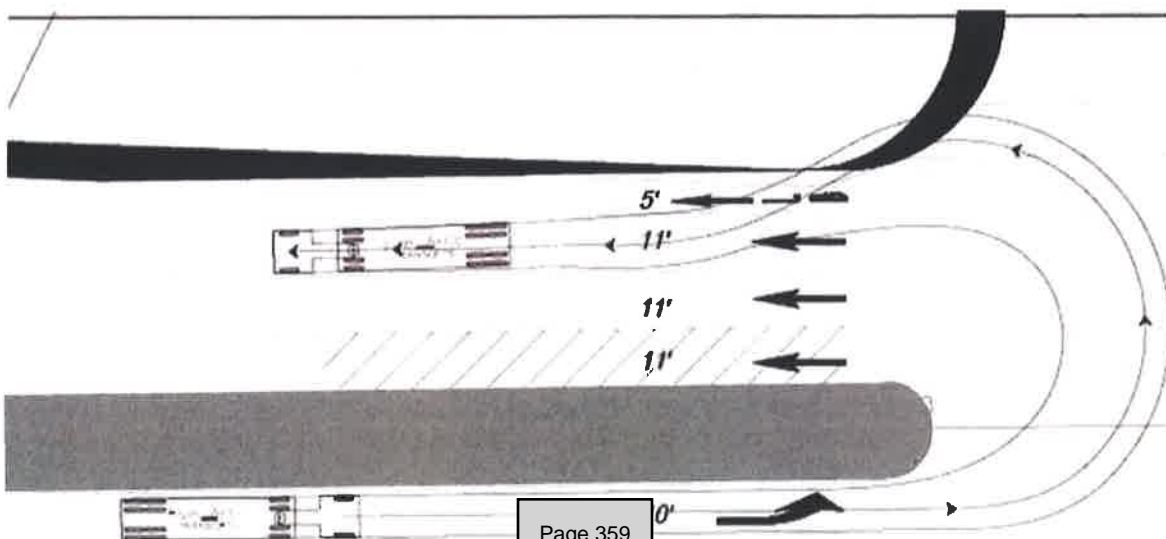
- Having BRT run eastward on the shoulder in mixed flow will cause design and operational issues further west and is not supported by the Specific Plan

Response:

- Implementation of the BRT will fall under SacRT and the design and operation will ultimately fall under their jurisdiction. The Progression Study demonstrated that a BRT service line can operate acceptably under Scenario 1 (i.e., with the proposed signal) as either a center lane running line or outside general purpose lane running service. SacRT has no fleet composition policy regarding left-door or right-door BRT bus vehicle types. As such, both center lane or outside general purpose lane BRT service can be considered as a viable option.
- The Specific Plan includes language supporting running the BRT in mixed flow both in general language for the Specific Plan transit in the Folsom Plan Area Specific Plan Section 7 Circulation Signature Corridors 7.6. See Exhibit E. Figure 7.3 shows the median as a Future Transit Corridor with two lanes designated as Future Travel Lanes. .
- If necessary, the bus queue jump proposed for Alder Creek at Bidwell could allow for the bus to shift from center running to shoulder running safely and efficiently.
- The City has had to “fix”, at their own cost, operational issues at other closely spaced signals.
 - Dignity is evaluating, as part of the future Amendment to Development Agreement, to identify operational thresholds, that if met, will trigger design and operational changes to be paid for by Dignity.

Potential Impacts on Truck Turning Analysis at Westwood Drive

- Planned fuel station located on Alder Creek between Bidwell and McCarthy, at the southeast corner of Alder Creek and Bidwell
- Fuel trucks will have to turn onto Alder Creek and make a U-turn to return to the freeway
- U-turns would be prohibited at McCarthy
- One of the potential truck types would need additional room at the northwest corner of Alder Creek Parkway and Westwood Drive (see following truck u-turn diagram)





Comparison to other closely spaced signalized intersections

The City has identified two other existing conditions where closely spaced signals have resulted in operational problems and queue spill-back. However, the surrounding land use and operations for both differ significantly from the study segment at Alder Creek. These characteristics are described in greater detail as follows:

E Bidwell Street and Placerville Road (Existing):

- Signals on Bidwell Street at Iron Point Road and Placerville Road are 500 ft apart
- Intersection at Placerville/Bidwell is the only entrance/exit for high-volume/turnover varied commercial land uses (In-N-Out Burger, Starbucks, and Chevron)
- The left-turn pockets for northbound Bidwell at Iron Point and southbound Bidwell at Placerville use the same center lane, meaning that the left-turn pocket at Iron Point was shortened to make room for the left-turn pocket at Placerville
- Vehicles must make a U-turn at Iron Point Rd to access the freeway ramp
- The reduced space for left- and u-turning vehicles at Iron Point Road was causing queue spill-back issues on Bidwell. The City recently restricted U-turns at Iron Point Rd causing 1-3 min detours to access the ramp
- *Side note:* There are four signals continuing along Bidwell for the Palladio shopping center and adjacent streets all spaced about 800 ft apart

E Bidwell Street and Montrose Drive (Existing):

- Signals on Bidwell Street at Montrose Drive and Blue Ravine Road are 800 ft apart
- Between the two signals there are five driveways for varied commercial land use allowing uncontrolled access to the segment
- Two of the driveways are served by midblock left turn lanes allowing vehicles to cross Bidwell traffic
- There are many opportunities for short queues to block traffic or access points

Alder Creek Parkway and McCarthy Way (Proposed as Scenario 1):

- Signals on Alder Creek at Bidwell Street, McCarthy Way (proposed) and Westwood Drive (planned) are 700 ft apart
- There are two primary movements that would produce queues (eastbound left [AM] and southbound right [PM])
- Queues would be managed with coordination between Bidwell and McCarthy signals
- There are only two right-in/right-out driveways between Bidwell and McCarthy on Alder Creek and both have more convenient exit opportunities on Bidwell
- Avoids u-turns for primary movements



- Signalized intersection is to serve drivers heading towards one single shared destination of the Medical Center, rather than a commercial development with mixed destinations

There are analogous examples located within the city of closely spaced signals that operate acceptably. One example is along Broadstone Parkway between Bidwell Street and Cavitt Drive.

Broadstone Parkway and Marketplace; Broadstone Parkway and Cavitt Drive (Existing):

- Signals on Broadstone Parkway at two intersections, in close proximity to another section of East Bidwell Street Broadstone Parkway/Marketplace and Bidwell Street/Cavitt Drive are 550 ft apart
- There are dual left-turn lanes from Bidwell onto Broadstone Parkway and then from Broadstone Parkway into the shopping center
- There is only one uncontrolled driveway along the segment
- High density residential is located on one side of Broadstone Parkway
- Significant commercial land use on the other side of Bidwell results in significant traffic crossing and turning onto Bidwell

Indications that the signalized intersection does not function as modeled

If the signal at Alder Creek Parkway and McCarthy Way is not working as planned or modeled, the following conditions would be observed:

- Queueing in the eastbound left turn pocket on Alder Creek blocking through traffic (lane starvation) or extending to the intersection with East Bidwell (spill-back)
- Queueing in the southbound left-turn pocket on Bidwell extending to the eastbound US-50 ramps (less likely)
- To validate that this is a persistent (chronic) issue, lane starvation or queue spill-back conditions should be observed during regular operations for more than 15 minutes during a peak hour and repeated three days in a week

Proposed Mitigation

A phased mitigation should be implemented before any capital changes are implemented:

1. Ensure that the signals are timed correctly and/or update the signal timing to ensure queues are being processed correctly.
2. If signal timing is unable to remove the persistent queues, convert signal into a pedestrian signal for crossing the eastern approach of Alder Creek Parkway and convert McCarthy Way into Right-In/Right-Out only stop-controlled approaches. Maintain a signal head that allows protected



southbound right turns from McCarthy Way when the pedestrian signal has blocked traffic on Alder Creek Parkway.



Scenario 2 — Roundabout at Alder Creek Parkway and McCarthy Way

This scenario includes the following components:

- A roundabout with two lanes for east- and westbound directions and one lane for north- and southbound directions
- BRT, when implemented, will be left running along the median heading westbound prior to and after the roundabout and eastbound will be in the right lane with mixed traffic



Potential Operating Concerns

- No operational or queuing concerns shown under full buildout conditions
- Issues with fitting BRT through the roundabout and location of bus stop further from intersection
- Larger footprint and either encroachment on parcel northeast of intersection to fit or potential non-standard design
- Spill-back issues from the Enclave gate spilling into the roundabout (low probability)

City Identified Concerns and Responses

- Spill-back issues from the Enclave gated entry south of McCarthy Way
 - The Progression Study showed no significant queue spill-back issues from entering vehicles delayed at the Enclave gate under Scenario 2 (roundabout control)
 - Queuing characteristics at the Enclave gate would be similar under Scenario 1 (signal) or Scenario 2 (roundabout)



- Field observations show the entryway for Enclave to be sufficiently wide and deep to store approximately five vehicles (125 feet of storage)
- It would be difficult to implement BRT through a roundabout
 - There are multiple examples of roundabouts on multilane roadways that operate BRT successfully and will design the roundabout to accommodate such transit operations (can be provided upon request)
- Roundabout design may impact the existing right of way to the south of Alder Creek Parkway where construction is already occurring

Truck Turning Analysis at Westwood Drive

- Planned fuel station located on Alder Creek between Bidwell and McCarthy
- Fuel trucks will have to turn onto Alder Creek and make a U-turn to return to the freeway
- If designed to permit it (larger footprint), the roundabout could provide a U-turn opportunity for fuel trucks at McCarthy
- If the roundabout does not provide sufficient turn radii for a fuel truck to make a U-turn, one potential truck type would require additional right-of-way at the northwest corner of Alder Creek Parkway and Westwood Drive (same as Scenario 1)

Indications that the Roundabout does not function as modeled (none)

- The roundabout provides the shortest queues of the analyzed segments, and no indications that it does not function as designed are anticipated
- Installation of the roundabout would occur in Phase 1. The roundabout is the most costly traffic control measure to install and, as such, Dignity Health will request the City's concurrence in the installation as a permanent traffic control measure (i.e., no triggers will be indicated to require Dignity Health to potentially fund the removal of this traffic control measure).
- If there are queueing concerns with the roundabout, then the other scenarios would not provide any mitigation

Future Potential Mitigation (none)

Given the footprint of the roundabout, reconstruction to convert to stop or signal control would be costly.

Roundabout Features and Benefits

- Roundabouts are safer, more efficient, less costly and reduce number of crashes where people are hurt or killed by 78-82% when compared to signalized intersections per the AASHTO Highway Safety Manual.



- **Roundabouts reduce the number of vehicles that must stop. This reduces delays and increases capacity of intersection.**
- **Reduces vehicle emissions as it eliminates stop and go driving, which produces the heaviest vehicle emissions.**
- **Reduces long term operational and maintenance costs compared to a signalized intersection. No signals to power and maintain.**
- **Folsom Plan Area Specific Plan includes roundabouts (see Exhibit D - Excerpt from Folsom Plan Area Specific Plan, 7.4 Traffic Calming Techniques)**



Scenario 3 — No Left Turn at McCarthy

This scenario includes the following components:

- Physical medians at McCarthy blocking left-turn movements for all approaches
- Pedestrian Signal on the eastern approach of Alder Creek Parkway at McCarthy to allow safe pedestrian crossing and access to BRT stations
- Double left turn lanes for the eastbound approach of Alder Creek Parkway and Westwood Drive
- BRT, when implemented, will be left running along the median heading westbound and in the right lane with mixed traffic running eastbound



Potential Operating Concerns

- Non-intuitive circulation resulting in wayfinding challenges for a driver population that can be under duress, older and/or unfamiliar with the area as typical for hospital visitors (See Exhibit E- Wayfinding Deficiencies)
- The resulting queue during the afternoon/evening extends up to Mercy Way along McCarthy Way
- The queue blocks vehicles from entering or exiting Dignity campus and adjacent parcels
- The average delay for vehicles on McCarthy Way is within the City's standard, but at the cusp of failure (within 0.3 seconds)
- Allowing vehicles to turn into an auxiliary lane on Alder Creek could alleviate queuing on McCarthy Way, but would require those vehicles to merge across multiple lanes in a short distance if motorists wish to continue to the west (Alder Creek) or south (Bidwell), resulting in an undesirable weaving condition with through vehicles from Westwood turning right onto Bidwell



Wayfinding

Wayfinding is a significant design aspect of healthcare experience and a functional necessity. The hospital wayfinding system must accommodate exterior vehicular, exterior pedestrian and interior pedestrian experiences in concert. Wayfinding design begins with the question of how does a first time patient, who may have vision and hearing impairments, arrive at the hospital facility? Measures to minimize visitor disorientation are critical in order to support patients in need of and deserving consideration and compassion.

Examples of inefficient wayfinding systems:

- Multiple pathways to the same area can create confusion
- More signs do not improve wayfinding, as the best wayfinding solution is intuitive

These examples result in time and resource waste when staff spend time giving directions to visitors.

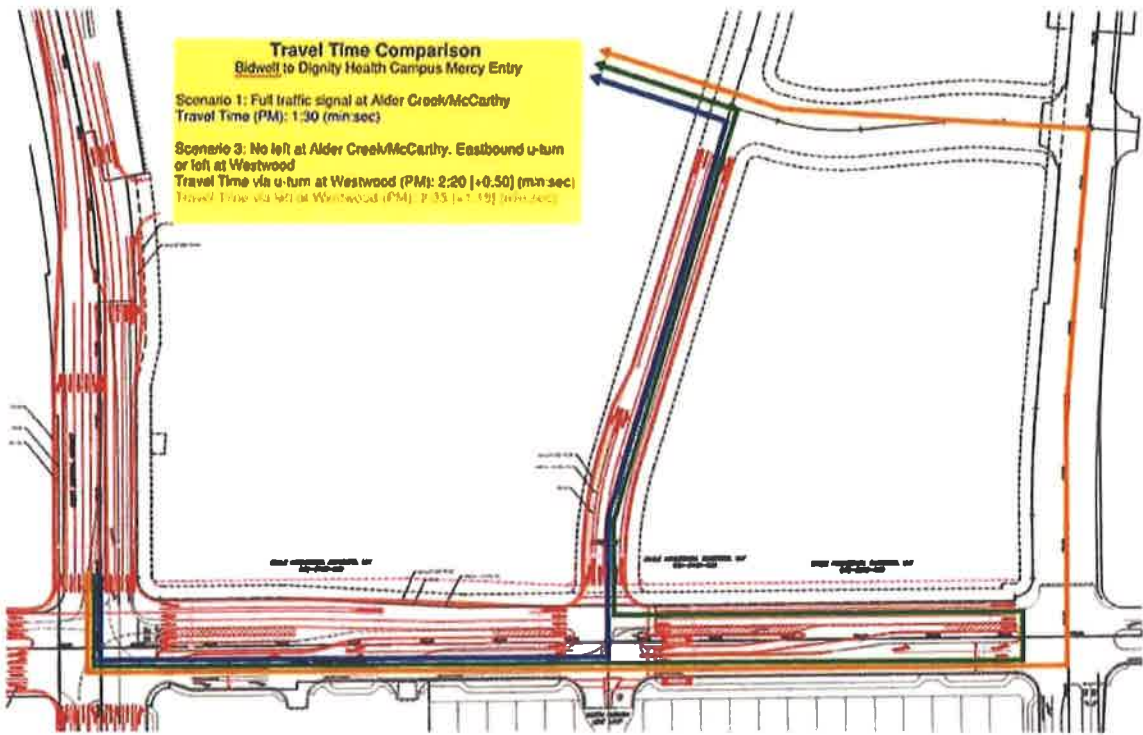
Wayfinding design principles:

- Clear sightlines to show what is ahead
- Limited navigation to not overwhelm users with choices
- Continuous visual connection
- Use landmarks to provide orientation
- Provide signs at decision points to assist in wayfinding decisions
- Comprehensive wayfinding design is the best approach

These design principles turn what could be a potentially frustrating ordeal into a smooth engaging journey. Successful wayfinding can alleviate visitor and patient stress and maximize staff efficiency.

Time is a limited resource and is a critical aspect of hospital operations, where a few minutes can mean the difference between life and death.

The direct access to the campus presented in Scenarios 1 and 2 are considerably superior over Scenario 3, where loss of visual connection presents significant wayfinding challenges and results in significant travel time delay.





Scenario 3 presents a wayfinding challenge to patients and visitors, as they lose visual connection to their destination as they drive past the campus along Alder Creek Parkway. This signage gap can trigger stress and confusion. From Alder Creek, a signature corridor, the driver then needs to negotiate one left turn at Placerville, a collector street. Passing McCarthy along Alder Creek, the driver has lost sight of the medical center. From Placerville Road, signage would be required to direct the driver to the medical center, as the view of the medical center is too far in the distance and obscured by the surrounding development, which is high density multifamily residential. Another left turn into Mercy is required to finally arrive at the destination.



Camera View 01



Camera View 02



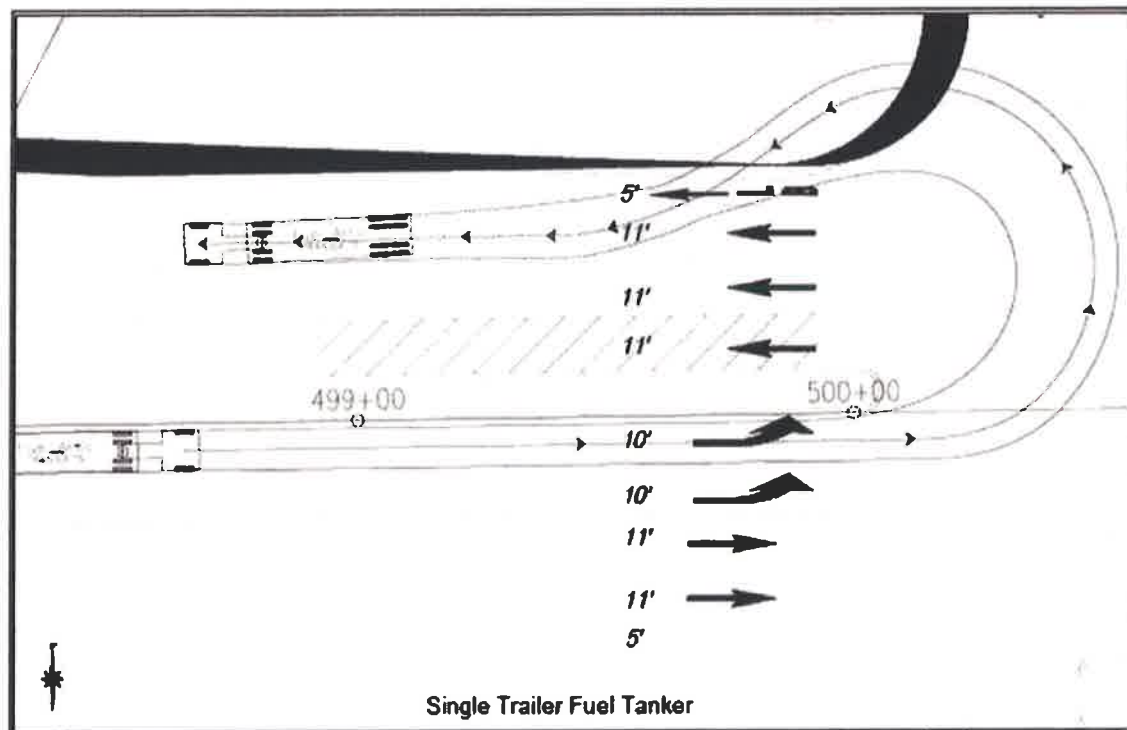
Camera View 03



Camera View 04

Truck Turning Analysis at Westwood Drive

- Planned fuel station located on Alder Creek between Bidwell and McCarthy Way
- Fuel trucks will have to turn onto Alder Creek and make a U-turn to return to the freeway
- U-turns would be blocked by a median at McCarthy Way
- One of the potential truck types would need additional room (More than Scenario 1 or 2) at the northwest corner of Alder Creek Parkway and Westwood Drive (see following truck uturn diagram). Scenario 3 has two left turn lanes at Alder Creek and the truck turn starts further north from the northwest corner. *In Scenarios 1 and 2 there is only one left turn lane and a larger median at Westwood, which accommodates the truck turn starting further south from the northwest corner.*



Indications that the Stop Control does not function as modeled

If the stop-control at Alder Creek Parkway and McCarthy Way operated as modeled, it would be observed as either:

- Queueing in the southbound approach on McCarthy Way extending to Mercy Drive, blocking driveways and operations at the intersection of McCarthy Way and Mercy Drive



- To validate that this is a chronic issue, the queue blocking (land starvation) should be observed during regular operations for more than 15 minutes during a peak hour and repeated three days in a week

Mitigation

Operations should be analyzed with peak hour counts, intersection modeling, and signal warrants before any capital changes are implemented. A phased mitigation should be implemented:

1. Maintain a pedestrian signal control for eastbound traffic and convert the intersection into a partial signal that stops westbound traffic on Alder Creek Parkway and allows protected southbound right turns to clear the queue if it extends halfway to Mercy Drive.
2. If the above remediation fails to clear the queue or causes queues on Alder Creek Parkway to extend to Westwood Drive, remove the median barrier and convert to the signalized intersection (i.e., Scenario 1).

Conclusion



Regards,
DEVENNEY GROUP LTD.

A handwritten signature in black ink, appearing to read "Joanie Palk", written over the typed name.

Joanie Palk

c Martha Lofgren, Brewer Lofgren LLP



Exhibit A – City of Folsom Standard Construction Specifications Design Standards, Section 12: Site Access

study for the project, and the analysis shall use the City's projected modeled traffic volumes for the model's horizon year.

- 3) The driveway is at least 400 feet downstream and 600 feet upstream of an intersection containing left-turn pockets.
- D. Turning movements may be restricted for any driveway where deemed necessary by the City Engineer because of safety concerns.

12.10 SIGNALIZED DRIVEWAYS

The need for traffic signals at driveways shall be based on warrants contained in the latest, edition of the Caltrans Traffic Manual (see Section 13 of these Design Standards). Any such evaluation shall be performed by the consultant as a part of the traffic study for the project.

The City will typically deny a request for a new signal if spacing requirements cannot be met. Typical minimum spacing required between traffic signals is one-quarter (¼) of a mile.

The City does not share in the cost of design and construction of traffic signals which solely serve private property (i.e. a "tee" intersection where the driveway is situated as the "stem" of the "tee"). The developer shall bear all costs of providing signalization at the private access point, including design and construction. In the case where a private access point comprises the fourth leg of an intersection where the other three legs are public streets, the developer shall ultimately be 100% financially responsible for the private leg (or approximately one-fourth the cost of signalizing the intersection). This obligation is in addition to sharing in the cost of the remaining signal via payment of the City's Major Road Mitigation Fee (if applicable).

12.11 MINIMUM REQUIRED THROAT DEPTH

Driveways shall meet the minimum required throat depth (MRTD) requirements (see Section 13.2 H). In the case of "drive-thru" facilities, attention is directed to the latter part of Section 13.2 H for minimum on-site storage distances for ingress vehicles.

On-site parking shall not be permitted within the MRTD area. The MRTD requirement does not apply to single family residential or duplex land uses.

In cases where a traffic study is not required, or in cases where there is insufficient data available to calculate the MRTD in accordance with Section 13.2 H, Table 12-1 shall be used to determine minimum required throat depth for access points for a site. In cases where a traffic study will be provided, but the access points have not yet been determined for a site, Table 12-1 shall be used to estimate the MRTD during the site design process. In these cases, the final MRTD, requirements shall be determined by the traffic study via the methodology in Section 13.2 H. The distances shown in Table 12-1 chart represent vehicle storage equivalents, which means the total required distance may be achieved by summing the throat depths for several access points if more than one access point is to serve the site. In these cases, the distance shown in Table 12-1 shall be prorated to each access point to the nearest 25 feet based on the estimated relative percent usage of each access point.

Exhibit B – City of Folsom Standard Construction Specifications Design Standards, Section 13: Traffic Impact Studies, 13.2 Traffic Study Format

TABLE 13-4

TYPE OF FACILITY	VEHICLE STORAGE
Drive-thru bank window ¹	10 spaces
Drive-thru restaurant ²	10 spaces
Drive-thru pharmacy ¹	3 spaces
Automatic car wash	10 spaces
Self-service car wash	3 spaces
Drive-in theater	15% of parking capacity
Hospital ³	1% of parking capacity
Service station	4 spaces
Drive-thru liquor store ¹	3 spaces
Drive-thru dry cleaners ²	3 spaces
Self-storage mini warehouse ⁴	2 spaces

- ¹ Reduce to 3 spaces for seats and non-imbedded and credit unions.
- ² Measured to pick-up window.
- ³ At the main entrance to the hospital.
- ⁴ Measured to gate.

K. Traffic Signals/Stop Signs - The need for new traffic signals and stop signs shall be based on warrants contained in the latest edition of the State Traffic Manual.

If a new traffic signal is being proposed which is not already a part of the City's Capital Improvement Program, and the signal installation would result in less than 1,120 feet between signals, then the study shall include a signal progression analysis. The section of roadway to be analyzed for signal progression shall be determined by the Public Works Department and will include all existing and possible future signalized intersections.

The progression pattern calculations shall use a cycle consistent with current signal timing policies of the City. A desirable bandwidth of 50% of the signal cycle shall be used where existing conditions allow. Where intersections have no signals presently, but are expected to have signals, typically a 60% mainline, 40% cross street cycle split should be assumed. Cycle split assumptions shall relate to volume assumptions in the capacity analysis of individual intersections, and, where computerized progression analysis techniques are used, they shall be of the type which utilize turning movement volume data and pedestrian clearance times in the development of time/space diagrams.

The green time allocated to the cross street will be considered no less than the time which is required for a pedestrian to clear the main street using the Federal Highway Administration (FHWA) Manual on Uniform Traffic Control Devices standards.

Those intersections which would reduce the optimum bandwidth if a traffic signal were installed may be required to remain unsignalized and have turning movements limited by access design or median islands.



Exhibit C – Folsom Plan Area Specific Plan - March 2018, Section 7: Circulation





Exhibit D - Folsom Plan Area Specific Plan - March 2018, Section 7: Circulation, 7.4 Traffic Calming Techniques

Section 7

INTERSECTION AND MID-BLOCK BULB-OUTS

Intersection and mid-block bulb-outs may be used along roadways with high pedestrian activity to reduce the amount of time that pedestrians are exposed during roadway crossings. With the use of mid-block bulb-outs, on-street parking near intersections is eliminated to improve visibility. In addition to an increased feeling of safety for pedestrians, bulb-outs also serve as a way to decrease traffic speed, especially when vehicles attempt to turn. This measure should include accent paving and landscaping that does not impair driver sight lines. Parking is restricted along bulb-out areas and curbs shall be painted red to indicate that no parking is allowed.

SPECIAL PAVEMENT MARKINGS AND TEXTURED PAVING

Special pavement markings and textured paving serve as a visual reference for motorists of the likely presence of pedestrians and cyclists in the area. This measure may be used in conjunction with any combination of the other traffic calming measures.



Example of Special Pavement Markings

ROUNDBABOUTS/TRAFFIC CIRCLES

Roundabouts and traffic circles are an alternative form of traffic control that reduce traffic speed and the amount of stopping at intersections while providing neighborhood focal points. The use of traffic circles and roundabouts depends on several factors, such as the amount of traffic projected along a street segment, surrounding land uses, and whether the roundabout or traffic circle is a more efficient intersection control device than a stop sign or signalized intersection. If roundabouts or traffic circles are utilized, they shall be landscaped with drought tolerant low growing shrubs and grasses to provide a clear line of sight for pedestrians and motorists. Crosswalks must be located outside the roundabout or traffic circle to provide a pedestrian refuge island at the median location. Parking shall be prohibited within the roundabout or traffic circle.



Example of a Traffic Circle

7.5 TRUCK MANAGEMENT PLAN

Sacramento County, the City of Folsom, the City of Rancho Cordova, El Dorado County, CalTrans, the Capital Southeast Connector JPA, and the quarries south of the Plan Area worked together on a Truck Management Plan (TMP) to address the issues associated with quarry truck traffic. On December 6, 2011, the Folsom City Council voted to support the Truck Management Plan and to authorize the City Manager to execute the Truck Management Plan Agreement upon adoption of the TMP by the Sacramento County Board of Supervisors. On December 14, 2011, the Quarry Truck Management Plan was approved by the Sacramento County Board of Supervisors (*Resolution No. 2011-0938*). The city may redesign the Plan Area circulation network to accommodate future quarry truck traffic.

Planning Commission
Folsom Ranch Medical Center
May 19, 2021

Attachment 20

**Transportation Analysis and CEQA Impact Study
Dated April 23, 2021 and April 29, 2021
(documents found on the City's website at
<https://www.folsom.ca.us/home/showpublisheddocument/6155/637558120792530000>)**

Planning Commission
Folsom Ranch Medical Center
May 19, 2021

Attachment 21

Arborist Report and Oak Tree Mitigation Strategy Dated May 11, 2021

Arborist Survey and Mitigation Strategy

Parcel 85A and Offsite Infrastructure Project

Folsom, Sacramento County, California

Prepared For:

Eagle Commercial Partners, LLC



ECORP Consulting, Inc.
ENVIRONMENTAL CONSULTANTS

May 11, 2021

Arborist Survey and Mitigation Strategy for the Parcel 85A and Offsite Infrastructure Project

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- Attachment B – Tree Survey Data (March 26, 2021)
- Attachment C – Representative Photographs
- Attachment D – Statement of Qualifications

Arborist Survey and Mitigation Strategy for the Parcel 85A and Offsite Infrastructure Project

LIST OF ACRONYMS AND ABBREVIATIONS

Arborist	ISA-certified arborist
ASCA	American Society of Consulting Arborists
City	City of Folsom
CRZ	Critical Root Zone
DSH	Diameter at standard height
EIR/EIS	Environmental Impact Report/Environmental Impact Statement
FPA	Folsom Plan Area
FPASP	Folsom Plan Area Specific Plan
ILF	In-lieu fee
NRCS	Natural Resources Conservation Service
Project	Parcel 85A and Offsite Infrastructure Project
TPZ	Tree Protection Zone
USACE	U.S. Army Corps of Engineers
USGS	U.S. Geological Survey

1.0 INTRODUCTION

ECORP Consulting, Inc. conducted an arborist survey for the Parcel 85A and Offsite Infrastructure Project (Project), located in the city of Folsom in Sacramento County, California. The purpose of this report is to quantify oak woodland canopy within the Project Area as defined and identified on Figure 10.2 in the Folsom Plan Area Specific Plan (FPASP) (Torrence Planning et al. 2011) and to identify, map, and assess the general condition of trees within the Project Area according to the Folsom Tree Preservation Ordinance (Chapter 12.16, City [City] of Folsom 2020a). In addition, this report presents a preliminary mitigation strategy for the Project which will be updated at a later time once final impacts to oak woodland are determined.

2.0 PROJECT DESCRIPTION

The Project is generally located south of U.S. Highway 50, east of East Bidwell and north of Alder Creek Parkway, in Sacramento County, California (Figure 1). Additional offsite infrastructure elements occur to the west of East Bidwell. The Project corresponds to Sections 8, 9, 16, and 17, Township 9 North, Range 8 East (Mount Diablo Base and Meridian) of the "Clarksville, California" and "Folsom, California" 7.5-minute quadrangles (U.S. Geological Survey [USGS] 1953, 1967, respectively). The approximate center of the Project site corresponds to Latitude 38.64032° and Longitude -121.111751° within the Lower American Watershed (Hydrologic Unit Code #18020111; Natural Resources Conservation Service [NRCS] et. al. 2016).

The Project consists of a commercial development along with supporting offsite infrastructure. The Project site is predominantly undeveloped annual grassland with a small area of oak woodlands at the western edge of the offsite infrastructure component. A small portion of the Project area was previously impacted by implementation of other projects within Folsom Plan Area.

3.0 METHODS

3.1 Field Survey

Oak woodland for the entire Folsom Plan Area (FPA) was previously mapped and presented in the Conceptual Oak Tree Mitigation Plan (ECORP 2017). The oak woodland habitat was determined by locating stands of oak trees¹ with more than 10-percent cover, then adding a 30-foot buffer to the outer edge of the tree canopy (ECORP 2017). Any oak trees not included within the oak woodland were determined to be individual or isolated oak trees.

¹ Oak woodlands were identified using geographic information systems technology, rectified aerial photographs, and field observation

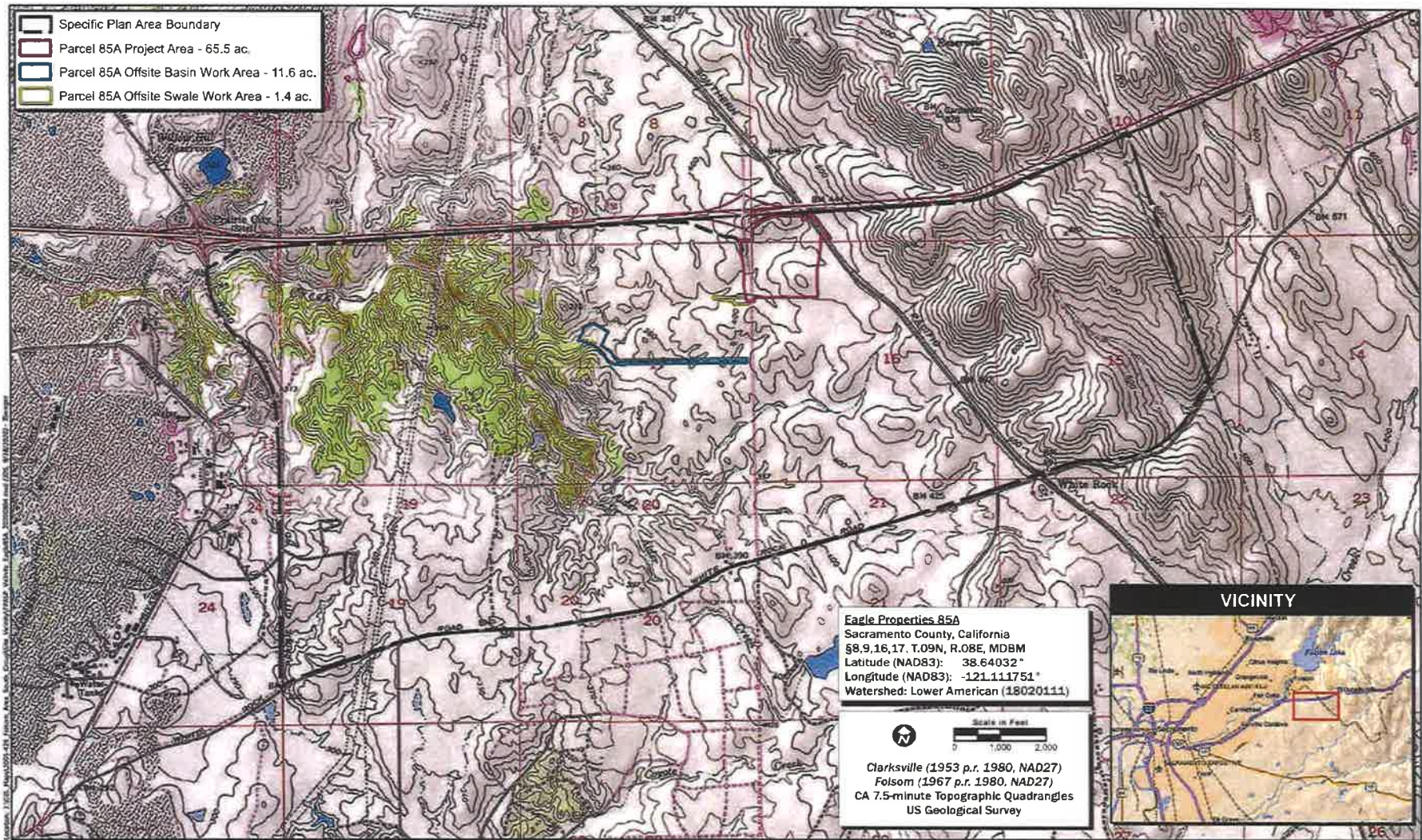


Figure 1. Project Location and Vicinity

2019-026 Eagle Properties 85A

Arborist Survey and Mitigation Strategy for the Parcel 85A and Offsite Infrastructure Project

ECORP arborist Krissy Walker-Berry (International Society of Arboriculture Certification #WE-11308A), with ECORP biologist Gabrielle Attisani, conducted the field survey on March 26, 2021. The Project Area was walked during the field survey, and data were recorded using a submeter capable Global Positioning System unit. No individual oak trees (as defined in the FPASP) were identified onsite. As such, the survey focused on the oak woodland area within the Project. While the approved FPASP Environmental Impact Report/Environmental Impact Statement (EIR/EIS; U.S. Army Corps of Engineers [USACE] and City of Folsom 2011) for the FPA does not require an arborist survey for trees within the oak woodland, at the request of the City, ECORP collected additional information on oak trees within the oak woodland. Oak tree locations within the oak woodland were previously surveyed and mapped by MacKay and Soms Civil Engineers and ECORP collected additional data on these specific individuals within the oak woodland.

All trees identified as Protected Trees (defined below) were surveyed. Trees excluded from the City of Folsom's Master Tree List as "not recommended" or "excluded from protection" or "heritage trees not eligible for heritage status" were not counted. Therefore, additional trees are located along within the Project that were not surveyed as part of this effort (e.g., known willow trees occurring along East Bidwell). Trees that occur outside of the Project area (i.e., trunk location is not within the Project Area) were not included in this survey. However, it is expected that these trees will be surveyed at a future time prior to ground breaking to provide baseline data for future monitoring (see Section 5.2).

3.2 Definitions

Data collected included species, tree tag number, diameter at standard height (DSH), dripline radius, structure, and condition. The survey results are intended for general Project planning purposes only; therefore, these results should not be considered a detailed tree analysis (i.e., results do not include hazard assessment, tree health diagnosis, preservation or removal recommendations, or pruning advisement). The following definitions from the Folsom Tree Preservation Ordinance were used to guide the methodology and data collection for this survey effort:

- **Critical Root Zone (CRZ):** A circle with a minimum radius of one foot for every one inch in trunk DSH. For Heritage trees, the CRZ shall be a circle with a minimum radius of 1.5 feet for every one inch in trunk DSH, unless reduced by the City Arborist.
- **Dripline Radius:** A perfect circle around the tree with the radius being equal to the longest branch of the tree.
- **Heritage Tree:** A tree on the City's Master Tree List that is over 30 inches DSH or a Multi-trunked Tree on the Master Tree List having a combined DSH of 50 inches or more, except for trees listed as "Not Eligible for Heritage Status."
- **Landmark Tree:** A tree or group of trees determined by the City Council to be a significant community benefit.

 Arborist Survey and Mitigation Strategy for the Parcel 85A and Offsite Infrastructure Project

- **Master Tree List:** A list prepared by the City, as amended from time to time, identifying the species of trees that may be planted as replacement trees, as Parking Lot Shading Trees, or as Street Trees. The list also identifies tree species that are not recommended for planting within the Project or that should be excluded from protection.
- **Native Oak Tree:** Any tree over six inches DSH of the genus *Quercus* and species *lobata* (valley oak), *douglasii* (blue oak), *wislizenii* (interior live oak), *agrifolia* (coast live oak), or hybrids, thereof; or multi-trunked native oak trees having an aggregate diameter of 20 inches DSH or more.
- **Parking Lot Shading Tree:** Any tree planted in a parking lot in order to meet shade coverage requirement and as identified in the Zoning Code.
- **Protected Tree:** Native Oak Trees, Heritage Trees, Landmark Trees, and Regulated Trees.
- **Regulated Tree:** Trees required by the City's Zoning Code (such as Parking Lot Shading Trees and Street Trees) or required as conditions of development project approval (such as landscape buffer or screening trees) or required by this Chapter as mitigation for the removal of a Protected Tree.
- **Street Tree:** A tree of an approved species, in accordance with the City Master Tree List, located within 12.5 feet of a street or sidewalk, measured from the back of the sidewalk or back of the curb if no sidewalk exists.
- **Tree Protection Zone (TPZ):** The circumference of the outermost edge of the tree's CRZ or Dripline Radius, whichever is greater, plus one foot.

The following additional terms define the collected data:

- **DSH:** The diameter of a tree measured at 4.5 feet above ground while standing on the high side of the tree. For a tree other than a Multi-trunked Tree that branches at or below 4.5 feet, DSH shall mean the diameter at the narrowest point between the grade and the lowest branching point. For Multi-trunked Trees (trees with multiple vertical trunks in contact at or near ground level), the DSH shall be the diameter of each stem added together in total. This report lists total aggregate diameter along with the total number of trunks that were measured and each trunk's diameter.
- **Condition:** An estimate of the tree's overall health. This includes evaluation of foliage, evidence of wound healing, evidence of fungal attack, density of insect galls, and the amount and condition of attached deadwood, as well as the obvious potential for structural failure of one or more major branches or trunks, the environment and condition of the root crown, symmetry of the canopy, and any noticeable effects of crowding caused by adjacent trees. Each tree's condition was rated according to the American Society of Consulting Arborists (ASCA) rating system (Table 1).

Arborist Survey and Mitigation Strategy for the Parcel 85A and Offsite Infrastructure Project

Rating	Rating Number	Rating Description
Excellent	5	No problem(s)
Good	4	No apparent problem(s)
Fair	3	Minor problem(s)
Poor	2	Major problem(s)
Hazardous or non-correctable	1	Extreme problem(s)
Dead	0	Dead

In addition, where appropriate, notes were taken regarding any unusual features (e.g., large trunk cavities, obvious damage or disease, girdling by barbed wire).

4.0 RESULTS AND IMPACT ANALYSIS

4.1 Survey Results

Seventeen trees were inventoried in the oak woodland within the Project area. All 17 trees are native blue oak trees (*Quercus douglasii*). A map depicting the locations of the inventoried trees is included as Attachment A. Detailed tree survey data for each tree are included as Attachment B. Representative site photographs are included as Attachment C.

The Project Area includes 17 trees that meet the definition of Protected Trees as defined above. One of these trees, tag number 1600, can also be categorized as a Heritage Tree. Photographs of this particular tree are also included in Attachment C. Particular attention is being given to this tree and the potential for preservation; however, engineering constraints may preclude available preservation options.

A Tree Work Permit and Tree Removal Permit will need to be obtained from the City prior to any construction activities affecting a Protected Tree on this Project.

5.0 MITIGATION STRATEGY

Mitigation for tree impacts within the FPA follows the requirements of the FPASP EIR/EIS, rather than the current (City of Folsom 2020a) City Tree Preservation Ordinance. The following sections provide a general mitigation strategy that will be updated once final impacts are determined through the rough grading plan approval process.

5.1 Direct Impact/Removed Tree

Up to 17 trees may be removed within the oak woodland for Project implementation. Once the Project impacts have been determined, per the FPASP, there are three options for mitigation:

1. Preserve offsite oak woodland habitat. While this option is allowed, the City has previously requested that oak impacts for this project are mitigated onsite to the extent feasible. This is the preferred method of mitigation for this Project if the City supports this approach.
2. Use of the in-lieu fee (ILF) program. The total cost of the ILF program is \$150 per DSH inch removed. It is expected that some portion of mitigation will utilize the ILF program if offsite preservation is not utilized.
3. Onsite plantings. As previously discussed with the City, this is their preferred method for mitigation. However, onsite planting is not feasible within the boundaries of this Project as there are no open space areas that can support plantings. In the event that offsite preservation is not utilized, the City initially agreed to a deferred planting plan to be implemented in conjunction with a future project or phase of a project within the FPA, where planting locations will be available. Per the EIR/EIS, planting options include the following:
 - a. Option 1: 24-inch box oak tree; one of these replaces two DSH inches removed.
 - b. Option 2: 15-gallon container oak tree; one of these replaces one DSH inch removed.
 - c. Option 3: five-gallon container oak tree; one of these replaces 0.5 DSH inch removed.
 - d. Option 4: A combination of the above options.

5.2 Indirect Impact/Preserved Trees

It is expected that there will be indirect impacts to trees as a result of this project. However, the exact trees have not been identified at this time. Indirect impacts include activities such as, but not limited to, grading, trenching, paving, soil disturbance, and pruning within the TPZ of the tree.

The City has established standards for the TPZs of all Protected Trees. These standards include the following:

- A pre-construction meeting between the Project Arborist, City Arborist, construction foreman/superintendent, and all applicable parties.
- High-visibility exclusionary fencing shall be shown on the Project plans and installed to protect the TPZ of all Protected Trees in proximity to the regulated activity. Unless the Project construction goes through the TPZ, the fencing shall encompass the entire TPZ.

Arborist Survey and Mitigation Strategy for the Parcel 85A and Offsite Infrastructure Project

- Weatherproof warning signs shall be attached to the TPZ fencing.
- Where construction access through the TPZ is necessary, the soil shall be protected via the installation of an access path. The access path shall be comprised minimally of one-inch minimum thickness plyboards laid over a four-inch layer of mulch. Additionally, trunk protection or high-visibility fencing shall be placed around the trunk.
- Tree pruning will be completed by an experienced arborist, preferably prior to the start of construction.

The following must occur to protect the Protected Trees during the Project:

- If any follow-up pruning is discovered, tree pruning will be completed by an experienced contractor under the direct supervision of the Project Arborist.
- Any necessary root severance shall be performed with sharp, sterile hand tools. Roots that have been ripped, torn, or crushed shall be traced back to undamaged tissue and cut cleanly, resulting in a flat surface with the adjacent bark firmly attached.
- Roots exposed from grade cuts or trenching shall not be allowed to dry out. If roots will be exposed overnight or if directed by the Project Arborist, the cut faces shall be moistened and covered until backfilled.
- The following actions are prohibited within the TPZ of Protected Trees:
 - Parking of vehicles, equipment, or storage of material within the TPZ, unless otherwise approved by the City.
 - Breaking roots with a backhoe or crushing them with a grader.
 - Wounding or breaking tree trunks or branches through contact with vehicles or heavy equipment.
 - Contamination of soil by washing out equipment or conducting vehicle maintenance.

In addition to these standards of protection during construction, the FPASP and the Folsom Tree Preservation Ordinance require monitoring of trees that are indirectly impacted by construction. The following measures are recommended to monitor indirect impacts:

- Prior to grading, an Arborist will tag and assess the trees that will be subject to indirect impacts.

Arborist Survey and Mitigation Strategy for the Parcel 85A and Offsite Infrastructure Project

- Monitor the health of all Protected Trees potentially impacted by the construction activities throughout the duration of the development Project. Per discussion with the City Arborist, this means the Project Arborist will be present onsite at the following times: the first one to two days of work within the Project when in the vicinity of Protected Trees, all work under native oak tree canopies, during all tree pruning, and anytime a backhoe is being used under any TPZ. In addition, the Project Arborist will be on call for questions related to work under all trees when not onsite. Because the Arborist will not likely be needed each day, a minimum of one weekly site visit will be conducted to check on the status of the trees within the Project when work is being conducted in the vicinity of Protected Trees.
- Prepare a certificate of compliance at the conclusion of the Project, attesting compliance with the standards outlined in the Maintenance Standards and the conditions of the tree permit (City of Folsom 2020b).
- Monitor the health of all indirectly impacted Protected Trees annually, for a period of five years. If any monitored tree appears to be dead or dying within five years of Project implementation, the tree shall be replaced using the direct impact mitigation described above.

6.0 CONCLUSION

This Arborist Report and Mitigation Strategy is intended to support the Project's entitlement process. Exact impacts and mitigation are unknown at this time. It is expected that a Final Mitigation Plan will be prepared at a later time once final impacts have been determined and exact mitigation has been calculated. Other documents or information may be required to support the Final Mitigation Plan (but are not expected at this stage). These may be standalone plans or included within the Final Mitigation Plan (if feasible). These may include the following:

- Planting and Irrigation Plan
- Performance Standards and Monitoring Plan for Mitigation Plantings
- Indirect Monitoring Methodology Plan

7.0 REFERENCES

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LIST OF ATTACHMENTS

Attachment A – Tree Inventory

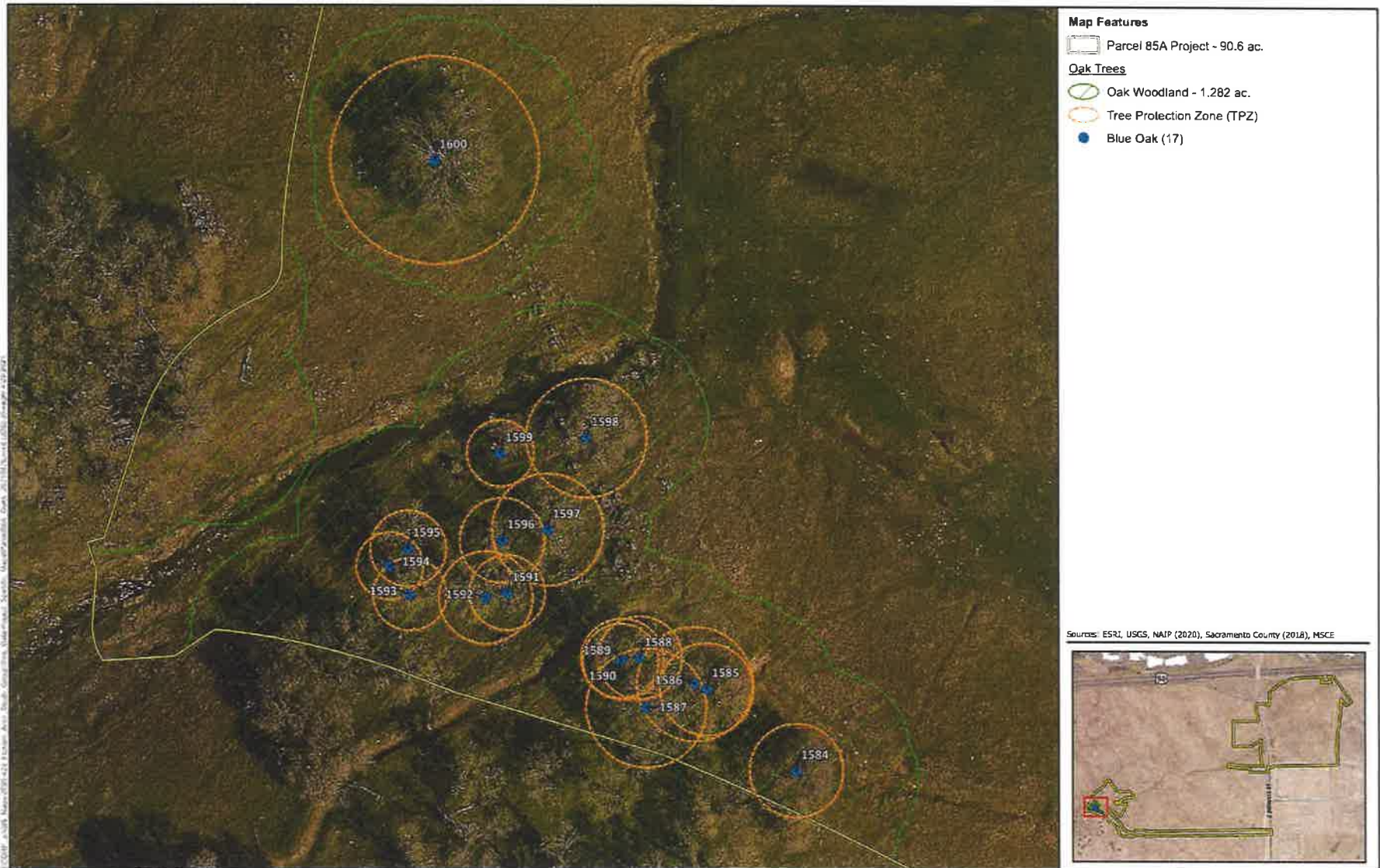
Attachment B – Tree Survey Data (March 26, 2021)

Attachment C – Representative Photographs

Attachment D – Statement of Qualifications

ATTACHMENT A

Tree Inventory



ATTACHMENT B

Tree Survey Data (March 26, 2021)

Tree Survey Data
March 26, 2021

ECORP Tag #	MSCE #	Common Name	Latin Name	DSH (inches)	Dripline (feet)	Condition	Heritage?	CRZ* (feet)	TPZ^ (feet)	Field Notes
1584	172	Blue Oak	<i>Quercus douglasii</i>	18	20	Good	No	18	21	Some suckering, good budding
1585	174	Blue Oak	<i>Quercus douglasii</i>	16	20	Good	No	16	21	Medium lean, good budding
1586	175	Blue Oak	<i>Quercus douglasii</i>	16	25	Fair	No	16	26	Large lean, suckering, cavities, good budding
1587	176	Blue Oak	<i>Quercus douglasii</i>	26	24	Fair	No	26	27	Codominant stems with included bark, suckering, broken branches
1588	179	Blue Oak	<i>Quercus douglasii</i>	12	18	Good	No	12	19	Some suckering and dead branches, good budding
1589	178	Blue Oak	<i>Quercus douglasii</i>	17.5	14	Good	No	17.5	18.5	Suckering and few dead branches but good budding
1590	177	Blue Oak	<i>Quercus douglasii</i>	13	16	Good	No	13	17	Few dead branches and suckering but good budding
1591	184	Blue Oak	<i>Quercus douglasii</i>	16	14	Poor	No	16	17	Decent budding but all from suckers along branches, top of main trunk dead
1592	183	Blue Oak	<i>Quercus douglasii</i>	17.5	20	Fair	No	17.5	21	Codominant branching, suckering, and large dead branches, good budding
1593	180	Blue Oak	<i>Quercus douglasii</i>	15.5	14	Fair	No	15.5	16.5	Lots of suckering, large dead branches, good budding
1594	181	Blue Oak	<i>Quercus douglasii</i>	11.5	14	Fair	No	11.5	15	Lots of suckering and some large dead branches
1595	182	Blue Oak	<i>Quercus douglasii</i>	11	16	Fair	No	11	17	Some suckering and dead branches, good budding
1596	185	Blue Oak	<i>Quercus douglasii</i>	12	18	Fair	No	12	19	Lots of suckering
1597	186	Blue Oak	<i>Quercus douglasii</i>	17	24	Fair	No	17	25	Codominant trunks, suckering, and some dead branches, good budding
1598	188	Blue Oak	<i>Quercus douglasii</i>	20.5	26	Fair	No	20.5	27	Codominant trunks and dead branches, good budding
1599	187	Blue Oak	<i>Quercus douglasii</i>	12.5	14	Fair	No	12.5	15	Lots of suckering and dead branches
1600	189	Blue Oak	<i>Quercus douglasii</i>	30.5	34	Fair	Yes	45.75	46.75	Large dead branches, low suckering, good budding
* CRZ = Critical Root Zone										
^ TPZ = Tree Protection Zone										

ATTACHMENT C

Representative Photographs



Photo 1: Tree 1585; March 26, 2021.



Photo 2: Tree 1587; March 26, 2021.



Photo 3: Tree 1589, 1590 and 1588 (left to right);
March 26, 2021.



Photo 4: Tree 1595; March 26, 2021.



Photo 5: Tree 1600; May 6, 2021.

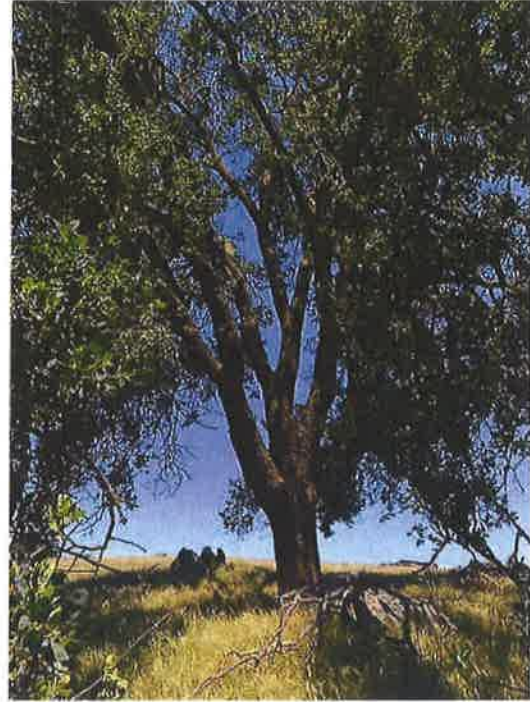


Photo 6: Tree 1600; May 6, 2021.

ATTACHMENT D

Statement of Qualifications

Attachment D

Statement of Qualifications

Krissy Walker-Berry

Senior Biologist and Certified Arborist, ECORP Consulting, Inc.

Ms. Walker-Berry is a biologist and certified arborist with more than 13 years of professional experience in field surveys, biological monitoring, preserve management, permitting, permit compliance, and report preparation for biological resources in both northern and southern California. Her arborist experience includes maintaining her ISA-certified arborist status since 2015 and her Tree Risk Assessment Qualification since 2018, as well as conducting multiple tree inventories, creation of tree mitigation plans, and monitoring construction activities around protected trees and other vegetation. She is also experienced in conducting focused and general surveys for listed and sensitive plants, general surveys for nonnative plants, vegetation mapping and assessment, jurisdictional wetland delineations, nesting bird surveys, and biological construction monitoring.

Planning Commission
Folsom Ranch Medical Center
May 19, 2021

Attachment 22

**Environmental Checklist and Addendum for
Folsom Ranch Medical Center
Dated April, 2020
(documents found on the City's website at
<https://www.folsom.ca.us/home/showpublisheddocument/6137/637558099893530000>)**

Planning Commission
Folsom Ranch Medical Center
May 19, 2021

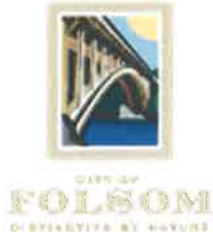
Attachment 23

**Mitigation Monitoring and Reporting Program for
Folsom Ranch Medical Center Project**

Dated April, 2021

**(documents found on the City's website at
<https://www.folsom.ca.us/home/showpublisheddocument/6141/637558120754730000>)**

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Folsom City Council Staff Report



MEETING DATE:	6/8/2021
AGENDA SECTION:	Old Business
SUBJECT:	Resolution No. 10646 - A Resolution Approving Changes to Transit Services and Vehicle Branding for Transit Services Provided by Sacramento Regional Transit Under the 2019 Annexation Agreement with the Sacramento Regional Transit District
FROM:	Public Works Department

RECOMMENDATION / CITY COUNCIL ACTION

Staff recommends that the City Council adopt Resolution No. 10646 - A Resolution Approving Changes to Transit Services and Vehicle Branding for Transit Services Provided by Sacramento Regional Transit Under the 2019 Annexation Agreement with the Sacramento Regional Transit District.

BACKGROUND / ISSUE

The City of Folsom officially annexed its transit operations known as Folsom Stage Line to Sacramento Regional Transit District (SacRT) on January 1, 2019. As a part of the annexation agreement SacRT agreed to implementing no major service changes to Folsom routes or transit services for a minimum of one year. This commitment has been fulfilled. SacRT is now looking for opportunities to make some new service changes that will ensure that quality transit services are being delivered to Folsom residents. This is best accomplished when SacRT works closely with the City of Folsom staff, residents and community partners to expand and improve transit throughout the City of Folsom's service area.

POLICY / RULE

In accordance with the Annexation Agreement, SacRT major service changes require the approval of the City Council and the SacRT Board of Directors. SacRT may make major service changes without City Council approval but must provide sixty (60) days written notification to the City.

ANALYSIS

SacRT is proposing to discontinue the Dial-A-Ride (DAR) service as it exists today and, in its place, customers who currently use Dial-A-Ride will be transitioned over to SacRT GO paratransit services. For new and existing customers, SacRT will provide expanded on-demand Smart Ride service, that allows customers to have same-day transit options available via a mobile app or call in. SacRT believes these changes are beneficial to Folsom residents for several reasons.

The original Dial-A-Ride (DAR) was provided as the complementary American with Disabilities Act (ADA) paratransit to Folsom Fixed Route buses; with the annexation of Folsom transit services in January 2019, Folsom was included into the SacRT ADA service area. With service as it is today, the DAR service is a redundant service to SacRT GO and Smart Ride. This plan allows for efficient and effective use of public funds.

SacRT believes SacRT GO with added Smart Ride coverage will provide similar if not better quantity and quality of service that is currently available through Folsom DAR. SacRT GO provides more access to the region and a longer span of service, including added benefits from above and beyond non-ADA hours. There will also be increased resources added to the Folsom Zone for Smart Ride in anticipation of increased demand.

SacRT will work to reduce any impacts of discontinuing the Folsom DAR for seniors who do not meet the ADA eligibility criteria, but still have a need for this service. SacRT plans to do this by allowing all current riders of Folsom DAR to maintain current eligibility when transitioned to SacRT GO services; this will allow the current active riders the ability to use the resources available to travel the region, as well as local-to-local trips within Folsom with same-day on-demand Smart Ride service. As Smart Ride is already available as a resource for the general public, new riders would not need to wait to be age eligible to ride the service like they did with Dial-A-Ride. They have the option to book for a flexible experience to reserve their rides within a matter of minutes.

SacRT staff has determined that under existing Federal Transit Administration (FTA) guidelines and under SacRT existing service change policy this does not constitute a major service change. It requires no Title VI action.

SacRT staff recognizes the importance of communication and outreach to existing customers and have conducted the following outreach efforts:

- SacRT presentation to Folsom City Council on April 21, 2021
- SacRT staff met with the Folsom Rider Task Force on April 22, 2021
- SacRT staff met with the Folsom City staff on April 23, 2021
- SacRT staff met with GM Henry Li and concerned citizens on April 27, 2021
- SacRT staff presentation to SacRT Mobility Advisory Council May 6, 2021

SacRT has mailed out information flyers to existing DAR customers notifying them of the upcoming changes. SacRT will also provide flyers for DAR operators to hand out to existing

customers. In addition, SacRT is scheduling a virtual workshop this summer to help existing customers transition over to SacRT GO and SmaRT Ride.

A summary of the proposed changes is attached for your review and approval.

SacRT is also proposing to update the branding of the vehicles to create a regional branding that will make it easier for customers to identify as SacRT service throughout the service area. It is important that SacRT customers can see that the system is coordinated and integrated. The new wrapped vehicles will be added to the SmaRT Ride system, two of which will be assigned to the Folsom zone and others will be spread throughout the service area. All new vehicles will have decals that recognize the City of Folsom's contribution to the new vehicles.

The proposed branding of the new vehicles is attached for your review and approval.

FINANCIAL IMPACT

There is no negative financial impact to the City in approving the requested changes to the SacRT Services or vehicle branding.

ENVIRONMENTAL REVIEW

This action is exempt from environmental review under the California Environmental Quality Act (CEQA).

ATTACHMENTS

1. Resolution No. 10646 - A Resolution Approving Changes to Transit Services and Vehicle Branding for Transit Services Provided by Sacramento Regional Transit Under the 2019 Annexation Agreement with the Sacramento Regional Transit District
2. Proposed Service Changes
3. Proposed Branding Changes

Submitted,

Dave Nugen, Public Works Director

Attachment 1
Resolution No. 10646

RESOLUTION NO. 10646

A RESOLUTION APPROVING CHANGES TO TRANSIT SERVICES AND VEHICLE BRANDING FOR TRANSIT SERVICES PROVIDED BY SACRAMENTO REGIONAL TRANSIT UNDER THE 2019 ANNEXATION AGREEMENT WITH THE SACRAMENTO REGIONAL TRANSIT DISTRICT

WHEREAS, the City of Folsom officially annexed its transit operations known as Folsom Stage Line to Sacramento Regional Transit District (SacRT) on January 1, 2019; and

WHEREAS, as a part of the annexation agreement, SacRT agreed to implementing no major service changes to Folsom routes or transit services for a minimum of one year; and

WHEREAS, SacRT has fulfilled its commitment to no major service changes; and

WHEREAS, SacRT is now requesting to phase-out the Dial-A-Ride Service and replace that service with the SmaRT Ride Micro-Transit and SacRT GO Services; and

WHEREAS, SacRT is now requesting to re-brand the Folsom Stage Line vehicles acquired in the annexation to SacRT to create a regional branding that will make it easier for customers to identify as SacRT service throughout the service area:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom approves of the changes to Transit Services and Vehicle Branding for transit services provided by Sacramento Regional Transit under the 2019 Annexation Agreement with the Sacramento Regional Transit District.

PASSED AND ADOPTED this 8th day of June 2021, by the following roll-call vote:

- AYES:** Councilmember(s):
- NOES:** Councilmember(s):
- ABSENT:** Councilmember(s):
- ABSTAIN:** Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

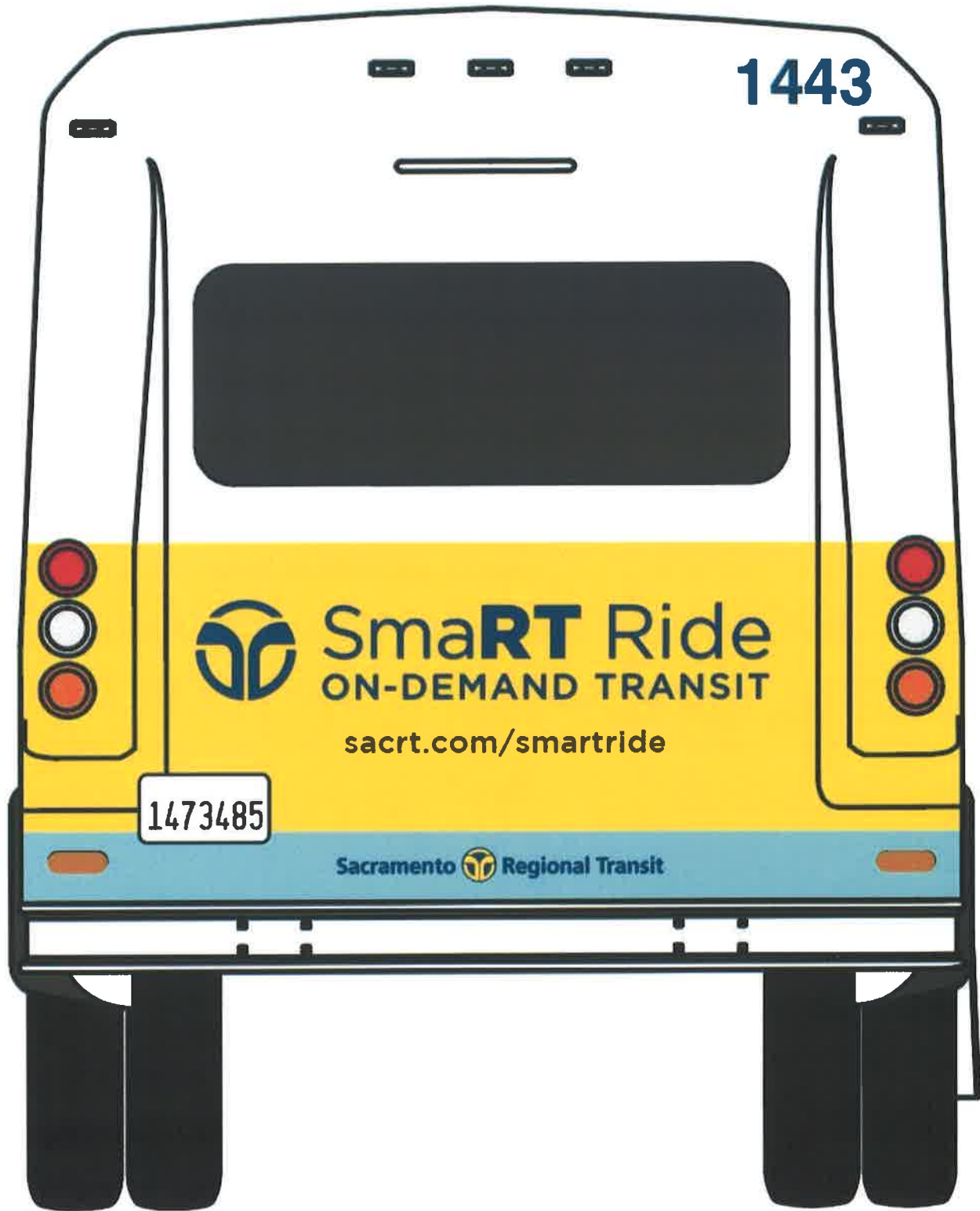
Attachment 2

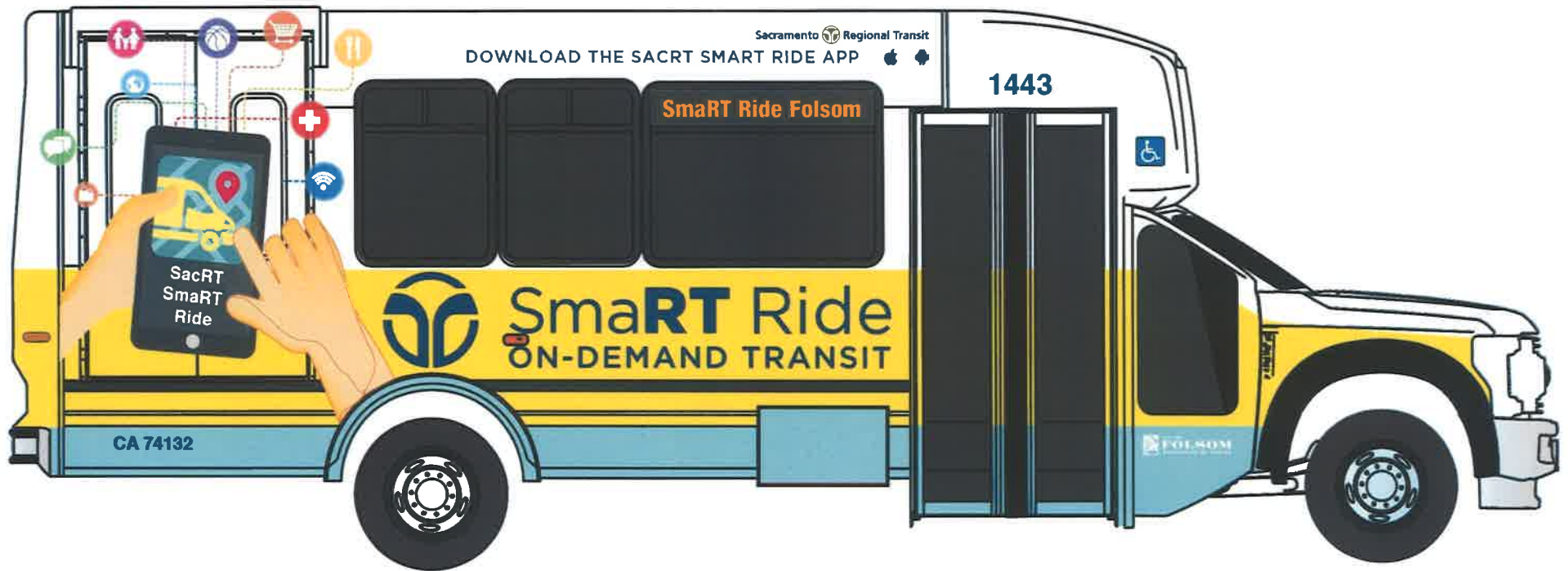
Proposed Service Changes

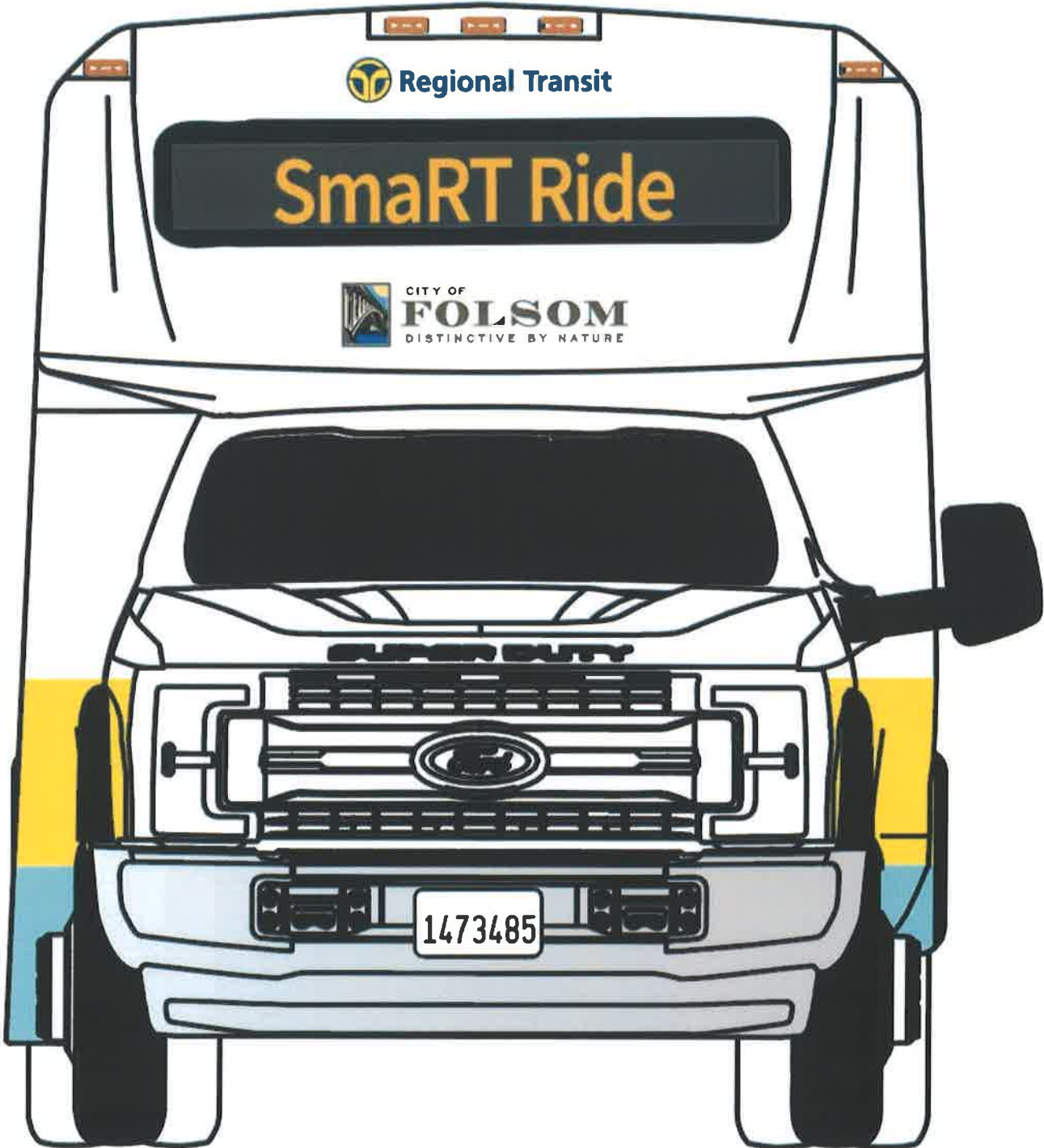
SacRT Service Table: Folsom DAR, Smart Ride, SacRT GO					
	Folsom Fixed Route	Folsom Dial-A-Ride (DAR)	Smart Ride	SacRT GO Paratransit Services (ADA)	SacRT GO Paratransit Services (Non-ADA)
Proposal	No changes at this time Route 10 (Fixed Route) Route 20 (School Trip) Route 30 (Commuter) September (Tentative)	Discontinue Service and grandfather riders August 28, 2021 (Last day of service)	Expand to include Folsom Ranch (South of Hwy 50) April/Mid June 2021 (tentative)	Expand to include Folsom Ranch (South of Hwy 50) April/Mid June 2021 (June 20, 2021)	Expand to include Folsom Ranch (South of Hwy 50) April/Mid June 2021 (June 20, 2021)
Service Area	<i>See Service Map</i> Folsom, CA	<i>See Service Map</i> DAR Service Area	<i>See Service Map</i> Folsom Zone	<i>See Service Map</i> SacRT ADA Service Area	<i>See Service Map</i> SacRT ADA Service Area
Service Description	Per Annexation Agreement Route 10 (32' bus) Route 20 (32' bus) Route 30 (Cutaway)	Per Annexation Agreement	Pilot Program	ADA Paratransit 3/4 mile radius of active fixed route or light rail station	Non-ADA Demand Response (Beyond 3/4 mile)
Eligibility	Anyone	55+ years old and older	Anyone	ADA Eligibility Grandfathered DAR Eligibility	ADA Eligibility Grandfathered DAR Eligibility
Fare: Single, One-way	\$2.50 Basic \$1.25 Senior (62+) \$1.25 ADA	\$4.00	\$2.50 Basic \$1.25 Senior (62+) \$1.25 ADA Free for 5+ Passengers	\$5.00	\$5.00
Fare: Monthly, Other	\$100 Basic \$50 Senior (62+) \$50 ADA \$40 Super Senior (75+)	\$95.00	\$100 Basic \$50 Senior (62+) \$50 ADA \$40 Super Senior (75+)	\$137.50 for 60 ADA trips + Free Fixed and Smart Ride	NA
Trip Booking Requirement	NA	Reserve 1-14 Days in Advance No Same-day Service	Reserve Day Of Same-day Service Only	Reserve 1-2 Days in Advance No Same-day Service	Reserve 1-2 Days in Advance No Same-day Service
Pick Up Window	NA	15m before, 15m after	On Demand 0-2 Minutes before	1m to 30m	1m to 30m
Hours: Mon - Fri	5:25 AM - 7:25 PM	5:25 AM - 7:25 PM	7:00 AM - 7:00 PM	Currently: 3:30 AM -11:30 PM Future: 3:30 AM -11:30 PM (Gold Line) <i>Both legs of trip must be ADA</i>	Currently: 4:30 AM -1:00 AM Future: 3:30 AM - 1:00 AM (Airport) - (Blue Line) <i>1 leg of trip must be an ADA trip</i>

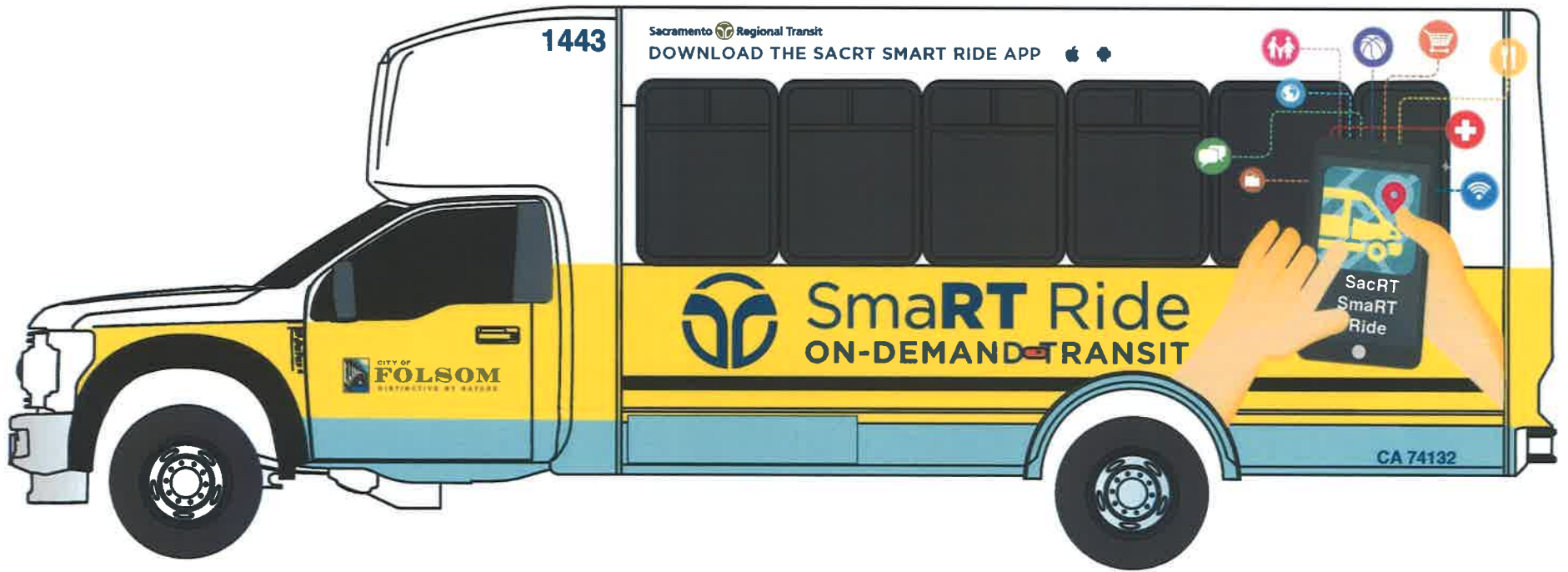
Attachment 3

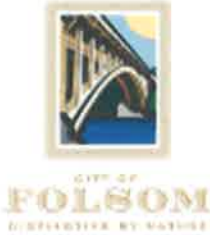
Proposed Branding Changes











Folsom City Council Staff Report

MEETING DATE:	6/8/2021
AGENDA SECTION:	New Business
SUBJECT:	Resolution No 10648 – A Resolution of Support for the Sacramento Abandoned Vehicle Service Authority to Pursue Extension of the Vehicle Registration Fee for Abatement of Abandoned Vehicles
FROM:	Community Development Department

RECOMMENDATION / CITY COUNCIL ACTION

Move to approve Resolution No 10648 – A Resolution of Support for the Sacramento Abandoned Vehicle Service Authority to Pursue Extension of the Vehicle Registration Fee for Abatement of Abandoned Vehicles.

BACKGROUND / ISSUE

California Vehicle Code section 22710 permits the creation of a service authority for the abatement of abandoned vehicles in a county if the Board of Supervisors of the County, by two thirds vote, and a majority of the cities having a majority of the incorporated population adopt resolutions providing for the creation of the Authority and the imposition of a one dollar (\$1) vehicle registration fee. The fee is collected by the Department of Motor Vehicles (DMV) and allocated to the Authority by the State Controller pursuant to California Vehicle Code section 9250.7. The revenue received by the Authority shall be used only for the abatement, removal, and disposal of any abandoned, wrecked, dismantled, or inoperative vehicles or parts thereof from private or public property. Member agencies implement, manage, and enforce their respective programs.

The Sacramento County Abandoned Vehicle Authority (AVA) Program was established in 1992 with the creation of the Sacramento Abandoned Vehicle Service Authority (SAVSA), the Governing Board that sits concurrently as the Sacramento Transportation Authority Governing Board (Board). AVA programs were initially authorized for a period of ten years

and could be extended in ten-year increments thereafter. The Board extended the program for an additional ten years in 2001 and in 2011.

The County of Sacramento and the Cities of Isleton, Galt, and Sacramento were the original members of the Sacramento AVA program. The City of Folsom joined in 1999 (Resolution 5754), followed by the City of Citrus Heights in 2000, the City of Elk Grove in 2003 and the City of Rancho Cordova in 2014. The City of Isleton withdrew from the program in 2014. There are currently seven member agencies in the Sacramento County AVA program.

Revenue collected by the DMV is distributed by formula (per California Vehicle Code section 22710) on a quarterly basis to the SAVSA members. The formula is based on two factors; 50 percent based on relative population of the SAVSA entities and 50 percent based on relative number of abatements. On average, the Sacramento Transportation Authority (STA) deducts two percent of the total revenues to pay for administration and overhead costs.

In the nearly 30 years of the program's existence, the program has generated about \$30 million in revenues and abated over 300,000 vehicles in Sacramento County. With Sacramento County's steady population growth over the last ten years, revenues have increased 24 percent and abatements by 207 percent. Over the last five-year period, Folsom's allocation of these funds has been equivalent to approximately \$70,000 annually to help cover the City's cost for removal of abandoned vehicles.

The current SAVSA fee program will expire on April 30, 2022. STA/SAVSA legal counsel has advised that the SAVSA fee qualifies as a tax under California Proposition 26 and, therefore, its extension would require a public vote with a supermajority (2/3) adoption threshold. An election effort in support of a SAVSA fee extension will incur ballot inclusion costs as well as discretionary polling and public information/education costs. SAVSA's budget does not include any set aside funds for covering the expenses related to taking a measure to public vote—the costs would have to be covered by SAVSA member agencies or other outside funding.

The SAVSA Board is requesting input from its seven member agencies about whether and how to proceed with an extension request. That feedback will inform the Board decision later this month for timely actions relative to a potential ballot measure.

POLICY / RULE

All powers of the City are vested in the City Council pursuant to Section 2.02 of the City Charter.

ANALYSIS

As a SAVSA member agency, Folsom has received quarterly distributions for vehicle abatement from the SAVSA since 1999. For Folsom, these quarterly payments over the last five-year period have resulted in annual total of approximately \$70,000 for vehicle abatements. The table below lists Folsom's annual distribution since 2010:

Year	AVA Annual Distribution
2020	\$60,033
2019	\$74,875
2018	\$75,187
2017	\$75,899
2016	\$63,273
2015	\$25,302
2014	\$39,820
2013	\$39,102
2012	\$95,435
2011	\$53,600
2010	\$59,699

Other than enforcement procedures and reporting, there have not been additional costs to the City of Folsom to participate in this program. The distribution simply helps the City offset the costs of cleaning up the blight that can be associated with abandoned vehicles, which is consistently one of Folsom's highest volume of code enforcement complaints. SAVSA receives about two percent of total revenue for program administration.

Given that this program will sunset on April 30, 2022, there are a few options that were considered by the STA/SAVSA Board on May 13, 2021 as follows

1. Allow the SAVSA program to sunset with no intention to continue;
2. Move forward with consideration of a fee extension, which may include polling, cost estimating and sourcing in coordination with member agencies, and coordination with County Elections Office; or
3. Engage Self Help Counties Coalition (SHCC) to work with the State Legislature to create a statutory amendment to classify the AVA program as an exception to the Proposition 26, and increase the one dollar (\$1) annual fee to reflect inflationary costs

At that meeting, the Board asked Staff to determine level of support from member agencies to fund the required ballot measure to extend the program. Most agencies are waiting to hear what the two largest member agencies (City and County of Sacramento) will do because full participation is essential for reasonable cost burden. Some member agencies are requesting preliminary polling of a potential ballot measure before supporting ballot measure funding.

Prior to the passage of Proposition 26, there were 41 active AVA programs statewide compared to 30 current active programs. To date, nine counties have proposed AVA tax measures with seven passing and two failing, resulting in program termination. Five other counties chose to voluntarily end their AVA programs.

The SAVSA estimated costs to place a measure on the ballot is \$1.5 million. Using the quarterly distribution formula, SAVSA staff have calculated how much each SAVSA member agency would have to cover in the cost allocation table below. Folsom's share of the ballot cost would be \$41,904 as shown in the table below.

BALLOT MEASURE COST ALLOCATION

Estimated Costs		\$1,500,000.00							
	2020 Population		50% alloc. based on population		2020-21 Abatements		50% alloc. based on abatements		Ballot Cost Allocation
Citrus Heights	87,811	5.65%	\$	42,365.19	607	2.70%	\$	20,217.16	\$ 62,582.35
Elk Grove	176,154	11.33%	\$	84,987.04	2,985	13.26%	\$	99,420.46	\$ 184,407.50
Folsom	81,610	5.25%	\$	39,373.46	76	0.34%	\$	2,531.31	\$ 41,904.77
Galt	25,849	1.66%	\$	12,471.08	456	2.03%	\$	15,187.85	\$ 27,658.93
Rancho Cordova	78,381	5.04%	\$	37,815.60	1,590	7.06%	\$	52,957.63	\$ 90,773.23
Sacramento	510,931	32.87%	\$	246,503.14	14,145	62.82%	\$	471,123.10	\$ 717,626.24
Sacramento County	593,801	38.20%	\$	286,484.50	2,659	11.81%	\$	88,562.48	\$ 375,046.98
Totals	1,554,537	100.00%	\$	750,000.00	22,518	100.00%	\$	750,000.00	\$ 1,500,000.00

At this point, staff is recommending that the City Council adopt a resolution of support for SAVSA to pursue the extension of the AVA program through vehicle registration fees. That support should be contingent upon full participation of the seven member agencies to either fund preliminary polling of a potential ballot measure or to fund appropriate ballot measure costs in accordance with the distribution formula established for the AVA program. Any specific fund expenditure would come back to the City Council for consideration and action.

FINANCIAL IMPACT

The proposed resolution of support does not have a direct financial impact. However, if all seven SAVSA member agencies support the pursuit of program extension, staff will return to the City Council with a specific funding request for fair share funding of preliminary polling and/or ballot measure costs. SAVSA staff estimated costs to place a measure on the ballot at \$1.5 million with Folsom's share of the ballot cost estimated at \$41,904. That cost could not qualify for reimbursement from the AVA program dollars.

ENVIRONMENTAL REVIEW

The project does not constitute a project under the California Environmental Quality Act (CEQA) pursuant to Government Code 65651(b)(2) and is therefore exempt from (CEQA) analysis.

ATTACHMENT

1. Resolution No 10648 – A Resolution of Support for the Sacramento Abandoned Vehicle Service Authority to Pursue Extension of the Vehicle Registration Fee for Abatement of Abandoned Vehicles
2. Sacramento Abandoned Vehicle Service Authority (SAVSA) Member Staff Memo

Submitted,

Pam Johns, Community Development Director

ATTACHMENT 1

RESOLUTION NO. 10648**A RESOLUTION OF SUPPORT FOR THE SACRAMENTO ABANDONED VEHICLE SERVICE AUTHORITY TO PURSUE EXTENSION OF THE VEHICLE REGISTRATION FEE FOR ABATEMENT OF ABANDONED VEHICLES**

WHEREAS, California Vehicle Code section 22710 permits the creation of a service authority for the abatement of abandoned vehicles in a county if the Board of Supervisors of the County, by two thirds vote, and a majority of the cities having a majority of the incorporated population adopt resolutions providing for the creation of the Authority and the imposition of a one-dollar (\$1) vehicle registration fee to be collected by the Department of Motor Vehicles (DMV) and allocated for the abatement, removal, and disposal of any abandoned, wrecked, dismantled, or inoperative vehicles or parts thereof from private or public property; and

WHEREAS, the Sacramento County Abandoned Vehicle Authority (AVA) Program was established in 1992 with the creation of the Sacramento Abandoned Vehicle Service Authority (SAVSA), the Governing Board that sits concurrently as the Sacramento Transportation Authority Governing Board (Board). AVA programs were initially authorized for a period of ten years and could be extended in ten-year increments thereafter. The Board extended the program for an additional ten years in 2001 and in 2011.; and

WHEREAS, the City of Folsom joined Sacramento AVA program in 1999 (Resolution 5754), along with six other agency members over time including the County of Sacramento and the Cities of Galt, Sacramento, Citrus Heights, Elk Grove and Rancho Cordova; and

WHEREAS, since 1999, revenue collected by the DMV has been distributed to the SAVSA members based on a set formula; 50 percent based on relative population of the SAVSA entities and 50 percent based on relative number of abatements. In the recent five-year period, Folsom has received approximately \$70,000 annually to help offset the cost of abandoned vehicle abatement; and

WHEREAS, the current SAVSA fee program will expire on April 30, 2022. Since the AVA fee qualifies as a tax under California Proposition 26, its extension would require a public vote with a supermajority (2/3) adoption threshold. Election costs associated with the ballot measure for extension would not be covered by SAVSA administration fees. Rather, agency members would collectively be responsible for funding the ballot measure at a total estimated cost of \$1.5 million. With full agency member participation at the current funding distribution formula, Folsom's fair share of the election cost is estimated at \$41,904; and

WHEREAS, the SAVSA Board is requesting that agency members identify level of support to fund the required ballot measure to extend the AVA program:

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Folsom declares support for the SAVSA to pursue extension of the vehicle registration fee for abatement of abandoned vehicles. If supported by all seven agency members, Folsom will take subsequent action to consider fair share funding support for preliminary polling of a potential ballot measure

or funding appropriate ballot measure costs for extension of the AVA Program; and

PASSED AND ADOPTED this 8th day of June, 2021, by the following roll-call vote:

AYES: Councilmember(s):
NOES: Councilmember(s):
ABSENT: Councilmember(s):
ABSTAIN: Councilmember(s):

Michael D. Kozlowski, MAYOR

ATTEST:

Christa Freemantle, CITY CLERK

ATTACHMENT 2



SACRAMENTO ABANDONED VEHICLE SERVICE AUTHORITY
Sacramento Transportation Authority
 801 12th Street, 5th Floor | Sacramento, California 95814

MEMO

To: Sacramento Abandoned Vehicle Service Authority (SAVSA) Members

From: Jennifer Doll, Special Programs Manager

Date: June 1, 2021

Re: **2022 SAVSA Sunset & Potential Tax Measure**

BACKGROUND

The Sacramento County Abandoned Vehicle Authority (AVA) Program was established in 1992 with the creation of the Sacramento Abandoned Vehicle Service Authority (SAVSA), the Governing Board that sits concurrently as the Sacramento Transportation Authority Governing Board (Board). AVA programs were initially authorized for a period of ten years and could be extended in 10-year increments thereafter. The Board extended the program for an additional ten years in 2001 and in 2011.

Revenue collected by the DMV is distributed by formula (per California Vehicle Code section 22710) on a quarterly basis to the SAVSA members. The formula is based on two factors: relative population of the SAVSA entities – 50 percent and relative number of abatements - 50 percent. On average, SAVSA deducts two percent of the total revenues to pay for administration and overhead costs. Per Vehicle Code section 9250.7 revenue shall be used only for the abatement, removal, and disposal of any abandoned, wrecked, dismantled, or inoperative vehicles or parts thereof from private or public property. Member agencies implement, manage, and enforce their respective programs

In the nearly 30 years of the program's existence, the program has generated about \$30 million in revenues and abated over 300,000 vehicles in Sacramento County. With Sacramento County's steady population growth over the last ten years, revenues have increased 24 percent (24%) and abatements by 207 percent (207%). See attachment A for performance metrics.

BEYOND THE 2022 SUNSET

The current SAVSA fee program will expire on April 30, 2022. SAVSA legal counsel has advised that the SAVSA fee qualifies as a tax under California Proposition 26 and, therefore, its extension would require a public vote with a supermajority (2/3) adoption threshold. An election effort in support of a SAVSA fee extension will certainly incur ballot inclusion costs as well as discretionary polling and public information/education costs. SAVSA's budget does not include any set aside

funds for covering the expenses related to taking a measure to public vote—the costs would have to be covered by SAVSA member agencies or other outside funding.

At the May 13th Board meeting all Board Members voted to pursue a potential ballot measure to extend the SAVSA program. Therefore, directing SAVSA staff to research the costs of polling/outreach/advocacy, the costs of placing a tax measure on the ballot and SAVSA members' ability to cover all possible tax measure costs.

SAVSA staff is currently working on estimated costs for advocacy and outreach but have a basic minimum cost for placing a tax measure on the ballot per the County Elections Office fee schedule of just over \$1.2M. For quick reference, a chart calculating the minimum ballot costs and the County Election's office fee schedule can be found on attachment B. Additionally, staff suggests distributing any tax measure costs among the SAVSA members based on the quarterly SAVSA distribution formula. Until we get a more detailed cost break down, we used \$1.5M as the tax measure total costs to be allocated among the members. This chart is included on attachment B with the other ballot costs.

SAVSA staff will continue to gather cost information, ballot processes and timeline information to share with the members. But to move forward with a tax measure to continue the SAVSA program and collection of the \$1 registration fee, the SAVSA members will have to agree and pay for any costs related to a tax measure.

Attachments

SAVSA Performance



30 mil

Revenue since program inception



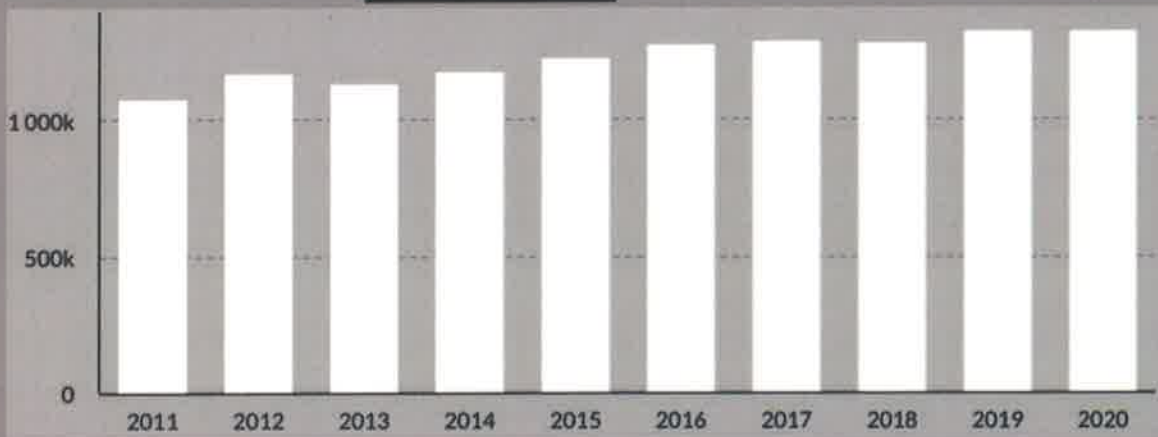
300,000

Abated vehicles since program inception

PROGRAM REVENUE

\$1,225,772

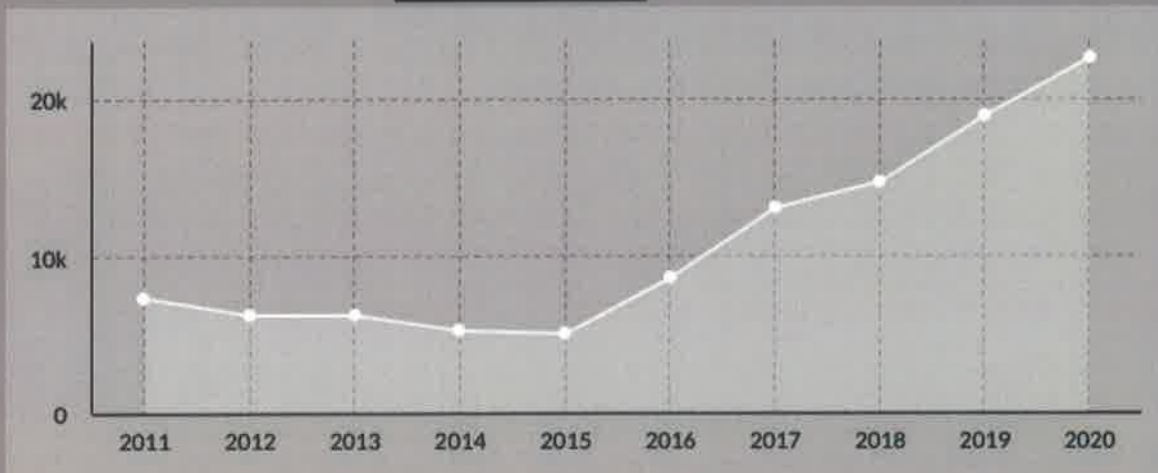
Yearly Average



VEHICLE ABATEMENTS

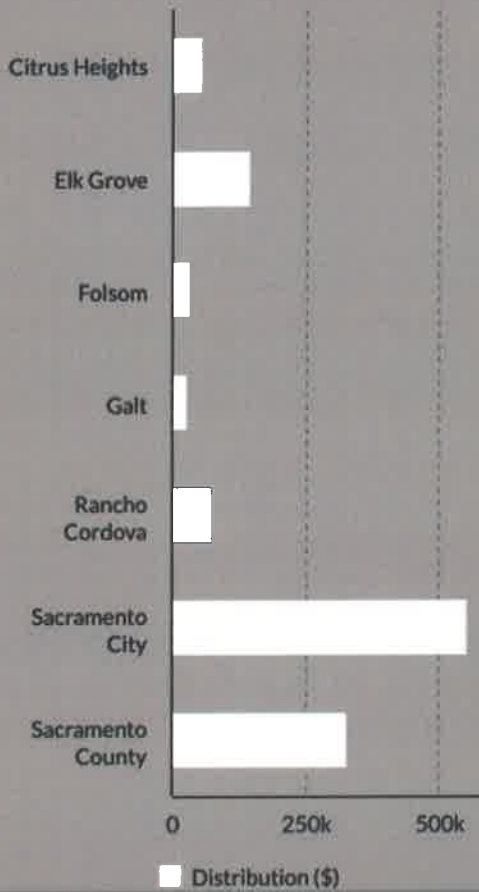
10,775

Yearly Average



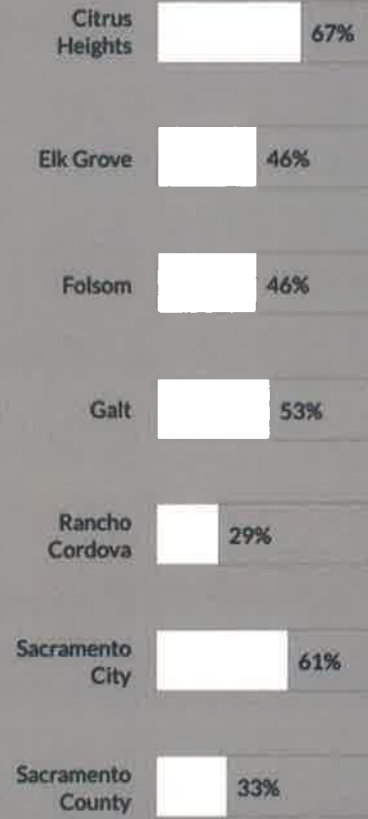
MEMBER DISTRIBUTION

Yearly Average (2018-2020)



MEMBER CLAIM COVERAGE

Yearly Average (2018-2020)



\$1,225,872
Member Distribution
yearly average

\$2,717,106
Member Claim
yearly average

24%

Revenue Growth

2011- 2020

207%

Abatement Growth

2011- 2020

Attachment B

Estimated Ballot Costs

Expense Type	Description	Rate	Qty *	Total
Base	General Election Base Admin Fee	\$ 1,887.00	1	\$ 1,887.00
Base	First Contest	\$ 1.2995	864,914	\$ 1,123,955.74
Additional	Additional Contest	\$ 0.1035	864,914	\$ 89,518.60
Additional	More than 4 pages in Voter Guide	\$ 0.0149	864,914	\$ 12,887.22
Additional	Contest requiring additional ballot cards	Actual Cost		
Estimated Total				\$ 1,228,248.56

* Registered Voters (5/14/21 count)

County Election's Office Fee Schedule (Most Recent)

November 3, 2020 Presidential General Election					
District Base Set-Up, including Measures	\$1,887.00		EC	\$\$	10002 10520
First Contest, including Measures	\$1.2295	per registered voter	EC	\$\$	10002 10520
Additional Contest, including Measures	\$0.1035	per registered voter	EC	\$\$	10002 10520
Additional Page over 4 in the County Voter Information Guide – Measures only	\$0.0149	per registered voter	EC	\$\$	10002 10520
Contest Requiring Additional Ballot Card(s)	Actual Cost		EC	\$\$	10002 10520
Special Elections	Actual Cost		EC	\$\$	10002 10520

Link to Fee Schedule

<https://elections.saccounty.net/Documents/Fee-Schedule.pdf>

Total Estimated Tax Measure Cost AllocationEstimated Costs **\$1,500,000.00**

	2020 Population		50% alloc. based on population	2020-21 Abatements		50% alloc. based on abatements	Cost Allocation
Citrus Heights	87,811	5.65%	\$ 42,365.19	607	2.70%	\$ 20,217.16	\$ 62,582.35
Elk Grove	176,154	11.33%	\$ 84,987.04	2,985	13.26%	\$ 99,420.46	\$ 184,407.50
Folsom	81,610	5.25%	\$ 39,373.46	76	0.34%	\$ 2,531.31	\$ 41,904.77
Galt	25,849	1.66%	\$ 12,471.08	456	2.03%	\$ 15,187.85	\$ 27,658.93
Rancho Cordova	78,381	5.04%	\$ 37,815.60	1,590	7.06%	\$ 52,957.63	\$ 90,773.23
Sacramento	510,931	32.87%	\$ 246,503.14	14,145	62.82%	\$ 471,123.10	\$ 717,626.24
Sacramento County	593,801	38.20%	\$ 286,484.50	2,659	11.81%	\$ 88,562.48	\$ 375,046.98
Totals	1,554,537	100.00%	\$ 750,000.00	22,518	100.00%	\$ 750,000.00	\$ 1,500,000.00

Meeting Minutes**Joint City Council / Successor Agency / Public Financing Authority Special Meeting /
Folsom South of 50 Parking Authority / Folsom Ranch Financing Authority Meeting
March 23, 2021****CALL TO ORDER:**

The joint City Council / Redevelopment Successor Agency / Public Financing Authority / Folsom South of 50 Parking Authority / Folsom Ranch Financing Authority meeting was called to order at 7:25 p.m. in City Council Chambers, 50 Natoma Street, Folsom, California, with Mayor Mike Kozlowski presiding.

ROLL CALL:

Council/Boardmembers: Howell, Rodriguez, Aquino, Chalamcherla, Kozlowski

CONSENT CALENDAR:

18. Approval of the December 8, 2020 Joint City Council / Successor Agency / Public Financing Authority / Folsom South of 50 Parking Authority / Folsom Ranch Financing Authority Meeting Minutes
19. Receive and File the City of Folsom, the Folsom Redevelopment Successor Agency, the Folsom Public Financing Authority, the Folsom Ranch Financing Authority, and the South of 50 Parking Authority Monthly Investment Reports for the Month of December 2020

Motion by Vice Mayor Sarah Aquino, second by Councilmember Kerri Howell to approve the Consent Calendar.

Motion carried with the following roll call vote:

AYES: Council/Boardmember(s): Howell, Rodriguez, Aquino, Chalamcherla, Kozlowski
NOES: Council/Boardmember(s): None
ABSENT: Council/Boardmember(s): None
ABSTAIN: Council/Boardmember(s): None

PUBLIC HEARING:

20. Folsom Ranch Financing Authority City of Folsom Community Facilities District No. 21 (White Rock Springs Ranch) Special Tax Revenue Bonds, Series 2021, and City of Folsom Community Facilities District No. 21 (White Rock Springs Ranch) Special Tax Bonds, Series 2021

i. Resolution No. 10603 – A Resolution of the City Council of the City of Folsom Authorizing the Issuance of the City of Folsom Community Facilities District No. 21 (White Rock Springs Ranch) Special Tax Bonds, Series, 2021, the Execution of a First Supplemental Indenture Providing therefor, Authorizing the Execution of a Local Obligation Purchase Contract, and Authorizing Necessary Actions and the Execution of Other Documents in Connection therewith

ii. Resolution No. 007 - Folsom Ranch FA - A Resolution of the Governing Board of the Folsom Ranch Financing Authority Authorizing the Issuance, Sale and Delivery of Not to Exceed \$15,000,000 Aggregate Principal Amount of City of Folsom Community Facilities District No. 21 (White Rock Springs Ranch) Special Tax Revenue Bonds, Series 2021; Approving the Form and Substance of a Trust Agreement, Authorizing Modifications thereof and Execution and Delivery as Modified; Approving a Preliminary Official Statement, Authorizing Changes thereto and Execution and Delivery thereof and of an Official Statement to be Derived therefrom; Approving a Local Obligation Purchase Contract and a Bond Purchase Contract and Execution and Delivery of Each; and Authorizing Related Actions Necessary to Implement the Proposed Financing

Finance Director/CFO Stacey Tamagni made a presentation and responded to questions from the City Council.

Mayor Mike Kozlowski opened the public hearing at 7:29 pm.

City Clerk Christa Freemantle read written comments from Laurette Laurent regarding bond issuance reports.

Motion by Councilmember Kerri Howell second by Councilmember Rosario Rodriguez to approve the Resolution No. 10603

Motion carried with the following roll call vote:

AYES: Councilmember(s): Howell, Rodriguez, Aquino, Chalamcherla, Kozlowski
NOES: Councilmember(s): None
ABSENT: Councilmember(s): None
ABSTAIN: Councilmember(s): None

Motion by Councilmember Rosario Rodriguez second by Councilmember Kerri Howell to approve the Resolution No. 007-Folsom Ranch FA.

Motion carried with the following roll call vote:

AYES: Council/Boardmember(s): Howell, Rodriguez, Aquino, Chalamcherla, Kozlowski
NOES: Council/Boardmember(s): None
ABSENT: Council/Boardmember(s): None
ABSTAIN: Council/Boardmember(s): None

ADJOURNMENT

There being no further business to come before the joint City Council / Redevelopment Successor Agency / Public Financing Authority / Folsom South of 50 Parking Authority / Folsom Ranch Financing Authority, the meeting was adjourned to the regular City Council meeting at 7:33 p.m.

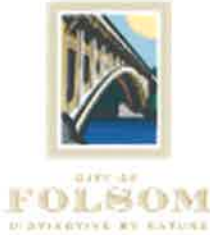
SUBMITTED BY:

Christa Freemantle, City Clerk/Board Secretary

ATTEST:

Mike Kozlowski, Mayor/Board Chair

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Folsom City Council Staff Report



MEETING DATE:	6/8/2021
AGENDA SECTION:	Consent Calendar
SUBJECT:	Receive and File the City of Folsom, the Folsom Redevelopment Successor Agency, the Folsom Public Financing Authority, the Folsom Ranch Financing Authority, and the South of 50 Parking Authority Monthly Investment Reports for the Month of March 2021
FROM:	Finance Department

RECOMMENDATION / CITY COUNCIL ACTION

The Finance Department recommends that the City Council receive and file the City of Folsom, the Folsom Redevelopment Successor Agency, the Folsom Public Financing Authority, the Folsom Ranch Financing Authority, and the South of 50 Parking Authority monthly Investment Reports for the month of March 2021.

BACKGROUND / ISSUE

Under the Charter of the City of Folsom and the authority granted by the City Council, the Finance Director is responsible for investing the unexpended cash of the City Treasury. The primary objectives of the City’s investment policy are to maintain the safety of investment principal, provide liquidity to meet the short and long-term cash flow needs of the City, and earn a market-average yield on investments. The City’s portfolio is managed in a manner responsive to the public trust and is consistent with state and local laws and the City’s investment policy. The Finance Department hereby submits the investment reports for the City of Folsom, the Folsom Redevelopment Successor Agency, the Folsom Public Financing Authority, the Folsom Ranch Financing Authority, and the South of 50 Parking Authority for the month of March 2021.

POLICY / RULE

1. Section 3.30.010(a) of the Folsom Municipal Code states “the term ‘city’ shall encompass the city of Folsom, the Folsom community *redevelopment agency*, and all other agencies and instrumentalities of the city under either the direct or indirect control of the city council, and this chapter regulates the investment of all moneys of those agencies.”
2. Section 3.30.030(f) of the Folsom Municipal Code states that “the city’s chief investment officer shall each month submit an investment report to the city council, which report shall include all required elements as prescribed by California Government code section 53646.”
3. California Government Code, Sections 53601 through 53659 sets forth the state law governing investments for municipal governments in California.
4. Section 3.30.020(g) of the Folsom Municipal Code states that “all city cash shall be consolidated into one general bank account as set out in this code and invested on a pooled concept basis. Interest earnings shall be allocated to all city funds and subfunds according to fund and subfund cash and investment balance on at least a quarterly basis.”

ANALYSIS

Overview

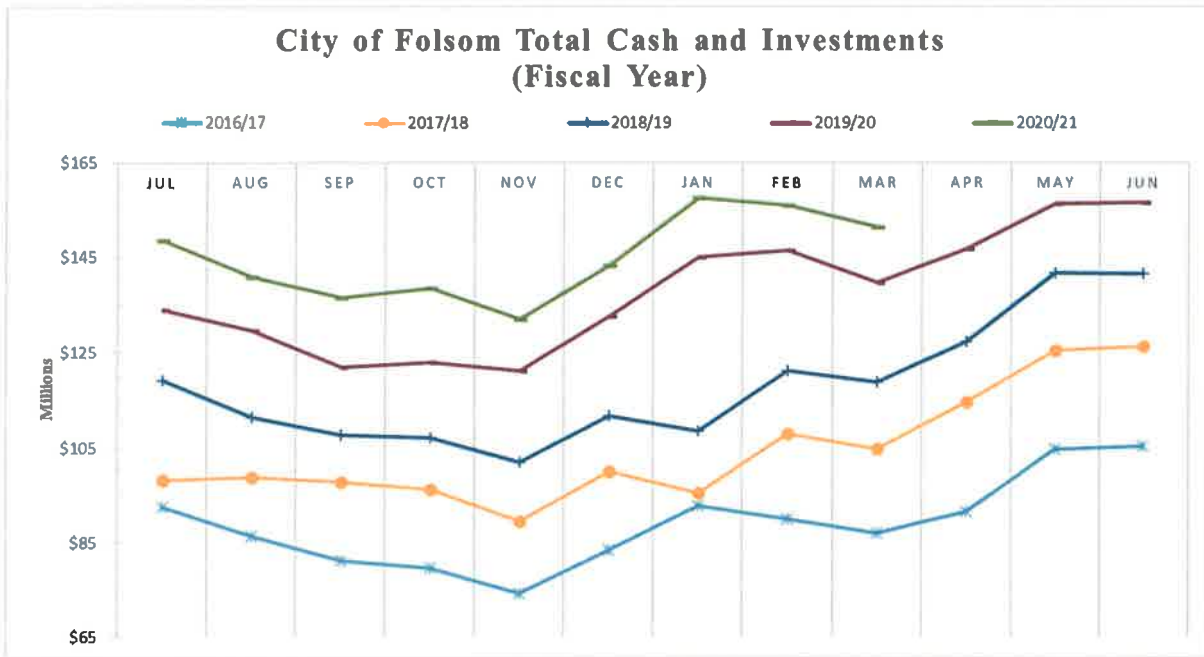
The City has diversified investments in accordance with the City Investment Policy and Government Code. The City of Folsom’s total cash and investments are invested on a pooled basis as required by the Folsom Municipal Code.

The Portfolio Summary of the City’s current report includes a “Pooled Equity Section” identifying the Redevelopment Successor Agency’s (RDSA) and Folsom Public Financing Authority’s (FPFA) portion of the investment pool. The RDSA and FPFA list these amounts under “Cash” in their respective sections. Currently, the Folsom Ranch Financing Authority (FRFA) has no funds invested in Pooled Equity.

City of Folsom

Total Cash and Investments

The following graph illustrates the City’s monthly cash and investment balances for fiscal years 2017 through 2021. Monthly fluctuations in cash and investments are the result of typical receipt of revenues less expenditures for operations, debt service, and capital improvements. As of March 31, 2021, the City’s cash and investments totaled \$151,601,388; an increase of \$11,787,024 (8%) from March 31, 2020.

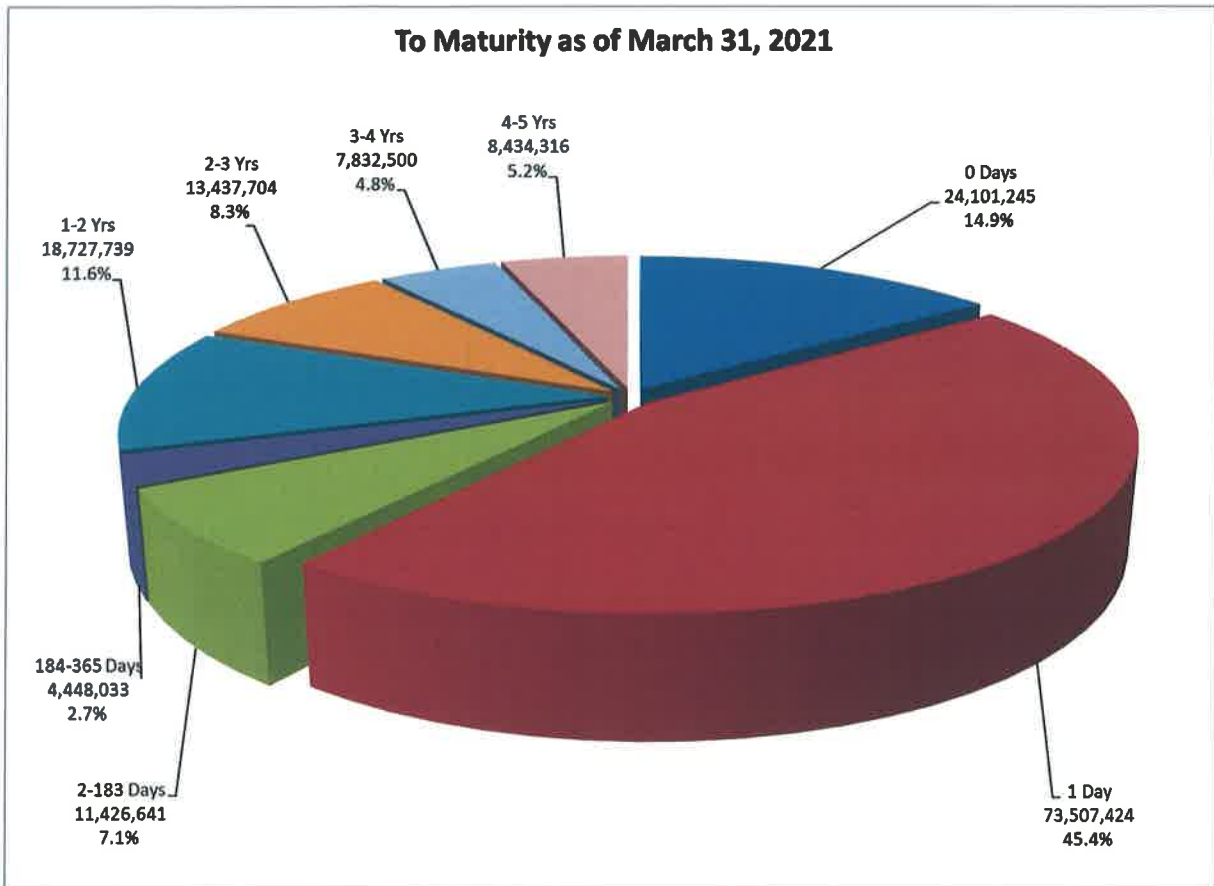


The following chart shows the City’s monthly cash and investment balances and percentage change for Fiscal Year 2020-21 along with the yearly dollar and percentage changes.

Total City Cash and Investments

	2020-21	Monthly Change	2019-20	Monthly Change	Yearly \$ Change	Yearly % Change
Jul	\$ 148,832,109		\$ 133,948,255		\$ 14,883,854	11%
Aug	\$ 140,982,833	-5%	\$ 129,657,395	-3%	\$ 11,325,438	9%
Sep	\$ 136,560,020	-3%	\$ 121,952,538	-6%	\$ 14,607,482	12%
Oct	\$ 138,527,756	1%	\$ 122,996,173	1%	\$ 15,531,582	13%
Nov	\$ 132,111,422	-5%	\$ 121,112,560	-2%	\$ 10,998,862	9%
Dec	\$ 143,343,763	9%	\$ 132,661,948	10%	\$ 10,681,815	8%
Jan	\$ 157,682,958	10%	\$ 145,156,622	9%	\$ 12,526,336	9%
Feb	\$ 156,040,724	-1%	\$ 146,494,239	1%	\$ 9,546,485	7%
Mar	\$ 151,601,388	-3%	\$ 139,814,364	-5%	\$ 11,787,024	8%

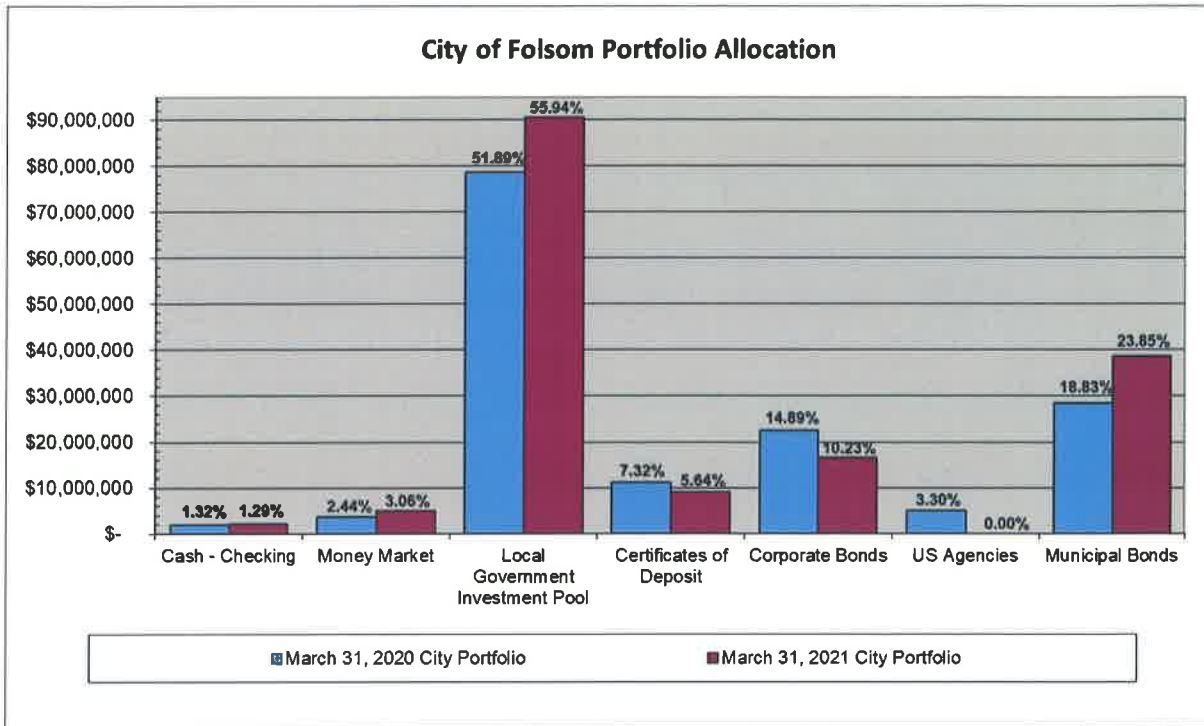
The City’s projected cash needs for the next six months are sufficiently provided for by anticipated revenues and the liquidity of its cash and investments. In addition, in accordance with Section 3.30.020(c) of the Municipal Code, less than 50% of the City’s total cash is invested for a period longer than one year.



Investment Performance

The City’s Portfolio Management Summary report for the month of March 2021 is presented in Attachment 1 to this report. Portfolio investment earnings (including pooled equity earnings for the RDSA and FPFA) for the three-month quarter ended March 31, 2021 totaled \$440,340. The total rate of return of the investment portfolio for the same period was 1.08%.

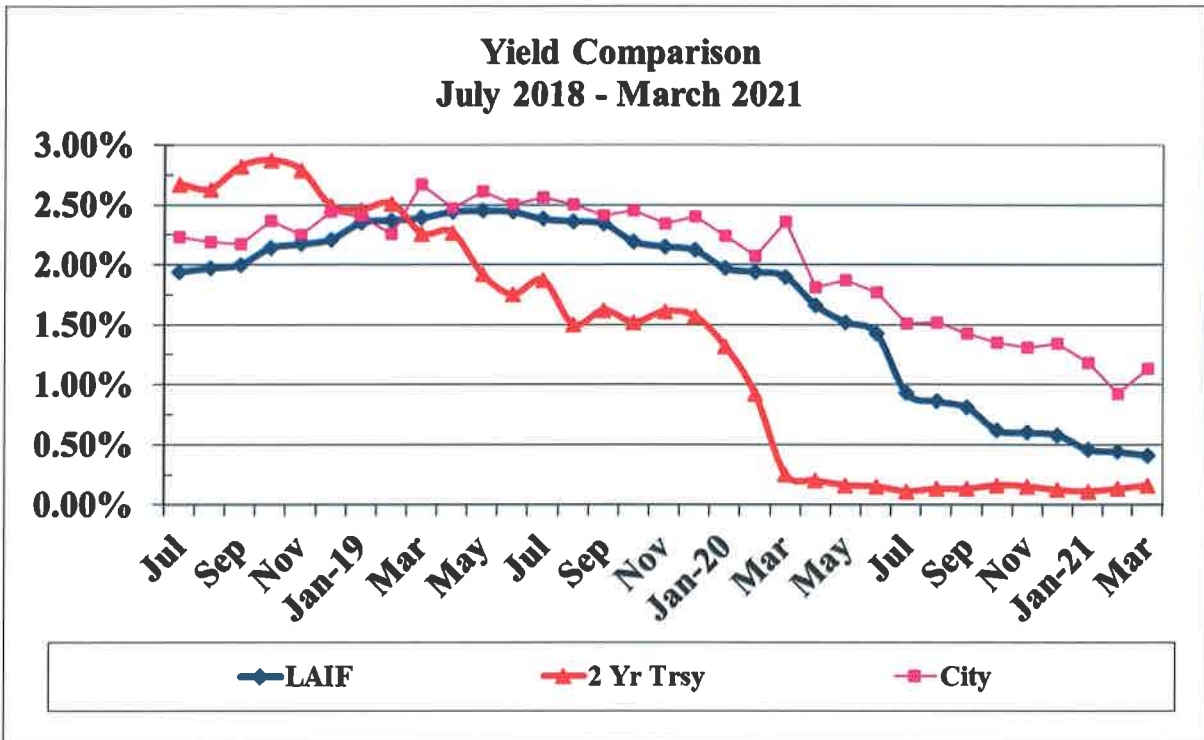
The following graph illustrates the total dollar amount and allocation percentages for December 31, 2019 and December 31, 2020. The percentages in this graph are based on book value.



A detailed listing of the portfolio holdings as of March 31, 2021 is included in Attachment 2 to this report.

The Local Government Investment Pool (LAIF) yield typically moves in the same direction as market yields, but is less volatile, lagging somewhat behind market moves. This can be seen in the chart on the next page, illustrating the historical monthly change in yield from July 2018 through December 2020 for LAIF and two-year US Treasury securities. The effective rate of return for the City Portfolio is also included.

The Federal Funds rate reached its post-2008 global economic downturn peak at 2.50% in December 2018. The Federal Open Market Committee (FOMC) began lowering the rate in July 2019, with three cuts resulting in the Federal Funds rate at 1.75% as of December 31, 2019. Due to the Covid-19 pandemic, the FOMC cut rates twice in March 2020, by 50 basis points on March 3rd, and 1% on March 15th. The effect of these rate cuts was to push down borrowing costs to help consumers and businesses handle the financial challenges posed by the economic slowdown that resulted from the pandemic. It has been anticipated that the Federal Funds rate will remain at this effective floor for a considerable period of time, likely until 2023 or beyond. On April 28, 2021 the Federal Reserve maintained its target for the Federal Funds rate at a range of 0% to .25%, and the FOMC reiterated that it intends to keep the benchmark rate at the current levels until inflation is at 2% for the long-term, allowing short term inflation to increase above 2% if necessary in an effort to achieve maximum employment.



A listing of transactions for the third quarter of fiscal year 2021 is included in Attachment 3 to this report.

Folsom Redevelopment Successor Agency

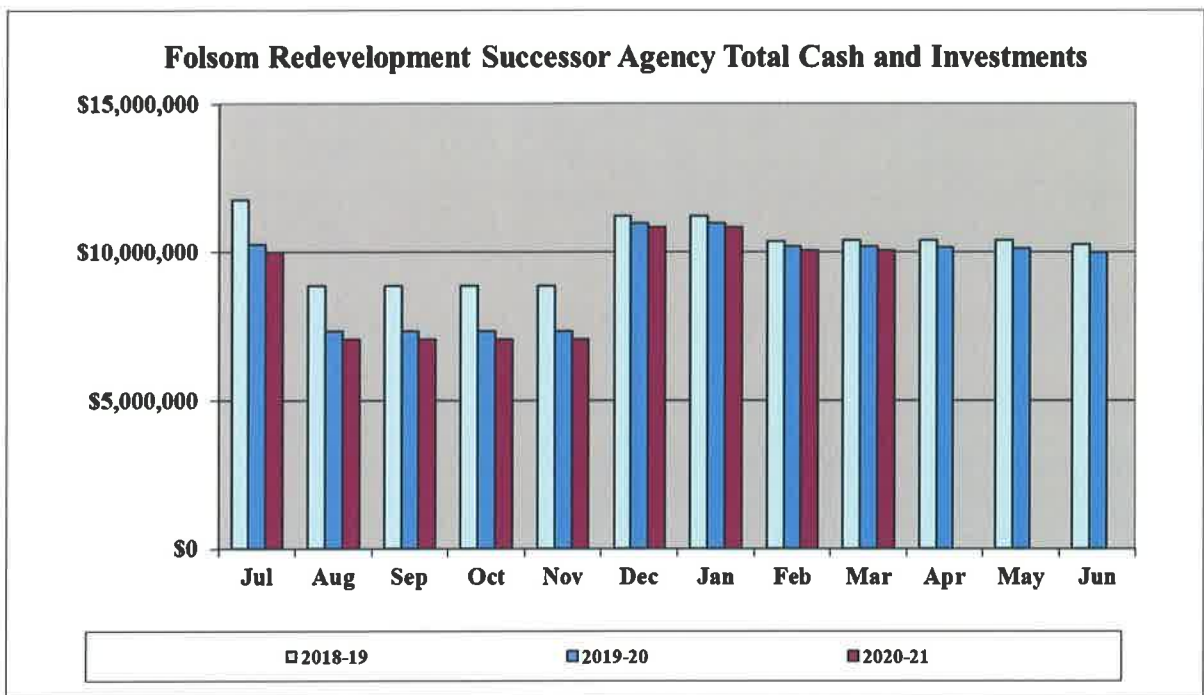
Total Cash and Investments

The RDSA had total cash and investments of \$10,077,532 as of March 31, 2021. This is a decrease of \$112,976 (1%) since March 31, 2020. The cash held by the RDSA is comprised of city-held funds, as well as 2011 bond proceeds to be utilized for housing and non-housing projects. These proceeds held by the Agency are broken out individually in the Portfolio Management Summary in Attachment 1.

The following table and graph illustrate the monthly balances and their respective percentage changes for the reporting period. Monthly fluctuations in cash and investments are the result of typical receipt of revenues less expenditures for operations, debt service, and capital improvements.

Folsom Redevelopment Successor Agency

	2020-21	Monthly Change	2019-20	Monthly Change	Yearly \$ Change	Yearly % Change
Jul	\$ 10,002,863		\$ 10,258,949		\$ (256,086)	-2%
Aug	\$ 7,059,737	-29%	\$ 7,325,874	-29%	\$ (266,137)	-4%
Sep	\$ 7,059,862	0%	\$ 7,325,990	0%	\$ (266,129)	-4%
Oct	\$ 7,056,132	0%	\$ 7,326,077	0%	\$ (269,944)	-4%
Nov	\$ 7,051,717	0%	\$ 7,321,623	0%	\$ (269,906)	-4%
Dec	\$ 10,861,946	54%	\$ 11,003,048	50%	\$ (141,101)	-1%
Jan	\$ 10,862,106	0%	\$ 11,003,290	0%	\$ (141,184)	-1%
Feb	\$ 10,077,418	-7%	\$ 10,190,327	-7%	\$ (112,910)	-1%
Mar	\$ 10,077,532	0%	\$ 10,190,508	0%	\$ (112,976)	-1%



The RDSA’s projected cash needs for the next six months are sufficiently provided for by anticipated revenues and the liquidity of its cash and investments.

Investment Performance

The RDSA’s Portfolio Management Summary report for the month of March 2021 is presented in Attachment 1. The RDSA’s investment earnings for the three-month quarter ended March 31, 2021 totaled \$26,892. The effective rate of return for the RDSA investment portfolio for the same time period is 1.04%.

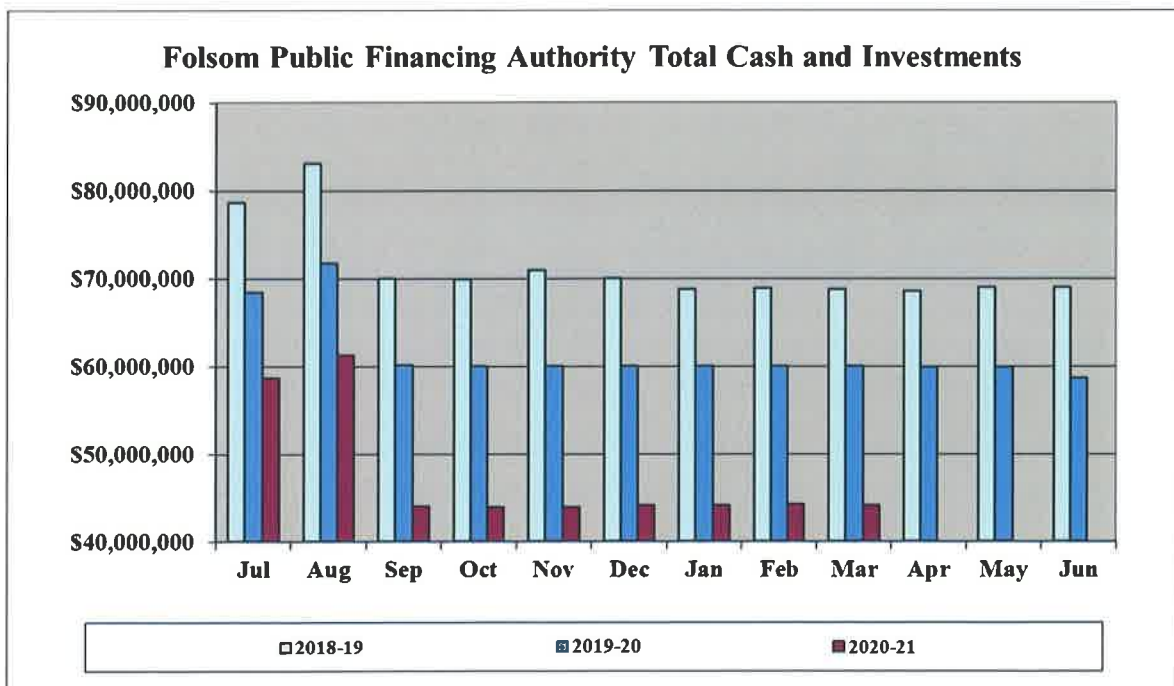
Folsom Public Financing Authority

Total Cash and Investments

The FPFA cash and investments totaled \$44,085,165 as of March 31, 2021. This is a decrease of \$15,915,966 (27%) from March 31, 2020. Monthly fluctuations in cash and investments are the result of typical receipt of debt service repayment revenues and the subsequent debt service expenditures. Additionally, on July 23, 2020, Community Facilities District No. 10 (CFD No. 10) issued \$6,394,000 in Special Tax Refunding Bonds, Series 2020 that refunded the Series 2010 CFD No. 10 Special Tax Bonds held as investments in FPFA, thus reducing FPFA investments by an additional \$9,325,000. The Series 2020 Bonds are not held as investments of FPFA. The following table and graph illustrate the monthly balances and their respective percentage changes for the reporting period.

Folsom Public Financing Authority

	2020-21	Monthly Change	2019-20	Monthly Change	Yearly \$ Change	Yearly % Change
Jul	\$ 58,623,181		\$ 68,442,988		\$ (9,819,807)	-14%
Aug	\$ 61,219,871	4%	\$ 71,705,665	5%	\$ (10,485,794)	-15%
Sep	\$ 43,955,323	-28%	\$ 60,112,114	-16%	\$ (16,156,790)	-27%
Oct	\$ 43,845,768	0%	\$ 60,000,597	0%	\$ (16,154,828)	-27%
Nov	\$ 43,845,770	0%	\$ 60,001,190	0%	\$ (16,155,420)	-27%
Dec	\$ 44,064,764	0%	\$ 60,001,153	0%	\$ (15,936,389)	-27%
Jan	\$ 44,064,765	0%	\$ 60,001,161	0%	\$ (15,936,395)	-27%
Feb	\$ 44,179,572	0%	\$ 60,001,168	0%	\$ (15,821,596)	-26%
Mar	\$ 44,085,165	0%	\$ 60,001,130	0%	\$ (15,915,966)	-27%



The FPFA's projected cash needs for the next six months are sufficiently provided for by anticipated revenues and the liquidity of its cash and investments.

Investment Performance

The FPFA's Portfolio Management Summary report for the month of March 2021 is presented in attachment 1. The FPFA's investment earnings for the three-month quarter ended March 31, 2021 totaled \$330,511. The effective rate of return for the FPFA investment portfolio for the same time period is 3.03%.

Folsom Ranch Financing Authority

Total Cash and Investments

The FRFA cash and investments totaled \$84,030,000 as of March 31, 2021, an increase of \$12,490,000 from the prior year. This increase is due to the issuance of \$12,925,000 Community Facilities District No. 23 (Folsom Ranch), Improvement Area 1 Local Obligations on October 30, 2020. The only other activity within FRFA was the maturing of \$435,000 of local obligations held by FRFA on September 1, 2020.

The Authority's projected cash needs for the next six months are sufficiently provided for by anticipated revenues and the liquidity of its cash and investments.

Investment Performance

The FRFA's Portfolio Management Summary report for the month of March 2021 is presented in attachment 1. The FRFA's investment earnings for the three-month quarter ended March 31, 2021 totaled \$994,712. The effective rate of return for the FRFA investment portfolio for the same time period is 4.82%.

South of 50 Parking Authority

Total Cash and Investments

The South of 50 Parking Authority cash and investments totaled \$0 as of March 31, 2021.

The Authority's projected cash needs for the next six months are sufficiently provided for by anticipated revenues and the liquidity of its cash and investments.

Investment Performance

There is no investment activity for the South of 50 Parking Authority.

ATTACHMENTS

1. City of Folsom, Folsom Redevelopment Successor Agency, Folsom Public Financing Authority, and Folsom Ranch Financing Authority Portfolio Management Summary March 2021
2. City of Folsom Portfolio Holdings as of March 31, 2021
3. City of Folsom Transaction Summary, Third Quarter of Fiscal Year 2021

Submitted,



Stacey Tamagni, Finance Director

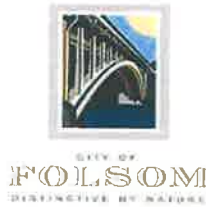
Agency Finance Officer

Folsom Public Financing Authority Treasurer

Folsom Ranch Financing Authority Treasurer

South of 50 Parking Authority Treasurer

ATTACHMENT 1



**City of Folsom
 Combined City of Folsom, Redevelopment
 Successor Agency, PFPA & FRFA
 Portfolio Summaries
 March 31, 2021**

06/08/2021 Item No.20.

50 Natoma St.
 Folsom, CA 95630
 (916) 461-8080

City of Folsom	YTM @ Cost	Face Amount/ Shares	Cost Value	Book Value	Market Value	Days to Maturity	Accrued Interest	% of Portfolio
Cash	0.038%	2,082,510.00	2,082,510.00	2,082,510.00	2,082,510.00	1	-	1.29
Certificate Of Deposit	2.340%	9,124,000.00	9,124,000.00	9,124,000.00	9,408,219.25	498	46,715.26	5.84
Corporate Bond	2.318%	16,530,000.00	16,609,340.00	16,561,563.78	16,700,723.90	858	138,108.99	10.21
Local Government Investment Pool	0.443%	90,571,756.92	90,571,756.92	90,571,756.92	90,571,756.92	1	-	55.97
Money Market	0.010%	4,954,402.25	4,954,402.25	4,954,402.25	4,954,402.25	1	-	3.08
Municipal Bond	1.717%	38,567,142.36	38,568,357.06	38,621,369.49	39,091,757.44	721	166,947.52	23.83
Total / Average	1.027%	\$ 161,829,811.53	\$ 161,910,366.23	\$ 161,915,602.44	\$ 162,809,369.76	288	\$ 351,771.77	100.00

Pooled Equity	Book Value	% of Portfolio	Interest Earnings (FY)	Total Rate of Return (FY)
City of Folsom Cash and Investments (excluding RDSA, PFPA & FRFA amounts)	\$ 151,601,387.92	93.63%	\$ 1,481,934.58	1.29%
Redevelopment Successor Agency (included in RDSA Investment Report)	10,077,528.48	6.22%	81,611.96	1.26%
Public Financing Authority (included in PFA Investment Report)	238,686.04	0.15%	1,106,538.29	3.13%
Folsom Ranch Financing Authority (included in FRFA Investment Report)	-	0.00%	2,823,121.58	4.83%
Total Pooled Equity	\$ 161,915,602.44	100.00%		

Redevelopment Successor Agency	YTM @ Cost	Face Amount/ Shares	Cost Value	Book Value	Market Value	Days to Maturity	Accrued Interest	% of Portfolio
Cash	1.130%	2,973,356.31	2,973,356.31	2,973,356.31	2,973,356.31	1	-	29.50
Non-Housing 2011A Proceeds	1.130%	2,591,295.00	2,591,295.00	2,591,295.00	2,591,295.00	1	-	25.71
Housing 2011B Proceeds	1.130%	4,512,877.17	4,512,877.17	4,512,877.17	4,512,877.17	1	-	44.78
Money Market	0.030%	3.78	3.78	3.78	3.78	1	-	0.00
Total / Average	1.130%	\$ 10,077,532.26	\$ 10,077,532.26	\$ 10,077,532.26	\$ 10,077,532.26	1	\$ -	100.00

Folsom Public Financing Authority	YTM @ Cost	Face Amount/ Shares	Cost Value	Book Value	Market Value	Days to Maturity	Accrued Interest	% of Portfolio
Cash	1.130%	236,686.04	236,686.04	236,686.04	236,686.04	1	-	0.54
Money Market	0.030%	88,271.54	88,271.54	88,271.54	88,271.54	1	-	0.20
Municipal Bond	3.015%	43,760,207.02	43,760,207.02	43,760,207.02	43,760,207.02	2823	109,958.45	99.26
Total / Average	2.999%	\$ 44,085,164.60	\$ 44,085,164.60	\$ 44,085,164.60	\$ 44,085,164.60	2802	\$ 109,958.45	100.00

Folsom Ranch Financing Authority	YTM @ Cost	Face Amount/ Shares	Cost Value	Book Value	Market Value	Days to Maturity	Accrued Interest	% of Portfolio
Money Market	0.030%	0.19	0.19	0.19	0.19	1	-	0.00
Municipal Bond	4.728%	84,030,000.00	84,030,000.00	84,030,000.00	84,030,000.00	7651	331,098.43	100.00
Total / Average	4.728%	\$ 84,030,000.19	\$ 84,030,000.19	\$ 84,030,000.19	\$ 84,030,000.19	7651	\$ 331,098.43	100.00

Stacey Tamagni

Stacey Tamagni, Finance Director Date
May 26, 2021

ATTACHMENT 2

City of Folsom Portfolio Holdings

Portfolio Holdings for Inv. Report
Report Format: By Transaction
Portfolio / Report Group: City of Folsom

Group By: Security Type
Average By: Face Amount / Shares
As of 3/31/2021

Description	Issuer	Coupon Rate	Face Amt/Shares	% of Portfolio
Cash				
Wells Fargo Cash	Wells Fargo	0.000	82,510.00	0.05
Wells Fargo Cash	Wells Fargo	0.040	2,000,000.00	1.24
Sub Total / Average Cash		0.038	2,082,510.00	1.29
Certificate Of Deposit				
American Express - Centurion 2.3 4/5/2021	American Express - Centurion	2.300	247,000.00	0.15
Appalachian Community CU 3.2 2/28/2022	Appalachian Community CU	3.200	245,000.00	0.15
Belmont Savings Bank 2.75 3/14/2023	Belmont Savings Bank	2.750	246,000.00	0.15
Beneficial Bank 2.15 10/18/2022	Beneficial Bank	2.150	247,000.00	0.15
BMW Bank 2.1 9/15/2021	BMW Bank	2.100	247,000.00	0.15
CenterState Bank, NA 1.4 3/30/2022	CenterState Bank, NA	1.400	248,000.00	0.15
CIT Bank, NA 1.05 3/28/2022	CIT Bank, NA	1.050	248,000.00	0.15
Citibank, NA 2.8 4/26/2021	Citibank, NA	2.800	245,000.00	0.15
Citizens Deposit Bank of Arlington, Inc. 3.1 8/29/	Citizens Deposit Bank of Arlington, Inc.	3.100	248,000.00	0.15
Discover Bank 2.25 2/15/2022	Discover Bank	2.250	247,000.00	0.15
East Boston Savings Bank 2.3 7/30/2021	East Boston Savings Bank	2.300	247,000.00	0.15
Essential FCU 3.55 12/5/2023	Essential FCU	3.550	245,000.00	0.15
Farmer's & Merchants Bank 3.3 9/27/2023	Farmer's & Merchants Bank	3.300	245,000.00	0.15
First Bank of Highland Park 2.2 8/23/2022	First Bank of Highland Park	2.200	247,000.00	0.15
Greenstate Credit Union 1.9 2/28/2022	Greenstate Credit Union	1.900	249,000.00	0.15
Jefferson Financial FCU 2.45 11/10/2022	Jefferson Financial FCU	2.450	245,000.00	0.15
Keesler FCU 3.05 8/30/2021	Keesler FCU	3.050	249,000.00	0.15
LCA Bank 2.1 6/21/2021	LCA Bank	2.100	247,000.00	0.15
Medallion Bank 2.15 10/11/2022	Medallion Bank	2.150	247,000.00	0.15
Mercantil Commercebank NA 1.65 6/24/2021	Mercantil Commercebank NA	1.650	247,000.00	0.15
Morgan Stanley Bank, NA 2.2 7/25/2024	Morgan Stanley Bank, NA	2.200	247,000.00	0.15
Morgan Stanley Private Bank, NA 2.2 7/18/2024	Morgan Stanley Private Bank, NA	2.200	247,000.00	0.15
National Cooperative Bank, NA 3.4 12/21/2023	National Cooperative Bank, NA	3.400	245,000.00	0.15
Neighbors FCU 3.3 9/19/2023	Neighbors FCU	3.300	245,000.00	0.15
Public Service CU 3.15 10/26/2021	Public Service CU	3.150	245,000.00	0.15
Raymond James Bank, NA 1.95 8/23/2023	Raymond James Bank, NA	1.950	247,000.00	0.15
Sallie Mae Bank 2.6 4/18/2022	Sallie Mae Bank	2.600	246,000.00	0.15
Synchrony Bank 2.4 5/19/2022	Synchrony Bank	2.400	240,000.00	0.15
Synovus Bank 1.45 10/17/2022	Synovus Bank	1.450	248,000.00	0.15
Third Federal Savings and Loan 2 7/28/2021	Third Federal Savings and Loan	2.000	245,000.00	0.15
TIAA, FSB 2.2 8/16/2022	TIAA, FSB	2.200	247,000.00	0.15
UBS Bank USA 2.9 4/3/2024	UBS Bank USA	2.900	249,000.00	0.15
Uinta County Bank 2.6 2/16/2023	Uinta County Bank	2.600	246,000.00	0.15

Valley Strong CU 1.1 9/20/2021	Valley Strong CU	1.100	249,000.00	0.15
VisionBank of Iowa 2.15 5/31/2022	VisionBank of Iowa	2.150	247,000.00	0.15
Wells Fargo National Bank West LV 1.9 1/29/2023	Wells Fargo National Bank West LV	1.900	249,000.00	0.15
Wex Bank 1.4 4/8/2022	Wex Bank	1.400	248,000.00	0.15
Sub Total / Average Certificate Of Deposit		2.340	9,124,000.00	5.64
Corporate Bond				
American Express Credit 2.25 5/5/2021-21	American Express Credit	2.250	1,000,000.00	0.62
Bank of America Corp 0.8 2/24/2026-22	Bank of America Corp	0.800	2,000,000.00	1.24
Bank of America Corp 3.499 8/17/2022-21	Bank of America Corp	3.499	2,000,000.00	1.24
Bank of Montreal Step 7/30/2025-21	Bank of Montreal	1.000	2,000,000.00	1.24
Bank of NY Mellon 2.6 2/7/2022-22	Bank of NY Mellon	2.600	1,000,000.00	0.62
Bank of NY Mellon 3.5 4/28/2023	Bank of NY Mellon	3.500	1,000,000.00	0.62
Barclays Bank PLC 3 10/17/2023-22	Barclays Bank PLC	3.000	2,000,000.00	1.24
MUFG Union Bank NA 3.15 4/1/2022-22	MUFG Union Bank NA	3.150	1,000,000.00	0.62
State Street Corp 2.653 5/15/2023-22	State Street Corp	2.653	2,530,000.00	1.56
Wells Fargo & Co 2.625 7/22/2022	Wells Fargo & Co	2.625	1,000,000.00	0.62
Wells Fargo & Co. 3.069 1/24/2023-22	Wells Fargo & Co.	3.069	1,000,000.00	0.62
Sub Total / Average Corporate Bond		2.450	16,530,000.00	10.21
Local Government Investment Pool				
LAIF City LGIP	LAIF City	0.443	60,360,900.51	37.30
LAIF FPFA LGIP	LAIF FPFA	0.443	30,210,856.41	18.67
Sub Total / Average Local Government Investment Pool		0.443	90,571,756.92	55.97
Money Market				
Wells Fargo MM	Wells Fargo	0.010	935,666.86	0.58
Wells Fargo MM	Wells Fargo	0.010	4,018,735.39	2.48
Sub Total / Average Money Market		0.010	4,954,402.25	3.06
Municipal Bond				
Alvord USD GOBs 1.062 8/1/2025	Alvord USD GOBs	1.062	1,280,000.00	0.79
Beverly Hills PFA Lease Rev 0.499 6/1/2023	Beverly Hills PFA Lease Rev	0.499	300,000.00	0.19
Beverly Hills PFA Lease Rev 0.73 6/1/2024	Beverly Hills PFA Lease Rev	0.730	300,000.00	0.19
CA St DWR Pwr Supp Rev 1.713 5/1/2021	CA St DWR Pwr Supp Rev	1.713	742,142.36	0.46
CA St DWR Pwr Supp Rev 2 5/1/2022	CA St DWR Pwr Supp Rev	2.000	1,000,000.00	0.62
CA St DWR Pwr Supp Rev 2 5/1/2022	CA St DWR Pwr Supp Rev	2.000	1,000,000.00	0.62
CA St DWR Pwr Supp Rev 5 5/1/2021	CA St DWR Pwr Supp Rev	5.000	500,000.00	0.31
Cabrillo CCD 1.913 8/1/2021	Cabrillo CCD	1.913	225,000.00	0.14
Cabrillo CCD 1.913 8/1/2022	Cabrillo CCD	1.913	230,000.00	0.14
Cal St Hlth Fac Fin Auth Rev 1.893 6/1/2022	Cal St Hlth Fac Fin Auth Rev	1.893	1,000,000.00	0.62
Carson RDA SA TABs 2.992 2/1/2022-17	Carson RDA SA TABs	2.992	100,000.00	0.06
Carson RDASA TABs 0.909 8/1/2022	Carson RDASA TABs	0.909	400,000.00	0.25
Carson RDASA TABs 0.981 8/1/2023	Carson RDASA TABs	0.981	300,000.00	0.19
Carson RDASA TABs 1.188 8/1/2024	Carson RDASA TABs	1.188	400,000.00	0.25
Carson RDASA TABs 1.288 8/1/2025	Carson RDASA TABs	1.288	300,000.00	0.19
Cent. Contra Costa San Dist Rev. Bonds 2.96 9/1/20	Cent. Contra Costa San Dist Rev. Bonds	2.960	235,000.00	0.15
Citrus CCD GOBs 0.669 8/1/2024	Citrus CCD GOBs	0.669	400,000.00	0.25
Citrus CCD GOBs 0.819 8/1/2025	Citrus CCD GOBs	0.819	400,000.00	0.25
Coast CCD GOBs 1.975 8/1/2023	Coast CCD GOBs	1.975	1,265,000.00	0.78
Colton USD 0.702 8/1/2023	Colton USD	0.702	1,000,000.00	0.62
Colton USD 0.882 8/1/2024	Colton USD	0.882	1,000,000.00	0.62

CSU Revenue Bonds 0.685 11/1/2024	CSU Revenue Bonds	0.685	500,000.00	0.31
CSU Revenue Bonds 2.982 11/1/2021	CSU Revenue Bonds	2.982	300,000.00	0.19
Davis RDA-SA TABs 1.68 9/1/2021	Davis RDA-SA TABs	1.680	1,200,000.00	0.74
Davis RDA-SA TABs 1.72 9/1/2022	Davis RDA-SA TABs	1.720	1,225,000.00	0.76
Davis RDA-SA TABs 1.75 9/1/2023	Davis RDA-SA TABs	1.750	625,000.00	0.39
Folsom Cordova USD Imp Dist 2 2 10/1/2021	Folsom Cordova USD Imp Dist 2	2.000	175,000.00	0.11
Folsom Cordova USD Imp Dist 2 2 10/1/2022	Folsom Cordova USD Imp Dist 2	2.000	160,000.00	0.10
Folsom Cordova USD Imp Dist 2 2 10/1/2023	Folsom Cordova USD Imp Dist 2	2.000	165,000.00	0.10
Folsom Cordova USD Imp Dist 2 2 10/1/2024	Folsom Cordova USD Imp Dist 2	2.000	1,000,000.00	0.62
Folsom Cordova USD Imp Dist 2 2 10/1/2025	Folsom Cordova USD Imp Dist 2	2.000	1,040,000.00	0.64
Gilroy USD 1.721 8/1/2021	Gilroy USD	1.721	320,000.00	0.20
Highland RDA-SA TABs 2.25 2/1/2022	Highland RDA-SA TABs	2.250	390,000.00	0.24
Imperial CCD 1.874 8/1/2021	Imperial CCD	1.874	200,000.00	0.12
LA Cnty RDA Ref Auth 2 9/1/2022	LA Cnty RDA Ref Auth	2.000	1,235,000.00	0.76
Long Beach CCD 1.738 8/1/2021	Long Beach CCD	1.738	780,000.00	0.48
Marin CCD GOBs 2.243 8/1/2021-16	Marin CCD GOBs	2.243	650,000.00	0.40
Murrieta RDA-SA TABs 2.25 8/1/2021	Murrieta RDA-SA TABs	2.250	350,000.00	0.22
Murrieta RDA-SA TABs 2.5 8/1/2022	Murrieta RDA-SA TABs	2.500	250,000.00	0.15
Oxnard SD GOBs 0.587 8/1/2022	Oxnard SD GOBs	0.587	700,000.00	0.43
Oxnard SD GOBs 0.82 8/1/2024	Oxnard SD GOBs	0.820	345,000.00	0.21
Rancho Santiago CCD GOBs 0.634 9/1/2024	Rancho Santiago CCD GOBs	0.634	1,000,000.00	0.62
Richmond RDASA 0.43 9/1/2021	Richmond RDASA	0.430	200,000.00	0.12
Riverside Cnty PFA 1.84 7/1/2023	Riverside Cnty PFA	1.840	195,000.00	0.12
San Francisco RDA SA TABs 2.796 8/1/2021	San Francisco RDA SA TABs	2.796	1,000,000.00	0.62
San Jose Evergreen CCD 0.921 9/1/2025	San Jose Evergreen CCD	0.921	1,000,000.00	0.62
San Jose RDA-SA 2.63 8/1/2022	San Jose RDA-SA	2.630	1,000,000.00	0.62
San Jose RDA-SA 2.83 8/1/2022	San Jose RDA-SA	2.630	1,000,000.00	0.62
San Jose RDASA TABs 2.828 8/1/2023	San Jose RDASA TABs	2.828	500,000.00	0.31
Santa Clarita PFA Lease Rev 0.976 6/1/2024-20	Santa Clarita PFA Lease Rev	0.976	355,000.00	0.22
Santa Clarita PFA Lease Rev 1.176 6/1/2025-20	Santa Clarita PFA Lease Rev	1.176	360,000.00	0.22
SF BART Rev Bonds 2.621 7/1/2023-17	SF BART Rev Bonds	2.621	735,000.00	0.45
State of CA GO 2.5 10/1/2022	State of CA GO	2.500	1,000,000.00	0.62
Univ of CA Revenue 0.628 5/15/2023-23	Univ of CA Revenue	0.628	125,000.00	0.08
Univ of CA Revenue 0.833 5/15/2024-24	Univ of CA Revenue	0.833	250,000.00	0.15
Univ of CA Revenue 2.15 5/15/2021-17	Univ of CA Revenue	2.150	1,000,000.00	0.62
Univ of CA Revenue 2.553 5/15/2021-19	Univ of CA Revenue	2.553	300,000.00	0.19
Univ of CA Revenue 2.657 5/15/2023-19	Univ of CA Revenue	2.657	500,000.00	0.31
Univ of CA Revenue 3.283 5/15/2022-18	Univ of CA Revenue	3.283	500,000.00	0.31
West Contra Costa USD 1.434 8/1/2022	West Contra Costa USD	1.434	360,000.00	0.22
West Contra Costa USD 1.761 8/1/2024	West Contra Costa USD	1.761	1,000,000.00	0.62
West Contra Costa USD GOBs 3.031 8/1/2021	West Contra Costa USD GOBs	3.031	300,000.00	0.19
West Sacramento Area Flood Control Agy 1.797 9/1/2	West Sacramento Area Flood Control Agy	1.797	200,000.00	0.12
West Sacramento Area Flood Control Agy 1.847 9/1/2	West Sacramento Area Flood Control Agy	1.847	200,000.00	0.12
Yosemite CCD 0.561 8/1/2023	Yosemite CCD	0.561	1,000,000.00	0.62
Yosemite CCD 0.804 8/1/2024	Yosemite CCD	0.804	500,000.00	0.31
Sub Total / Average Municipal Bond		1.761	38,567,142.36	23.83
Total / Average		1.051	161,829,811.53	100.00

ATTACHMENT 3

City of Folsom Transactions Summary

Transaction Summary - Investment Report
Portfolio / Report Group: City of Folsom

Group By: Action
Begin Date: 01/01/2021, End Date: 03/31/2021

Description	Security Type	Settlement Date	Maturity Date	Face Amt/Shares	Principal	YTM @ Cost
Buy						
Bank of America Corp 0.8 2/24/2026-22	Corporate Bond	2/24/2021	2/24/2026	2,000,000.00	2,000,000.00	0.800
Richmond RDASA 0.43 9/1/2021	Municipal Bond	2/9/2021	9/1/2021	200,000.00	200,000.00	0.430
Sub Total / Average Buy				2,200,000.00	2,200,000.00	
Called						
Bank of America Corp Step 2/10/2025-21	Corporate Bond	2/10/2021	2/10/2025	2,000,000.00	2,000,000.00	0.000
JPMorgan Chase & Co 2.55 3/1/2021-21	Corporate Bond	2/18/2021	3/1/2021	2,000,000.00	2,000,000.00	0.000
PNC Bank NA 2.15 4/29/2021-21	Corporate Bond	3/30/2021	4/29/2021	1,000,000.00	1,000,000.00	0.000
PNC Bank NA 2.15 4/29/2021-21	Corporate Bond	3/30/2021	4/29/2021	1,000,000.00	1,000,000.00	0.000
Sub Total / Average Called				6,000,000.00	6,000,000.00	
Matured						
Carson RDA SA TABs 2.742 2/1/2021	Municipal Bond	2/1/2021	2/1/2021	145,000.00	145,000.00	0.000
Highland RDA-SA TABs 2 2/1/2021	Municipal Bond	2/1/2021	2/1/2021	405,000.00	405,000.00	0.000
Valliance Bank 1.55 2/19/2021	Certificate Of Deposit	2/19/2021	2/19/2021	247,000.00	247,000.00	0.000
Wells Fargo & Co 2.5 3/4/2021	Corporate Bond	3/4/2021	3/4/2021	2,000,000.00	2,000,000.00	0.000
Sub Total / Average Matured				2,797,000.00	2,797,000.00	